



SCAPPOOSE

Oregon

Monday, January 5, 2026
City Council Meeting Agenda

Regular Meeting 7:00pm
Council Chambers
33568 East Columbia Avenue
Scappoose, Oregon 97056

| ITEM AGENDA TOPIC | Action |
|---|------------------------------|
| Call to Order | |
| Pledge of Allegiance | |
| Roll Call | |
| Approval of the Agenda | |
| Public Comment ~ Items not on the agenda | |
| Please sign a speaker request form and turn it in to the City Recorder along with any written testimony | |
| 1. Consent Agenda December 15, 2025 City Council work session minutes and December 15, 2025 City Council meeting minutes | |
| New Business | |
| 2. Discussion on Library District Bond to be placed on the May Election Ballot | |
| Jolene Jonas & Karen Kessi | |
| 3. Scappoose Community Events ~ Earth Day, Adventure Fest and Movies in the park | |
| JJ Duehren | |
| 4. Ordinance No. 924: An Ordinance Approving Annexation of Property to the City of Scappoose, Amending The Zoning Map, And Minor Partition | |
| Assistant to City Manager/City Planner N.J. Johnson | Public Hearing/First Reading |

Announcements – information only

5 Calendar

6. Updates: City Manager; Police Chief; Councilors; and Mayor

Adjournment Please note: If you would like to speak with City staff about a particular agenda item, please call City Hall at 503-543-7146, no later than 3:00 pm on the day of the meeting.

This meeting will be conducted in a handicap accessible room. If special accommodations are needed, please contact City Hall at (503) 543-7146, ext. 224 in advance.

TTY 1-503-378-5938



SCAPPOOSE

Oregon

Monday, December 15, 2025

Work Session ~ City Council & Planning Commission ~ 50 Year Plan, 6:00pm

Council Chambers, 33568 East Columbia Avenue, Scappoose, Oregon 97056

Disclaimer: These minutes are intended to summarize the conversations that took place in this meeting rather than provide a full transcript. Anyone wishing to view the full conversation can find a recording of this meeting on YouTube at www.youtube.com/watch?v=DW7I_mpVcRk.

Call to Order

Mayor Backus called the December 15, 2025 City Council work session to order at 6:00pm.

Present: Mayor Joseph A. Backus; Councilor Jeanett Santiago; Councilor Kim Holmes; Councilor Joel Haugen; Councilor Marty Marquis; Councilor John E. Riutta; Student Representative Tyler Ferreira; Planning Commission Chair Scott Jensen; Planning Commission Vice Chair Harlow ; Vernwald; Planning Commissioner Rita Bernhard; Planning Commissioner Sara Jones-Graham; Planning Commissioner Harry Bludworth; and Planning Commissioner Peter Williamson; City Manager Benjamin Burgener; Community Development Director Laurie Oliver Joseph; City Engineer Chris Neglespach; City Recorder Susan M. Reeves; Assistant to City Manager/City Planner N.J. Johnson; and Consultants; Greg McGreeey, Romano VP of Development and Stacey Shields, Romano Director of Entitlements.

Remote: Council President Tyler Miller; and Legal Counsel Ashleigh Dougill (joined at 6:49pm)

Community Development Director Laurie Oliver Joseph stated thank you to Council and the Planning Commissioner's for being here tonight. She is just going to do a quick recap and then we'll launch into our purpose here tonight, which is to hear some results of the market study. She explained on September 30th we had a work session to basically get two questions answered; Should the City redesignate Mr. Parker's property as part of our 50-year plan project, and which subareas should the City include in the UGB expansion area. She explained that at that work session they went over the staff response memo to Council questions that were submitted after the August 18th work session. It was determined that Council wanted to wait for the results of the market study before deciding on what to do with Mr. Parker's redesignation. We talked through subareas 7 and 8 in terms of inclusion in the Urban Growth Boundary. A majority of Council was in favor of including subarea 7, since it would allow the City to include those areas in our master plans and then once they are in the master plans we could start collecting system development charges for the improvements that are needed in those areas. Since the last meeting she has received a letter of support from Doug Bean on October 8, which was handed out this evening. Doug is representing Ed Freeman's Columbia Commerce.

Greg McGreeey, Romano VP of Development and Stacey Shields, Romano Director of Entitlements went over the presentation.

Greg McGreeey explained as Laurie alluded to, we are working with Mr. Parker and his team up front to help them work through some of the preliminary steps needed to redevelop a significant site such as the quarry site. He explained they are also simultaneously negotiating what a potential partnership might look like for them to participate in that site on the longer term. He stated what Mr. Parker and his team asked them to do was to come and walk Council through some fairly high-level points of the market analysis that they just completed. He explained you'll see the logo of ECONorthwest in the presentation because that is a firm that they use on a regular basis. He stated you all will know them because he believes they've participated in some of the City's economic analysis as well, but their team is separate from the one that does the municipal work for the City. That's been the consultant that Stacey and he have used for economic analysis for some time. He explained he and Stacey are going to walk through the presentation. They gave an overview of Romano and who they are.



PARKER QUARRY

ROMANO TEAM AND PRESENTATION PURPOSE



GREG MCGREEVY
VP of Development

Romano is supporting Scott Parker and team on the initial steps toward the redevelopment of the quarry.

Also, we are in negotiations to become the development partner on the site along side Scott and team.



STACEY SHIELDS
Director of Entitlements

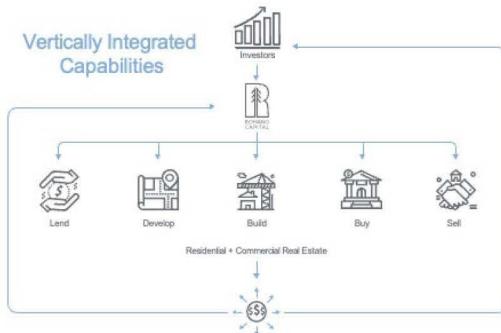
Tonight's purpose:

- Highlight our newly completed market study conclusions;
- Support Scott Parker's request for a rezone of his site into the expanded commercial designation.

ABOUT



WHO WE ARE



Investment First: Proven performance over 20 years as an investment company means we can navigate all market cycles and understand complex capital stacks

Developer and Lender Roots: Our dual role as developer and lender provides us with intimate knowledge of both sides of transactions.

Co-Investment with Investors: We invest alongside our investors, forging shared commitment and mutual success. **Structure allows for local investment.**

Romano Capital stands for strategic excellence, ethical commitment, and steadfast dedication to our investors and community. With a successful track record and a future guided by our principles, we strive to innovate and uphold integrity with every undertaking.

ROMANO CAPITAL QUALIFICATIONS

\$750M+

Total Projects

\$280M+

Total invested in
Romano offerings

190M+

Total actively invested

Execution and Agility: balance of nimble size and capability to execute.

Experienced Team: experience executing multiple master plans and complex projects.

Investment First Structure: allows for local investors to participate.

Successful Partnerships: numerous successful municipality partnerships through multiple developer and tax abatement agreements.

Well-Positioned: no downturn "baggage" means ready to deploy capital.

Diverse Project Portfolio: focus on top locations in emerging communities

Track record: successful navigation of full market cycles

MARKET ANALYSIS

SCAPPOOSE

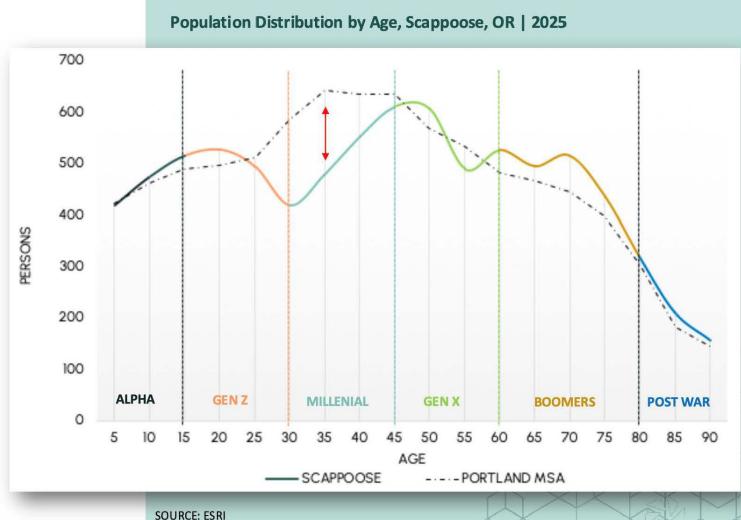
OREGON

ECONOMIC TRENDS AND CONDITIONS

GENERATIONAL AGE

Scappoose's age profile deviates from the Portland metro considerably, with a far lower concentration of working-age millennials and young families. This **likely reflects limited local jobs and rental options**. In contrast, Boomer residents are slightly overrepresented, consistent with a stable, aging homeowner base.

Long-term opportunities for economic growth and diversification in housing options have the potential to bring local demographics more in line with regional trends.



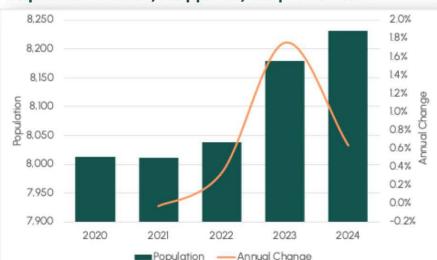
ECONOMIC TRENDS AND CONDITIONS

POPULATION GROWTH

Scappoose makes up about 15.3 percent of the Columbia County population. Growth has been slow over the current decade, expanding by only **2.7 percent since 2020**. However, the City experienced rapid growth between 2000 and 2020.

| Area | 2000 | 2010 | 2020 | 2021 | 2022 | 2023 | 2024 |
|-----------------|---------------|---------------|---------------|---------------|---------------|---------------|---------------|
| Columbia County | 43,560 | 49,351 | 52,589 | 52,610 | 52,899 | 53,262 | 53,639 |
| St. Helens | 10,019 | 12,883 | 13,817 | 14,238 | 14,223 | 14,383 | 14,492 |
| Scappoose | 4,976 | 6,592 | 8,010 | 8,011 | 8,038 | 8,179 | 8,231 |

Population Growth, Scappoose, OR | 2020-2024



SOURCE: Portland State University Population Research Center

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Scappoose's population grew only modestly over the past four years, adding roughly 218 residents since 2020. Growth peaked in 2023 with additions to the housing stock and pandemic-related migration shifts.

Stable growth suggests long-term confidence; however, **low rates indicate that market support will have to rely on migration from outside the area.**

 ECONorthwest

ECONOMIC TRENDS AND CONDITIONS

DEMOGRAPHIC PROFILE

Scappoose is projected to experience steady but moderate growth over the next five-years, with population forecasted to increase by only 162 residents through 2030, according to ESRI. Household growth (4.8%) has been outpacing population growth (3.1%), indicating continued formation of smaller households and incremental infill. Family households have also risen gradually, a sign that Scappoose is appealing more to family-oriented buyers and renters.

Although renter growth is slightly higher proportionally, the market continues to behave like a stable, ownership-driven suburban community with limited multifamily supply and strong demand for attainable workforce housing,

Demographic Profile, Scappoose, OR | 2020-2030

| Metric | 2020 | 2025 | 2030 | '20-'25 # | '20-'25 % |
|-------------------------------|------------|------------|------------|------------|-------------|
| Population | 8,010 | 8,262 | 8,424 | 252 | 3.1% |
| Households | 3,094 | 3,243 | 3,332 | 149 | 4.8% |
| Families | 2,114 | 2,186 | 2,177 | 72 | 3.4% |
| Owner Occupied Housing Units | 2,208 | 2,296 | 2,387 | 88 | 4.0% |
| Renter Occupied Housing Units | 886 | 947 | 945 | 61 | 6.9% |
| Owner Tenure | 71% | 71% | 72% | - | - |
| Renter Tenure | 29% | 29% | 28% | - | - |

SOURCE: ESRI

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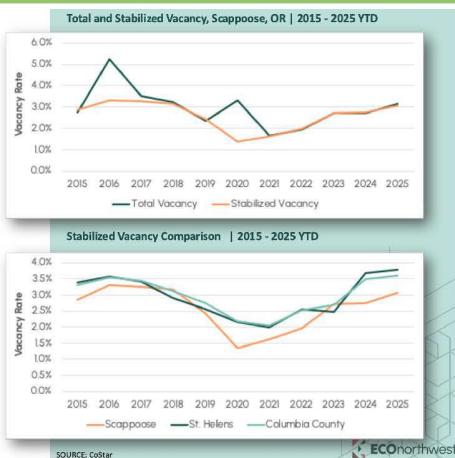
 ECONorthwest

DEMOGRAPHIC SUMMARY



- Low to Moderate Growth not accounting for in-migration
- Lack of job opportunities and new housing options likely limiting opportunity for growth, particularly with younger families
- Opportunity to capitalize on regional migration trends out of core markets
- Growing bedroom community with expectation for maturation of market into additional construction, services and lifestyle jobs

MF HOUSING MARKET DYNAMICS



INDICATIVE OF STABLE MARKET WITH LOW SUPPLY
NEW PRODUCT SHOULD ABSORB QUICKLY
UNCERTAIN DEPTH OF MARKET DUE TO DEPENDENCE ON IN MIGRATION

RETAIL MARKET DYNAMICS

LOW SUPPLY MARKET MEANS LIKELY SIGNIFICANT RETAIL LEAKAGE

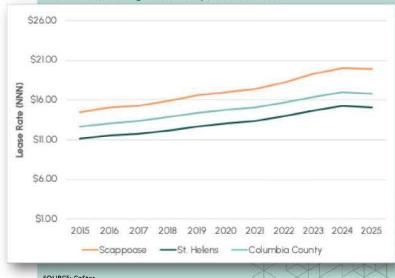
LOW VACANCY RATE MAINTAINS STRONG RENTS

DEMAND ON LOW END OF 12,000 SQFT OF WELL POSITIONED RETAIL ANNUALLY OVER 10 YEARS

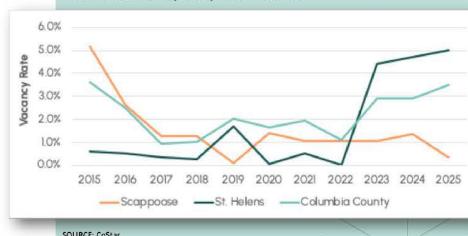
Market Comparison | Fourth Quarter 2025

| Market Variables | St. Helens | Scappoose | Columbia County |
|---------------------|------------|-----------|-----------------|
| Total Structures | 93 | 55 | 220 |
| Total Inventory | 734,000 | 446,000 | 1,600,000 |
| Vacant Space | 35,961 | 1,002 | 54,000 |
| Under Construction | 0 | 0 | 0 |
| 12-Month Deliveries | 0 | 0 | 0 |
| 12-Month Absorption | -2,863 | 5,000 | -5,885 |

Commercial Asking Rent Trend | 2015 - 2025 YTD

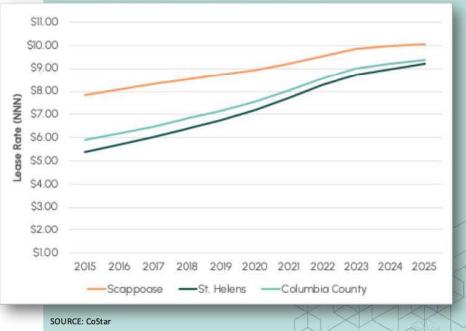


Commercial Vacancy Rate | 2015 - 2025 YTD



INDUSTRIAL MARKET DYNAMICS

Industrial Asking Rent Trend | 2015 - 2025 YTD

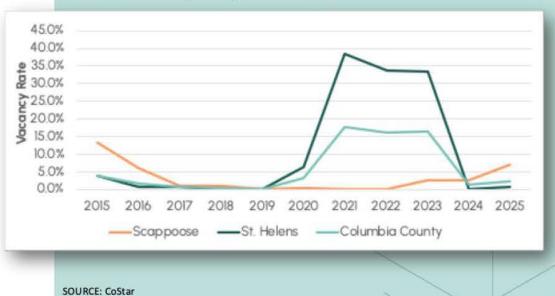


Rents 14% Below Portland Metro Average

Low Vacancy due to lack of deliveries, Does not include Cascades Tissue space that is now vacant.

Industrial demand at scale will require relocation into market

Industrial Vacancy Rate | 2015 - 2025 YTD



Market Comparison | Fourth Quarter 2025

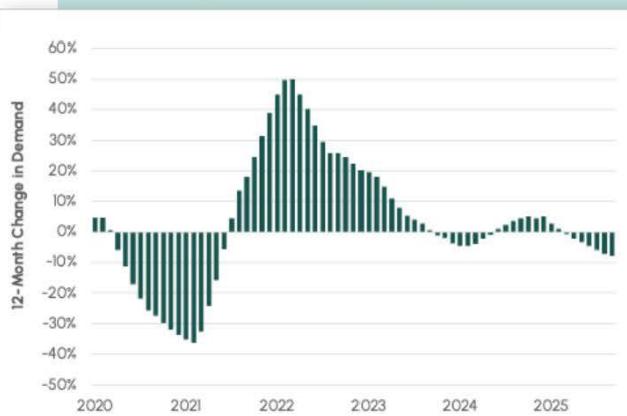
| Market Variables | St. Helens | Scappoose | Columbia County |
|---------------------|------------|-----------|-----------------|
| Total Structures | 37 | 19 | 75 |
| Total Inventory | 1,300,000 | 568,000 | 2,700,000 |
| Vacant Space | N/A | N/A | N/A |
| Under Construction | 0 | 0 | 0 |
| 12-Month Deliveries | 0 | 0 | 0 |
| 12-Month Absorption | -6,300 | -24,000 | -24,900 |

HOTEL DEMAND

Hotel demand in the market followed a sharp contraction and rebound cycle over the past five years. Demand fell nearly 40% in 2020 amid pandemic disruptions, then surged by more than 50% in 2022 as travel resumed and leisure visitation spiked. Growth has since moderated, with small year-over-year declines in 2024–2025 suggesting the market has stabilized at a mature, post-recovery level rather than weakening fundamentally. The market's strong recovery and subsequent normalization indicate a durable demand base.

**Demand most likely driven by
Business travel tied to
growing economic base and
OMIC**

12-month Change in Hotel Demand | 2020 – 2025 YTD



SOURCE: CoStar



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INDUSTRIAL MARKET ANALYSIS

FACTORS DRIVING INDUSTRIAL DEMAND | COMPETITIVE ADVANTAGES IN SCAPPOOSE

OMIC

OMIC and PCC anchor Scappoose's advanced manufacturing ecosystem, providing R&D, prototyping, and industry-aligned training that supplies skilled workers, attracts global partners, supports high-wage jobs, and strengthens opportunities for manufacturing cluster growth.

LOCATION

Scappoose offers quick access to Portland's labor market and amenities while providing a high quality of life, with short commutes to downtown and Portland International Airport.

TRANSPORTATION

Scappoose sits on Highway 30 with direct connections to I-5 and is 40 minutes from Port of Portland freight facilities, supporting efficient trucking, distribution, and global goods movement.

SCAPPOOSE AIRPORT

Scappoose Airport provides a 5,000-foot runway supporting regional economic growth and future OMIC activity, with strong demand for personal hangars and remaining capacity for corporate expansion.

PNWR RAILROAD

PNWR rail service connects Scappoose to major regional markets, offering an efficient, sustainable freight alternative to trucking and supporting the county's freight-dependent industries.

BUSINESS SUPPORT SERVICES

Scappoose businesses receive strong support from CET and SBDC, offering advising, permitting help, capital access, and planning assistance for companies pursuing growth and expansion.

SOURCE: Summarized from ECOnorthwest's work on the City of Scappoose's 2023 Economic Opportunities Analysis

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FACTORS DRIVING INDUSTRIAL DEMAND | COMPETITIVE ADVANTAGES IN SCAPPOOSE

LABOR MARKET

Scappoose has a young, highly engaged workforce, strong regional talent access, and PCC-OMIC pathways that equip residents with skills for modern manufacturing careers.

BUILDABLE INDUSTRIAL LAND

Scappoose has nearly 500 acres of buildable industrial land, including multiple large sites near the airport, providing significant capacity for future manufacturing and employment growth.

BUSINESS-FRIENDLY ENVIRONMENT

Scappoose has comparatively lower property tax rates and has an urban renewal district with funds dedicated to supporting business expansion.

TOURISM AND OUTDOOR RECREATION

Scappoose offers strong tourism and lifestyle appeal with easy access to parks, trails, cycling, and boating—enhancing its attractiveness to residents, visitors, and employers.

QUALITY OF LIFE

Scappoose offers small-town quality of life, strong schools, and community safety, along with events and festivals that enhance livability while maintaining easy access to Portland and outdoor recreation.

REGIONAL PARTNERSHIPS AND COOPERATION

Scappoose benefits from strong regional partnerships—CET, GPI, and statewide organizations—working together to attract investment, support employers, and advance economic vitality and family-wage job growth.

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SOURCE: Summarized from ECONorthwest's work on the City of Scappoose's 2023 Economic Opportunities Analysis



KEY MARKET STUDY TAKEAWAYS

MODERATE TO LOW ORGANIC GROWTH: While population growth has been steady, it has declined over the last 5 years. Low organic growth will mean a high dependency on in-migration tied to job opportunities and housing availability. Need to serve younger demographic and young family formation.

MF ABSORPTION IS LIMITED: While MF units are quickly absorbed, the overall population growth limits the potential for development of MF at larger scale. New housing likely will be necessary to support ongoing economic expansion and build out of existing industrial space

RETAIL AND SERVICES IN DEMAND: Low Vacancy, growing rents, and quick absorption demonstrate an underserved market with demand for retail and services. This demand will increase and is necessary to provide for desired job growth. Maturation of the market will require capturing existing leakage and creating new retail and service jobs.

HOSPITALITY TIED TO BUSINESS GROWTH: Without a significant leisure travel draw, hospitality will be tied to business and economic growth. There is likely support for new product in proximity to OMIC.

INDUSTRIAL OPPORTUNITY: Currently Scappoose has existing vacancies for industrial and manufacturing, as well as over 500 acres of developable land. This inventory is dependent on attracting businesses by leveraging competitive advantages. Growth will require the associated growth in housing options for employees and retail and services that are expected for quality of life.

PARKER QUARRY ZONING

A UNIQUE SITE

This site presents a unique opportunity for forward planning around city growth and expansion of services



RARE SITE:

- Large Parcel with relatively straight forward access to utilities and transportation
- Adjacency to City boundary for easy annexation
- Proximity to major highway/travel corridor and centrally located
- Blank Slate opportunity

LEVERAGE UNIQUE SITE

Maximize Flexibility

- A site of this scale is a 10-15 year min build out
- Community needs will change over that time
- Very high likelihood that this site will provide options for services and housing to support large industrial land opportunity next to airport

Market Demand Drives Diversity

- High need for new and modern commercial offerings. Large sites create best chance of proper mix
- ***Retail is the most underserved sector***
- Housing options to drive employment will be critical to economic growth
- Industrial use opportunities already exist elsewhere

Housing Demand

- New housing is in demand
- Immediate demand (5 year) for around 200 units
- Economic and Job growth could impact need for housing
- Expectation for market demand to increases across region

LEVERAGE UNIQUE SITE



Rezone to EC

- Allows greatest flexibility
- Site is well positioned to serve diverse uses
- Other sites already available for large scale industrial
- “Tells the story” of how economic growth is to be supported with services and housing
- High quality opportunity for Retail and Hospitality

DEVELOPMENT PROCESS IS COLLABORATIVE

| Stages | Timing |
|--|--------------|
| CREATE VISION: Market demand analysis, schematic master plan, financial modeling, code review | Months 1-6 |
| SHAPE PROJECT IDENTITY: Collaboration with City staff and other stakeholders, community engagement, master planning process | Months 7-14 |
| CITY FORMAL REVIEW: Annexation and Developer Agreement | Months 15-19 |
| IMPLEMENTATION: Formalize annexation and Developers Agreements, First phase engineering design and approval | Months 20-29 |
| CONSTRUCTION: Build out and record first phase | TBD |

Clear and collaborative process with City review along the way including specific required approvals

WHY HERE, WHY NOW?

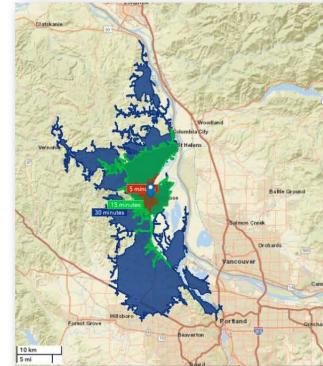
Phase of Market Cycle



- 2016-2019 core focus
- 2020-2021 covid
- 2022-2025 late cycle
- 2026 – early cycle

Convenient access to Portland's industrial and employment centers along the Columbia River corridor.

Well positioned relative to other major job hubs: Hillsboro—home to Oregon's Silicon Forest employers—is roughly a 30-minute drive.



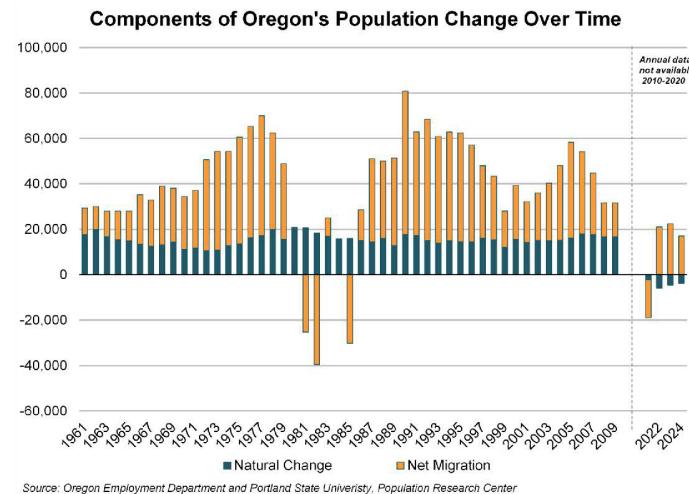
ECONorthwest

Growing regional relevance:
Housing Need, Core Struggles

FUTURE GROWTH

MIGRATION IS CRITICAL

- Attractive communities will win
- Need to provide housing options and services
- Available jobs and quality of life
- Remote work isn't going away
- Flexible growth required
- Oregon birth rate is 1.4 (2.1 is replacement)



Council, Planning Commission, Staff and the Consultants discussed this in more detail.

Community Development Director Laurie Oliver Joseph explained ultimately we are trying to be able to wrap up our part of the analysis so that we know is this part of our process, is it one parcel, is it both parcels and then the UGB question she still wants to get some clear direction on that. She explained there are a couple of things she just wanted to bring up. There were some questions asked during the last meeting and she wanted to follow up on those. First of all, just a broad overview if we were to not include all of Mr. Parker's property, it means that we would expand the UGB further from existing city utilities. We would have to go outside of what we're looking at now in addition to the urban reserves. Rezoning this parcel that's already in the existing UGB means we're using the utilities that are here that are already close by and surrounding this property. Limitation on the types of housing, she believes this was a question that Council President Miller asked about. Our attorney did not recommend doing this for just one parcel or owner. The expanded commercial zone only allows a couple types of housing, as is. It allows multifamily mixed-use, which is of course commercial on the first-floor housing and above and then live work townhomes. Live work townhomes are where you have essentially a townhouse on its own legal lot and business is limited to the ground floor and may not exceed 50% of the floor area. You would have a small business, basically storefront on the main floor if some living and then living above. That type of housing would be more expensive than say, multifamily because it's on its own legal lot, but it would provide some flexibility. As Greg

mentioned, they'd like to have some options. Of course, we would not be able to restrict what we refer to as capital A, Affordable housing income restricted housing the State says that would be permitted regardless. Again, as we heard from Greg and incidentally, in our housing capacity analysis findings, we need housing in a variety of price points. For more flexibility this parcel has the better. She explained there was a question about limitation on total number of housing units that would be conceptually possible. Our Attorney would want to be involved in the drafting of any type of agreement. Our Attorney suggested a restrictive covenant route rather than a developer's agreement or annexation agreement and that's just so that there's no question that the City is regulating development on the property and again, we couldn't subvert any state requirements for affordable housing. She explained our attorney would want the agreement to be able to be terminated upon mutual written consent in case either party changes their mind. We do not have any kind of a draft of that at this time. We just saw the results of the market study on December 9th. We just wanted to bring this to you. Consider what we've heard tonight in terms of the desire for flexibility, for it to really develop out in the best way. She wanted to follow up on one other item. We discussed the Urban Growth Boundaries subarea 7 and 8. We did have some cost to the storm improvements that we mentioned. She stated Councilor Holmes asked about that and we could follow up on that when we reopen the work session. One thing she did want to mention was there was an issue of subarea 7 owners not wanting to be a part of the UGB expansion. She explained we kind of talked through that and explained it made sense to include them from the standpoint of master planning in system development charges allotment. She has since heard from a person who is identified for Urban Reserve scenario 7 and they have a strong interest of being either UGB or Urban Reserves, but also they mentioned two of the owners to the north of them, the two largest parcels in subarea 8 do not want to be in the UGB. So now we have basically area 7 and a large parcel of area 8 not interested, but again, these are parcels of abutting City limits, abutting City utilities so we'd still want them in to have that natural growth of the City. If they don't develop we would have other parcels in the City developed first and at the next opportunity that we need more land for housing we could go past those areas, into the Urban Reserves. They don't have to develop, but it does set us up to be able to collect SDC's.

Mayor Backus explained the work session will be continued in the City Council meeting after public comments.

Adjournment

Mayor Backus adjourned the work session at 6:59pm.

Mayor Joseph A. Backus

Attest:

City Recorder/HR Susan M. Reeves, MMC



SCAPPOOSE

Oregon

Monday, December 15, 2025

City Council Meeting, Regular Meeting 7:00pm

Council Chambers, 33568 East Columbia Avenue, Scappoose, Oregon 97056

Disclaimer: These minutes are intended to summarize the conversations that took place in this meeting rather than provide a full transcript. Anyone wishing to view the full conversation can find a recording of this meeting on YouTube at www.youtube.com/watch?v=VNR8mdmwrEg.

Call to Order

Mayor Backus called the December 15, 2025 City Council meeting to order at 7:00pm.

Pledge of Allegiance

Roll Call

| | | | |
|------------------|------------------------|-------------------|--|
| Joseph A. Backus | Mayor | Benjamin Burgener | City Manager |
| Jeannet Santiago | Councilor | Chris Fluellen | Police Chief |
| Kim Holmes | Councilor | Susan M. Reeves | City Recorder/HR |
| Joel Haugen | Councilor | Dave Sukau | Public Works Director |
| Marty Marquis | Councilor | Kevin Turner | Treatment Plant Superintendent |
| John E. Riutta | Councilor | N.J. Johnson | Assistant to City Manager/City Planner |
| Tyler Ferreira | Student Representative | | |

Also present: Planning Commission Chair Scott Jensen; Planning Commission Vice Chair Harlow Vernwald; Planning Commissioner Rita Bernhard; Planning Commissioner Sara Jones-Graham; Planning Commissioner Harry Bludworth; and Planning Commissioner Peter Williamson.

Remote: Council President Tyler Miller; Legal Counsel Ashleigh Dougill (left at 7:22pm; rejoined at 8:48pm, and left at 9:39pm)

Mayor Backus explained he is looking to, right after public comment, reopen the work session to continue the discussion

Approval of the Agenda

Councilor Santiago moved, and Councilor Holmes seconded the motion to approve the agenda with the continuation of the 50 Year Plan work session. Motion Passed (7-0). Mayor Backus, aye; Councilor President Miller, aye; Councilor Santiago, aye; Councilor Holmes, aye; Councilor Haugen, aye; Councilor Marquis, aye; and Councilor Riutta, aye.

Public Comment ~ Items not on the agenda

Brian Rosenthal, Scappoose, explained the presentation was nice. He thinks that there's a lot of good that could happen out there. He does want to point out a couple things. As far as this idea of collaboration the opportunity for that is now. Once they go into the City with a fixed zoning, that opportunity is gone because they're allowed to do whatever the zoning says. As far as he understands it, the zoning says the zoning is, meaning you bring them in, you give them a certain zoning it's over. The collaboration period other than what they choose to do voluntarily, which they may choose to do it, is not open for discussion. He really would strongly recommend that you consider a restrictive covenant limiting the number of housing units. It doesn't have to be punitive; it could be 400 and you can always increase it later. Definitely would want the restrictive covenant setup, where without the cities agreeing to, it can't be rescinded. It was a little unclear earlier about how that dynamic would work, but you would want the City to definitely not to be in a situation where they end up in mitigation if they don't agree to rescind. To be able to change it seems fine, but the more restrictive covenant you have, the more power you have to help guide the development. It's just something that is positive for the City. Besides that, the overall package looks fine. He does think that some of the numbers are off a little as far as commercial development. He can tell you that he has a 1,500 square foot space that's been available for a year and a half, and his price point is \$1.40 a square foot no triple net, no cams, which is below the price point they are talking about here. He explained they were talking about a different type of development than he does, and that could open some opportunities. He won't challenge that, because he thinks they're right. Again, if you look at price per square foot, it is going to be housing. So, if you don't have the restricted covenant and cap it what you're going to get is housing. It's not because anybody's bad or mean or anything else, it's just because that's what the numbers say.

Mayor Backus thanked Brian.

David Sideras, Scappoose, explained he wanted to comment a little bit on the plan, just in general. He had some knowledge of condos and development. He explained when you have a large parcel and you do a humongous condo complex, he believes you need to sell 51% or 50% of those units under contract before the first one and be financed, so they may have the funds to bankroll that 50% but it's tough and the larger the project gets, the more unwieldy it gets you. The best thing in terms of value is subdividing the parcel into smaller pieces and allowing things to happen organically. He looked at this and thought well, why aren't we doing that with the rest of the City? Are we not wanting to be organic elsewhere and just be organic there? He thinks they have had that conversation in the Economic Development Committee (EDC) where encouraging changes in zoning that would allow for more housing to happen or more flexibility for businesses because they've identified that what they said in EDC is that things aren't happening historically. We are kind of stale or we're slowing down, and we need to open things up a little bit to beat momentum. That was the takeaway that he got. He has been talking to members of the community about the Urban Growth Boundary expansion, and there are people that he knows that were going to be included, who either are talking about doing deed restrictions on their land so that they will not participate in that and you can undo it eventually, but it's very difficult and if someone does a deed restriction on their land to prevent

development the heirs are not going to be able to circumvent that, or once it divides into three or four, you have to collect more signatures instead of just one. He knows two people like that near him and so he would encourage you to look further than the properties that you've identified, assuming that you're going to have a certain number of rejections and it's not just in Portland. In Portland, they kind of say you don't have to like it, we'll deal with the next owner of your property. That doesn't work with deed restrictions and with covenants. So, he is glad that that was brought up. He explained personally he doesn't think that the covenant route, it's with regard to Parker's pit, anything that you do is going to have an unintended consequence. So, he would encourage you to have your zoning be in order so that the developers could do what they need to do to make money. What you don't want to do is get the property into the City and then have it sit and do nothing because you've painted yourself into a corner with your covenants.

Mayor Backus thanked David.

Alicia Heiges, Scappoose, read her formal written statement (which was submitted, scanned in and included below) into the record.

Scappoose City Council – Public Comment Submission

Submitted by: Alicia Heiges
Date: December 15th, 2025
To: Scappoose City Council
Re: Public Comment Regarding Surveillance, Governance, and Accountability

Good evening. My name is Alicia Heiges, and I am a resident of Scappoose.

I want to begin by addressing Chief Fluellen's comments about pursuing a grant for Flock Safety cameras.

License plate reader systems are often presented as tools that can help investigations. However, based on my family's direct experiences with the Scappoose Police Department, I have serious concerns about how such a system would be used in this community.

My family has provided SPD with recordings and documentation specifically intended to assist investigations. That information was not properly documented, was not meaningfully acted upon, and was not used to protect us. That history matters. When accountability and documentation already fail, expanding surveillance does not build trust, it undermines it.

Given that history, I am concerned these cameras could be used in a retaliatory or targeted manner against residents. Surveillance tools do not exist in a vacuum; they reflect the practices, priorities, and culture of the agency using them.

I also want to ask some basic governance questions.

Has this Council approved Chief Fluellen to move forward with Flock Safety cameras if a grant is obtained? If not, what is the approval process?

Was there any plan to seek community input before installing these cameras, or would residents simply be notified after they are already operational? Decisions involving expanded surveillance should include public discussion, transparency, and clear policy guardrails.

These concerns are not hypothetical. Multiple Oregon cities, including Eugene, Springfield, and Woodburn, have paused or canceled their use of Flock Safety cameras due to community concerns and legal scrutiny. In Eugene, the police department identified vulnerabilities and limitations that raised concerns about operational needs, data security, and community expectations, and ultimately decided to discontinue the contract.

That decision reflects broader issues with these systems, including data security vulnerabilities, access and data-sharing concerns, and the potential for misuse or manipulation. Once data is collected, questions of oversight, retention, and secondary use become serious civil liberty issues.

I also want to address the context in which statements about the FBI were made. I have been one of the primary speakers at these meetings, often with very few others providing public comment.

During the last meeting, Chief Fluellen stated that he had been reported to the FBI by a public commenter. That statement was made without providing documentation or clarification and reasonably appeared to single me out as a frequent and visible public commenter.

Even if intent is disputed, the effect of that statement is to associate a private citizen's protected speech with federal law enforcement. That framing was unnecessary, inappropriate, and reasonably perceived as retaliatory. It discourages public participation and discourages residents from filing complaints or reports out of fear that they may be identified or targeted.

If that statement was intended to reference me, then I am requesting clarification and documentation. If it was not, then that should be clearly stated on the record.

That brings me to my next point.

Being contacted by the FBI does not mean a public official has been cleared or vindicated. Federal contact simply means a complaint was serious enough to warrant review or inquiry. The FBI does not issue verbal exonerations or informal findings.

Without written confirmation, a case number, or a public finding, no conclusion can be drawn about the status or outcome of any allegation. Referencing FBI contact without documentation can mislead the public and discourage accountability.

I also want to address recent comments from council members about the importance of upholding the Constitution.

I find it deeply ironic that these principles were not meaningfully defended when my family was experiencing retaliation, unequal treatment, and failures of protection by city officials and SPD. Upholding the Constitution is not situational, and it is not optional.

Before this city expands surveillance, pursues new enforcement tools, or invokes constitutional values, it must first address its existing failures of accountability, documentation, and equal protection. Trust is not built through cameras or grants. It is built through consistent, lawful, and transparent conduct.

Thank you.

(end of Alicia Heiges formal written statement)

Mayor Backus thanked Alicia Heiges. He stated he has many of the same questions that Alicia does regarding flock. If we do continue down that road, he would hope that the community members' education and understanding would be a top priority. He explained that as we learn more, we'll definitely let you all know more as well.

Kathryn Hugill, Scappoose Community Club, explained she was before Council about 10 months ago and spoke with Dave Sukau concerning the upgrade of the holiday decorations on Columbia Avenue and 2nd Street. She wanted to specifically come in and thank Council for any action they took on this matter; they are beautiful. She believes it is definitely an upgrade to the City. She has had a lot of communication with different people in the park, many of our visitors, they greatly appreciate those streets being lit up the way they are, and they are very beautiful. She stated the only comment she is hearing is that they would like to see them on Highway 30, but money is a factor. Maybe it will happen all in good time. She just wanted to make that point, that the public definitely appreciates that and would like to see more of it. In regard to displays our Scappoose Community Club and Connection has greatly increased the displays in the park. You probably have noticed the displays as the park is a whole lot brighter. With our community partners it has been a very successful month. Again, we are seeing a lot more traffic in the park, regardless of the time. She explained the event on November 30 was a phenomenal success. They did the event in partnership with Whoville. She stated Mayor Backus was wonderful and she appreciates him speaking to the crowd and having such a resounding effect on the community. She explained they have had a lot of community partners assisting them and they have already bought two new displays for next year. As they continue to grow she is really optimistic about what we can do with this town and that would include everything from the North welcome island to the South welcome island. She explained she greatly appreciates the Council's support. She stated along with the community partners they have had phenomenal assistance from the Parks Department and Dave Sukau. They would not do any of this without all of their help. She just wanted to let Council know that they greatly appreciate that. She stated it takes a partnership. She is looking forward to doing some really neat things in the future.

Mayor Backus thanked Kathryn.

Mayor Backus opened the 50 Year Plan work session. He asked about the steps, do we rezone it first and then it gets annexed at a later time?

Community Development Director Laurie Oliver Joseph replied that for this part of the process, the only thing the City would potentially be doing is redesignating it from industrial to commercial on the comprehensive plan map. It doesn't change the zoning, and it does not change the fact that it's still in the County. At a later date, when the landowner decides, they would apply for annexation and as part of that they would need to develop their master plan. For our purpose we're looking at the potential future land use and to make sure that the water, the sewer, the storm on site can support the proposed uses. That is a hearing for annexation that is required to go before the Planning Commission, and they would make a recommendation to City Council. What Romano Group is proposing is that they would take some additional steps prior to applying for annexation to get feedback from Council and the

community in terms of what they would like to see there, because obviously at the end of the day they want their investment to go well and they want to be serving the community in a way that's going to be successful in the future and attracting again industrial users. She explained when we get requests from really large industrial users, they do ask about livability, and they do ask about how much vacant land is available for housing. The rezone happens at the time of annexation.

Community Development Director Laurie Oliver Joseph explained what she needs in order to move forward and not have increased costs, hopefully to the project, is to keep it moving. She asked if there is a sense of interest in redesignating as part of our process; one of the parcels; both of the parcels; and in what capacity. Should we just move forward and wait until annexation is proposed to think about a restrictive covenant?

Mayor Backus prefers the flexibility of doing it all at once. He explained if we can at the time of annexation maybe have a discussion, maybe they're working with us along the way before that and we'll get a sense of what their plans are. If we see that a restricted covenant might be kind of what we're looking at, then he would entertain it because they could have all these plans in a master plan and nothing get developed and years down the road it could be completely changed. He is fine with moving forward.

Councilor Santiago wants to make sure we are flexible, be able to build, but Romanos is an investment company, which they, but we want to make sure that they work with us as a community because for us that is what's important. She just wants to make sure that we are able to work with the development as they go along.

Councilor Holmes explained she does think this is a very important decision for the long-term growth and development of the community, so she is in favor of moving with a bit more caution. She would say if the property owner and the developer were in favor of exploring a restrictive covenant then she would be more comfortable with rezoning the entire parcel rather than taking it stepwise with just a partial portion of the rezoning.

Councilor Haugen explained he is on the maximum flexibility side of the ledger on this. His inclination would be to look at one parcel first, but first he wants to see the conceptual plan. If we are working in the blind we don't know what we are approving. He asked would that fit on one parcel.

Community Development Director Laurie Oliver Joseph explained just to be clear, if we were to move forward with just the redesignation not the annexation or rezone as part of the 50-year plan, we would not see anything conceptual prior to heading to adoption to change this to commercial on the comp plan map.

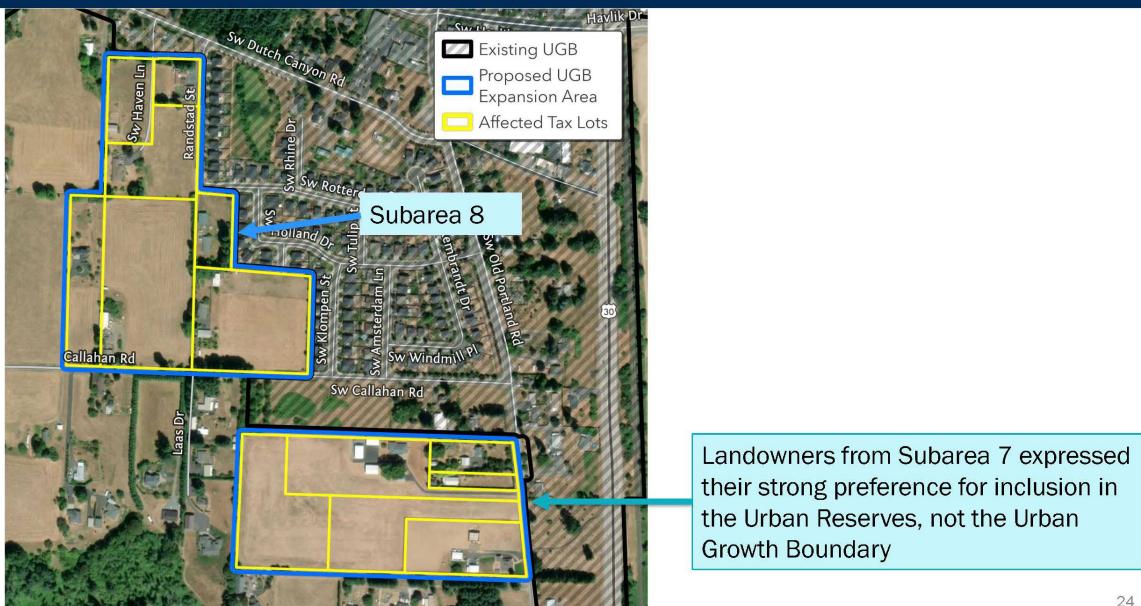
Councilor Marquis explained he is in favor of redesignation.

Councilor Riutta explained he thinks all that could be said has been said in various ways and he would simply fall back on the old adage in for a penny, in for a pound so he would do exactly what Mayor Backus said and do the whole lot.

Council President Miller feels similar to what Councilor Holmes stated. His biggest concern is the number of apartments that are going to go in and be a drain on City resources. He also shares some of the same opinions that Councilor Haugen stated.

Community Development Director Laurie Oliver Joseph explained in terms of UGB areas, that's the last item that she needs to get clear on. She shared her screen (as shown below).

Comment from Landowners in Subarea 7



24

Community Development Director Laurie Oliver Joseph explained we have these two areas ~ subarea 7 & 8, which were selected based on the way that we have to look at and categorize and consider areas for UGB expansion. She explained they have to write findings to state why the areas that we're considering meet or don't meet those criteria. These are the areas that meet the criteria best, mostly because of the provision of public utilities. She explained let's just say area 7 & 8 don't get developed, as was mentioned, and if that's the case, then they certainly could do that, provided it's not against anything in State Statute, like it's not referencing certain housing types or things like that. There are restrictions around recording restrictive covenants that call out types of housing, specifically. If they were to just do a blanket, sort of restriction on any development, they may be able to do that, it would greatly reduce their property value, likely, but that would be their choice. Our point is, and she thinks you've heard her say this, is that essentially if these properties don't develop, we still need to be able to have these areas then around it would be in the Urban Reserve which become first priority for the next UGB expansion. If these properties identified for the UGB don't develop, that's fine, they'll retain their current County zoning and the next UGB would push past them, and utilities would have to get extended past them to have the next development move forward. That is their choice, but in terms of the natural growth of the form of the City, these by virtue of the fact where

they're located next to existing development still need to move forward, still needs to happen. Again, it just might mean if they don't develop it, we'll have a UGB expansion sooner, as other parcels in the City that are generally vacant and have developable abilities would go first. Mayor Backus explained he is okay with continuing with those, understanding their feelings and moving forward.

Community Development Director Laurie Oliver Joseph stated knowing that we are not going to force development. She explained the City is able to move forward with this plan even with the opposition, because as long as we are writing the findings that are proper they could appeal but they would have to show how we are not meeting the criteria to include these areas essentially. She explained there are going to be some challenges in serving these areas with utilities because of some storm improvements that are needed for both of these areas to move forward. However, these are still the best situated UGB lands in the City. There and area 4 that was not contested.

Councilor Haugen stated the umbrella thought here is to maintain livability and part of that is maintaining some balance of green space. We don't need to fill up every property with houses.

Planning Commissioner Harry Bludworth asked in the demographic studies, considering the fact that millennials have a low participation here and the cost per square foot of monthly rate on housing and Scappoose is somewhat higher than St. Helen's and the County, was that taken into consideration when thinking about absorption rate of new housing within the City? Since there's already a low participation rate of millennials within the Community and the housing is slightly higher, in new housing development, was that considered in the absorption rate in being able to build those houses?

Greg McGreeey, Romano VP of Development, replied that's a great question. He explained all of the units you're building now are getting absorbed very fast. On a national level, the trend is toward what they're calling the flight to quality, and particularly amongst young people, where they're looking for higher quality, higher amenities, spaces, which are slightly more expensive and so the rent doesn't seem to be necessarily the sole driving factor, it's the availability of newer amenity products that has much of the livability things that young people are looking for.

Councilor Holmes stated this change would have no tax impact or public easement requirements on them?

Community Development Director Laurie Oliver Joseph replied that is correct. She thanked everyone and stated she thinks she has what she needs to move forward. She will continue to explore the restrictive covenant concept and may touch base with Council again before they head to adoption on that.

Mayor Backus and Council thanked Laurie.

Consent Agenda November 17, 2025 City Council work session minutes; November 17, 2025 City Council meeting minutes; Appointment of Willow Ryan to the Planning Commission as Alternate; and Reappointment of Sandie Wiggs to the Budget Committee

Councilor Santiago moved, and Councilor Riutta seconded the motion to approve the Consent Agenda November 17, 2025 City Council work session minutes; November 17, 2025 City Council meeting minutes; Appointment of Willow Ryan to the Planning Commission as Alternate; and Reappointment of Sandie Wiggs to the Budget Committee. Motion Passed (7-0). Mayor Backus, aye; Councilor President Miller, aye; Councilor Santiago, aye; Councilor Holmes, aye; Councilor Haugen, aye; Councilor Marquis, aye; and Councilor Riutta, aye.

New Business

Contract Amendment – Contract #2025-04

Public Works Director Dave Sukau explained in attendance this evening is Treatment Plant Superintendent Kevin Turner; GIS Consultants Robyn Cook; Kenny Jansen and Matt Thomas. He went over the staff report. Contract amendment authorization for additional well shaft and screen drilling services for the Miller Road Basalt Well project. The City of Scappoose had multiple projects funded from the State of Oregon's ARPA allocation -- the engineering and construction of the Miller Road Basalt Well is one of those projects, with \$1.95 million dedicated for the completion of the Basalt Well. In 2023, City Council authorized then-City Manager Rains to enter into a contract with Carpenter Drilling for \$949,780. While Carpenter made good headway initially, unforeseen circumstances, varying from equipment issues to geological formations not being as anticipated, caused delays to the project that went beyond the initial contract expiration date. In May of 2025, the City signed a new contract with Carpenter to continue construction of the well. Shortly after this work resumed, it was determined that a different drilling method was needed due to the depths the drilling was beginning to enter and the soil conditions that were present. At that time, Carpenter Drilling subcontracted with Yellow Jacket Drilling to utilize a different method that was better suited to the heaving soils that were present in the hole. Yellow Jacket Drilling was able to successfully reach the basalt layer at just over 800 feet in depth, but was unable to successfully get a casing to that depth to begin drilling through the rock formation due to the continued heaving soils that are present @ 600 foot in depth. A plan was then created to remove the upper casing that was successfully installed and replace it with a larger casing that would allow for larger drill bits to pass through the hole creating a larger tolerance for a casing to pass through. Yellow jacket and Carpenter have both attempted to remove the casing, but the upper 200 feet is stuck and now risks being damaged by continued efforts to remove it.

With the current scenario the City is facing, a decision of how to move forward needs to be made. Those choices are as follows:

- Complete an alluvial well with the current casing and stop construction of a basalt well.

- Drill an exploratory testing well through the existing casing and backfill to an alluvial well after basalt well feasibility data has been collected.
- Move over approximately 20 feet and drill a new basalt well with the information gathered about the formation factored into the design to allow for a more positive outcome. The approach would also include the development of the currently drilled well as an alluvial well which would ultimately give the City two functioning wells from two different water sources.

Approximate pricing for the scenarios listed above:

Option #1 - \$340,000

Option # 2 - \$970,000

Option # 3 - \$1,500,000

*Note that this pricing does not include costs for pumping and pumping infrastructure.

The City received \$1,950,000 in ARPA funds toward this project and has been awarded \$350,000 in a forgivable loan from Business Oregon from the Semiconductor Industrial Land Loan Program. Additional funds required to complete this project would be funded from the City's Water Department budget and potentially a loan. Staff is currently working with Carpenter Drilling and Schneider Drilling to explore other potential options and pricing for cost savings. With time being of essence due to ARPA timeline guidelines and contract terms, Staff is seeking an approval from Council to move forward as soon as possible with one of the identified options. Staff recommends an approval for pursuing Option #3 to complete two wells with an additional \$200,000 in contingency. We believe that well drilling costs will continue to rise and that delaying this work would lead to higher costs down the road.

Robyn Cook, GSI, went over the presentation.



GSI's Prior Work for Scappoose

- City of Scappoose Hydrogeologist of Record since 2022 (first project was in 2014)
- Construction of DC-2, DC-3, and CZ-1
- Rehabilitation efforts at Dutch Canyon and Miller Road Wellfields
- Provided water right support since 2015

Development of MR-4 Project

- Groundwater alternatives study (2015) identified basalt aquifer as a potential option.
- Depth to basalt unknown between southern tip of Sauvie Island and St. Helens – contour lines through Scappoose are estimates.
- Reviewed prior mapping of top of basalt surface

USGS/OWRD: Ground-Water Hydrology of the Willamette Basin, Oregon (Conlon et al., 2005) – shown on this slide

PSU: The spatial and temporal evolution of the Portland and Tualatin forearc basins, Oregon, USA (Scanlon et al., 2021)

USGS: Geologic Map of the Saint Helens Quadrangle, Columbia County, Oregon, and Cowlitz and Clark Counties, Washington (Evarts, 2004)

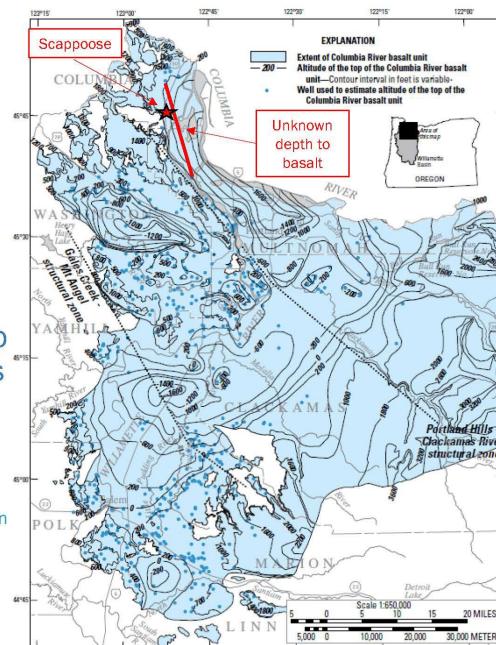


Figure 3. Extent and altitude of the top of the Columbia River basalt unit, Willamette Basin, Oregon (modified from Gannett and Caldwell, 1998).

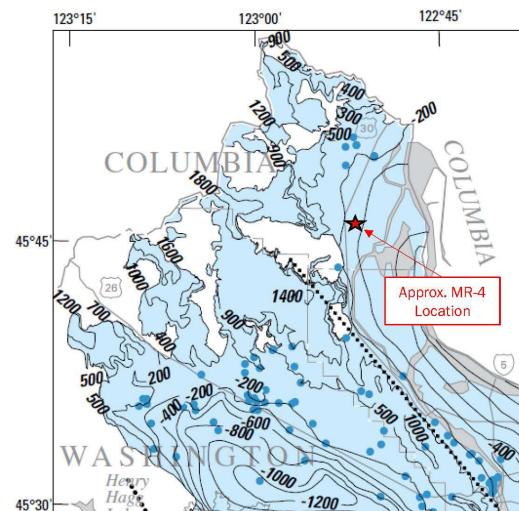
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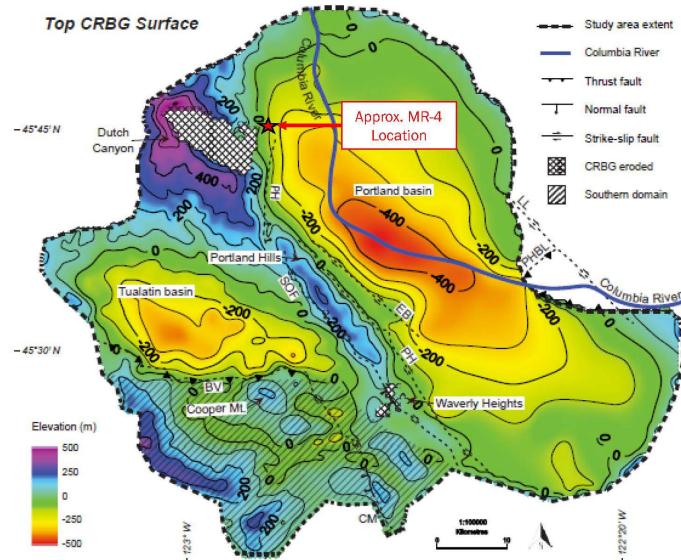
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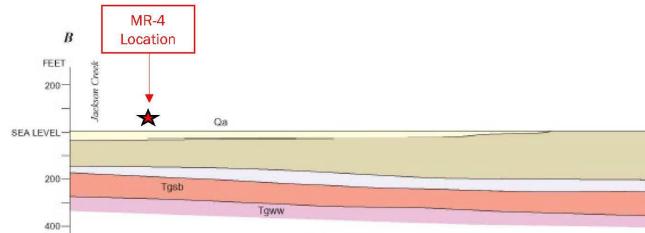
Development of MR-4 Project



From Scanlon et al., 2021 (PSU). Top of basalt contours displayed in meters.

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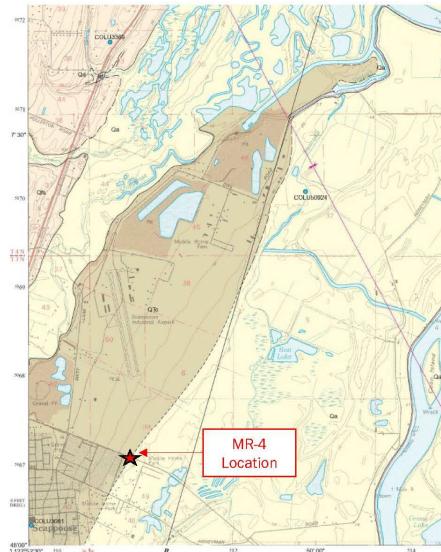
Development of MR-4 Project



- Basalt = red (Tgsb) and purple (Tgww)

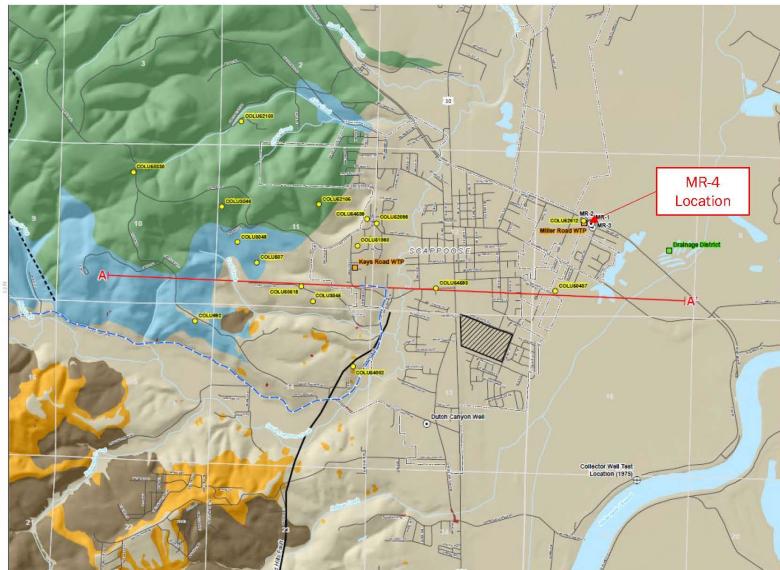
From Evarts, 2004 (USGS)

GSI Water Solutions, Inc.



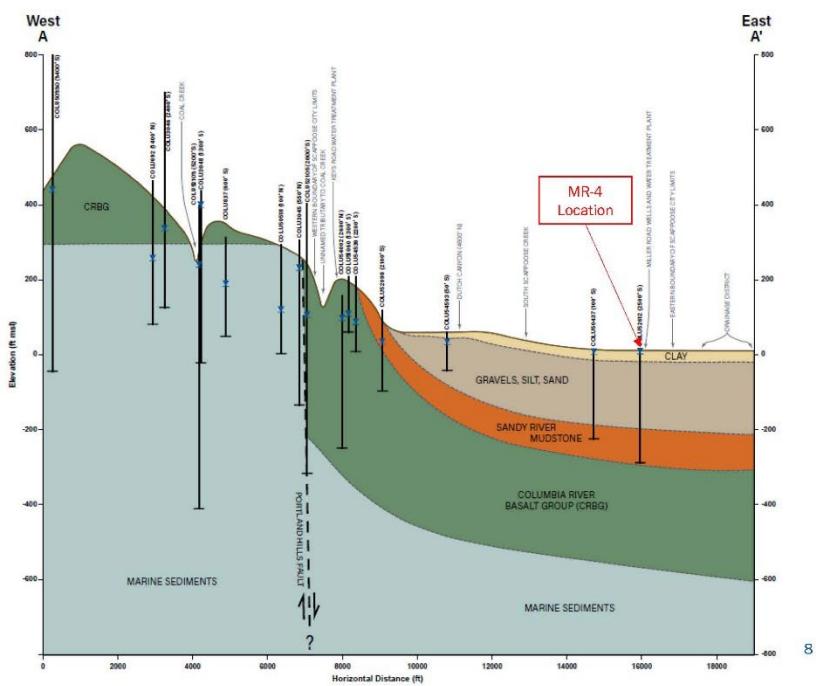
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Development of MR-4 Project

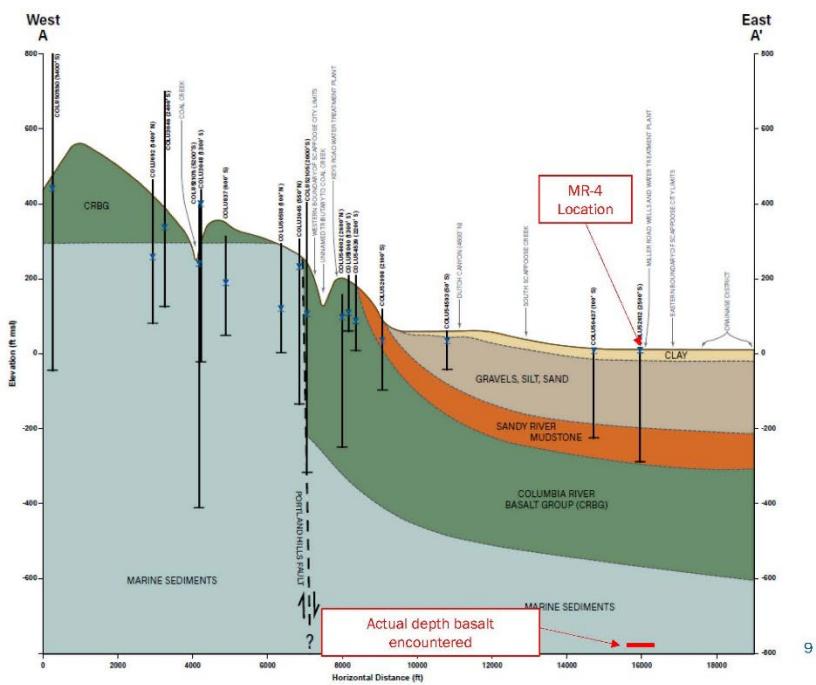


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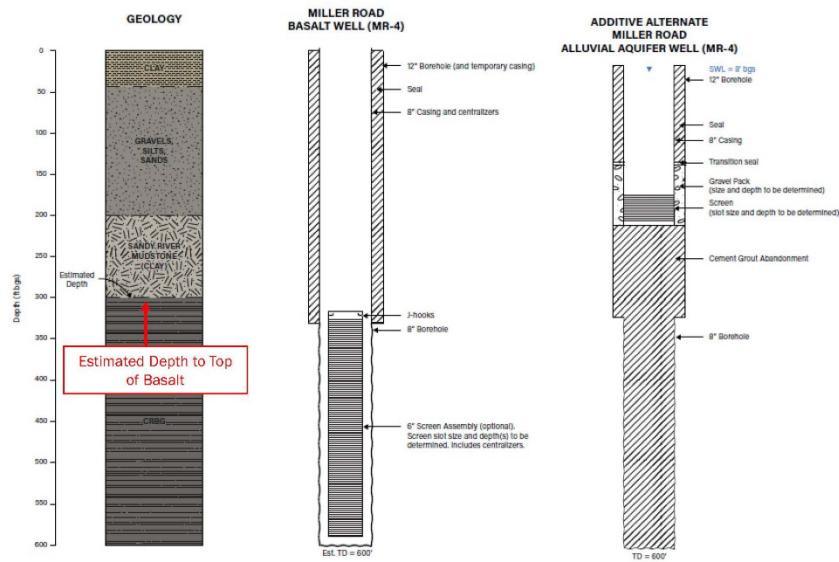
Development of MR-4 project



Development of MR-4 Project



MR-4 Conceptual Diagrams (included in bid documents)



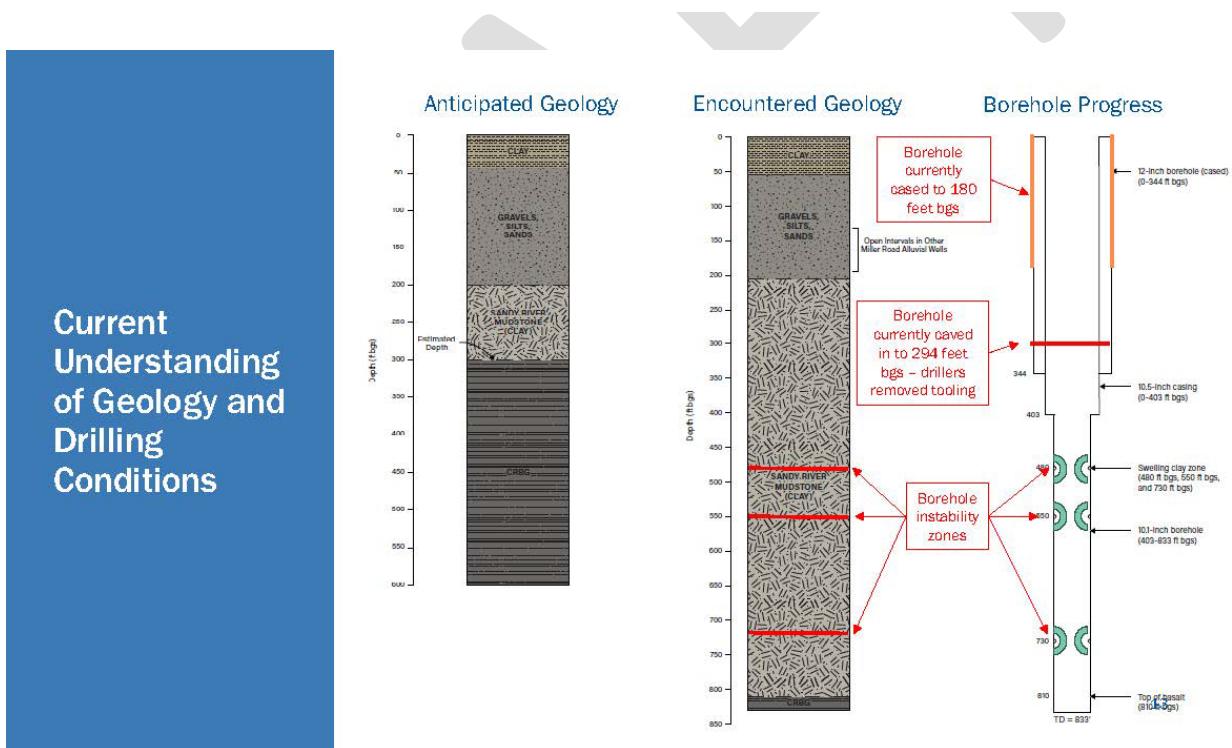
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Project Goals

- Drill, construct, and test a new basalt well
- If basalt aquifer not suitable, backfill and complete wellbore as alluvial well in same aquifer as existing Miller Road wells
 - Key factors in basalt aquifer suitability: (1) production rate; (2) water quality.
- Target production rate: 300 gpm

Project Narrative

- Bidding: Fall 2023
- Carpenter Drilling: Started February 2024
 - Carpenter Drilling only bidder (cable tool)
 - Slow drilling due to cable tool method, equipment issues
- Cobalt Drilling (rotary): July 2024-October 2024
- Carpenter Drilling resumes: October 2024-January 2025
- Yellow Jacket Drilling (rotary): June 2025-October 2025
- Schneider Water Services: January 2026?



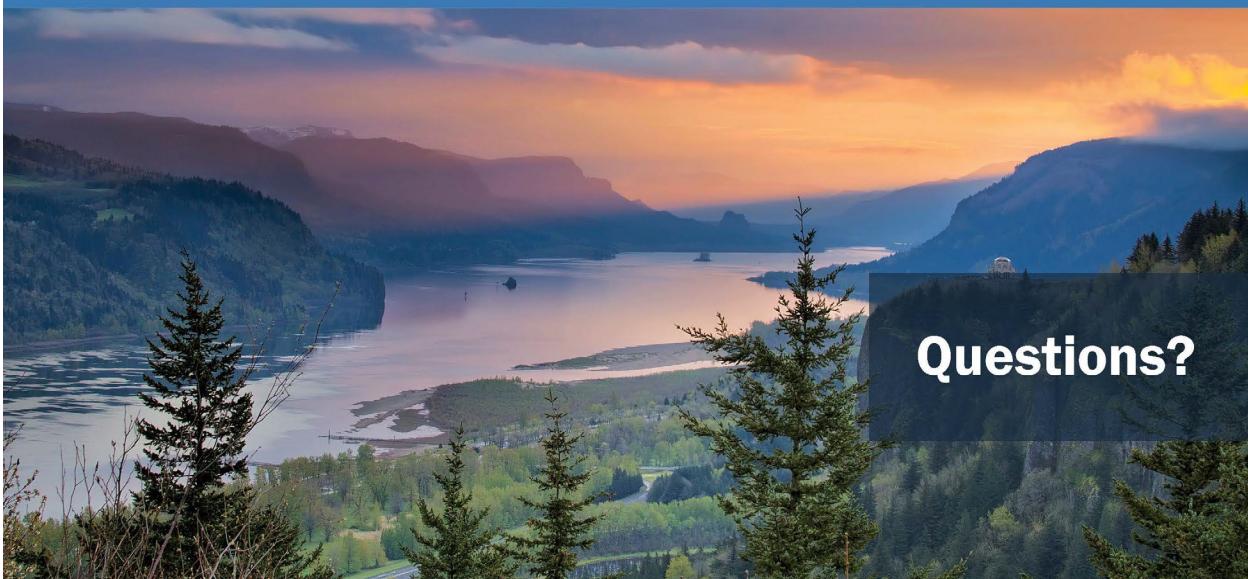
Next Steps

- Carpenter Drilling to subcontract Schneider Water Services
- Option #1: Drill new borehole with larger starting diameter. Gives drillers more options to deal with heaving/swelling material.
 - If basalt aquifer suitable for supply purposes, construct basalt well in new borehole. Option to construct alluvial well in current borehole or backfill to surface.
 - If basalt aquifer not suitable, backfill and construct alluvial well in new borehole. Backfill current borehole to surface.
- Option #2: Re-drill existing borehole to explore into basalt with small borehole. Enough to test water, not enough to complete as municipal supply well.
 - Goal: get data to inform design and drilling approach for future well.
 - Regardless of basalt aquifer suitability, backfill and construct alluvial well in new borehole; backfill current borehole to surface.
 - Option exists to attempt to complete a basalt well if basalt aquifer suitable. Currently waiting on approach and costs from drilling team.



Next Steps – Costs and Possible Scenarios

| Scenario | Details | Outcome for City | Total Remaining Cost |
|-------------|---|---|----------------------|
| Scenario #1 | Construct alluvial well in current borehole | (1) New alluvial well in Miller Road wellfield | \$340,000 |
| Scenario #2 | Re-drill existing borehole, explore basalt with small borehole, backfill, construct alluvial well | (1) Data on production capability and quality of basalt aquifer for future planning (2) New alluvial well in Miller Road wellfield | \$970,000 |
| Scenario #3 | Drill new borehole, construct basalt well in new borehole, construct alluvial well in existing borehole | (1) New basalt well (2) New alluvial well in Miller Road wellfield | \$1,500,000 |



Council, Staff and the Consultants discussed the basalt well into more detail.

Councilor Holmes moved and Councilor Riutta seconded the motion that Council authorize City Manager Burgener to contract with Carpenter Drilling, LLC, for the completion of an alluvial well and a new basalt well in a not to exceed limit of \$1,700,000.

Public Works Director Dave Sukau explained what staff is doing is getting a pre-authorization for some contingency because if something does go a little bit sideways, it gives City Manager Burgener some buffer there to sign a change order and not pause the whole thing. He explained what we were facing before was \$9,500 a day in standby time. He stated we are trying to put in some contingency, that is not the amount we are going to sign a contract for.

Councilor Haugen explained he would condition this more on getting it more paid for from some kind of State fund because he thinks it is highly unlikely that they are going to hit water based on his experience. He would go for option 1 unless we can get some more funding out of option 3 because he thinks that it's almost a pipe dream getting into water at this point.

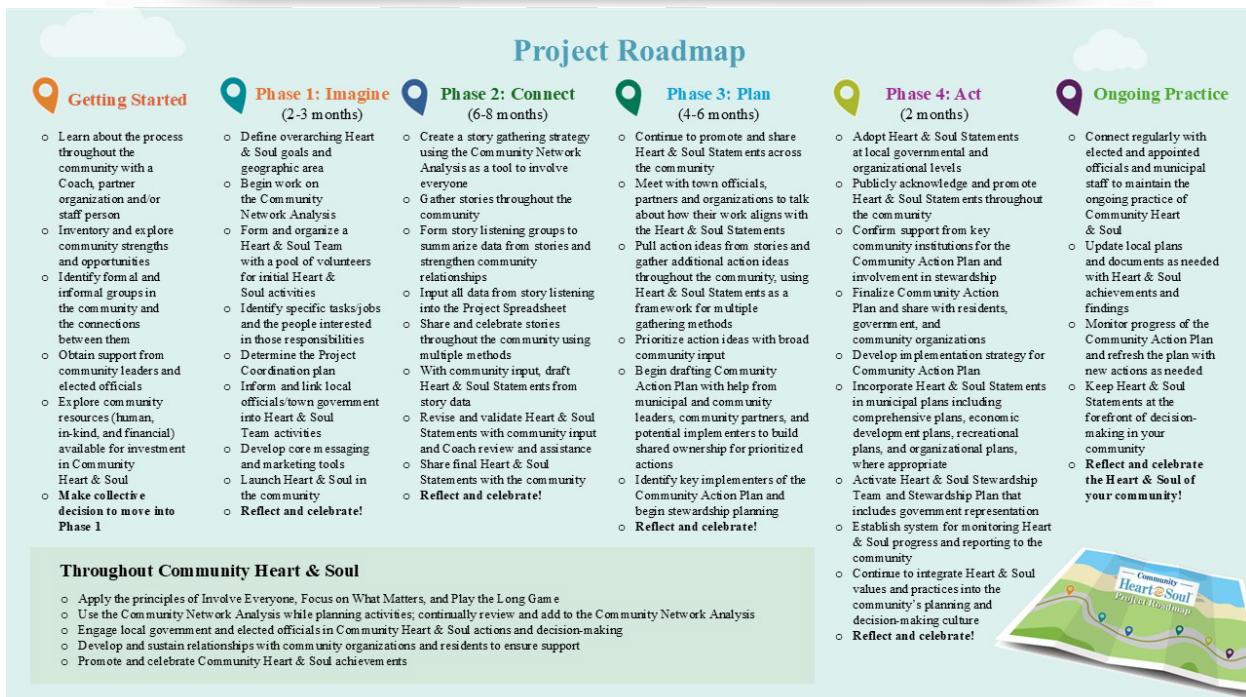
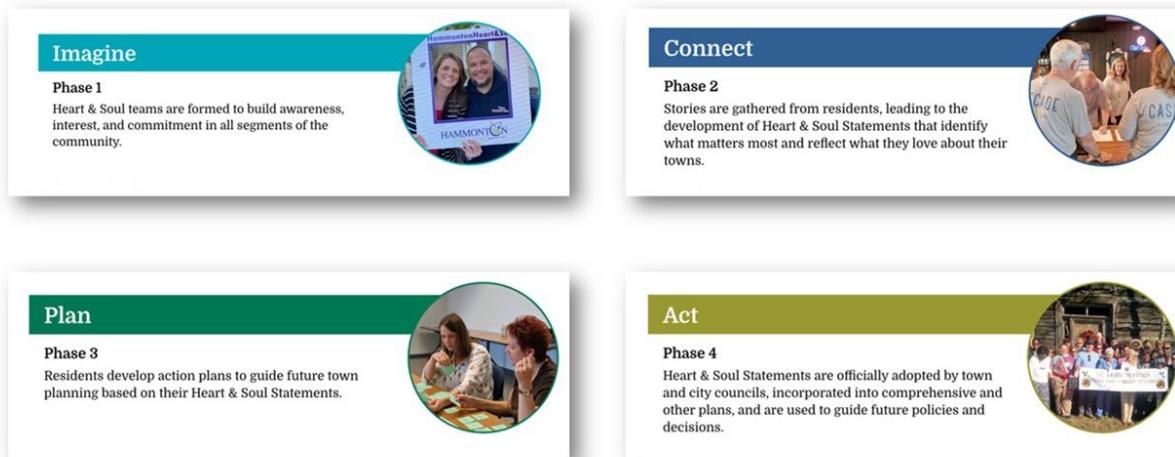
Mayor Backus explained he would like to pursue grants, any funding, to help.

Motion Passed (6-1). Mayor Backus, aye; Councilor President Miller, aye; Councilor Santiago, aye; Councilor Holmes, aye; Councilor Marquis, aye; and Councilor Riutta, aye; and Councilor Haugen, nay.

Community Heart & Soul Presentation

Certified Project Coach Bill Flood, Community Heart & Soul showed a video and then went over the presentation.

Four Phases



Getting Started

- Learn about the process throughout the community with a Coach, partner organization and/or staff person
- Inventory and explore community strengths and opportunities
- Identify formal and informal groups in the community and the connections between them
- Obtain support from community leaders and elected officials
- Explore community resources (human, in-kind, and financial) available for investment in Community Heart & Soul
- **Make collective decision to move into Phase 1**



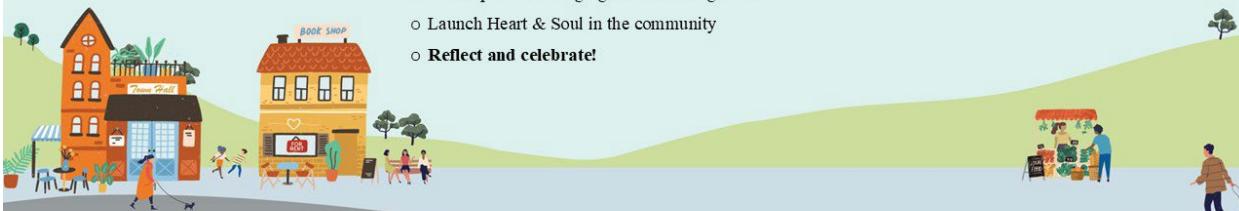
Throughout Community Heart & Soul

- Apply the principles of Involve Everyone, Focus on What Matters, and Play the Long Game
- Use the Community Network Analysis while planning activities; continually review and add to it
- Engage local government and elected officials in Community Heart & Soul actions and decision-making
- Develop and sustain relationships with community organizations and residents to ensure support
- **Promote and celebrate Community Heart & Soul achievements**



Phase 1: Imagine (2-3 months)

- Define overarching Heart & Soul goals and geographic area
- Begin work on the Community Network Analysis
- Form and organize a Heart & Soul Team with a pool of volunteers for initial Heart & Soul activities
- Identify specific tasks/jobs and the people interested in those responsibilities
- Determine the project coordination plan
- Inform and link local officials/town government into Heart & Soul Team activities
- Develop core messaging and marketing tools
- Launch Heart & Soul in the community
- **Reflect and celebrate!**



Throughout Community Heart & Soul

- Apply the principles of Involve Everyone, Focus on What Matters, and Play the Long Game
- Use the Community Network Analysis while planning activities; continually review and add to it
- Engage local government and elected officials in Community Heart & Soul actions and decision-making
- Develop and sustain relationships with community organizations and residents to ensure support
- **Promote and celebrate Community Heart & Soul achievements**



— Community —
Heart & Soul
Phase 2: Connect
(6-8 months)

- Create a story gathering strategy using the Community Network Analysis as a tool to involve everyone
- Gather stories throughout the community
- Form story listening groups to summarize data from stories and strengthen community relationships
- Input all data from story listening into the Project Spreadsheet
- Share and celebrate stories throughout the community using multiple methods
- With community input, draft Heart & Soul Statements from story data
- Revise and validate Heart & Soul Statements with community input and Coach review and assistance
- Share final Heart & Soul Statements with the community
- **Reflect and celebrate!**



Throughout Community Heart & Soul

- Apply the principles of Involve Everyone, Focus on What Matters, and Play the Long Game
- Use the Community Network Analysis while planning activities; continually review and add to it
- Engage local government and elected officials in Community Heart & Soul actions and decision-making
- Develop and sustain relationships with community organizations and residents to ensure support
- **Promote and celebrate Community Heart & Soul achievements**



— Community —
Heart & Soul
Phase 3: Plan
(4-6 months)

- Continue to promote and share Heart & Soul Statements across the community
- Meet with town officials, partners and organizations to talk about how their work aligns with the Heart & Soul Statements
- Pull action ideas from stories and gather additional action ideas throughout the community, using Heart & Soul Statements as a framework for multiple gathering methods
- Prioritize action ideas with broad community input
- Begin drafting Community Action Plan with help from municipal and community leaders, community partners, and potential implementers to build shared ownership for prioritized actions
- Identify key implementers of the Community Action Plan and begin stewardship planning
- **Reflect and celebrate!**



Throughout Community Heart & Soul

- Apply the principles of Involve Everyone, Focus on What Matters, and Play the Long Game
- Use the Community Network Analysis while planning activities; continually review and add to it
- Engage local government and elected officials in Community Heart & Soul actions and decision-making
- Develop and sustain relationships with community organizations and residents to ensure support
- **Promote and celebrate Community Heart & Soul achievements**



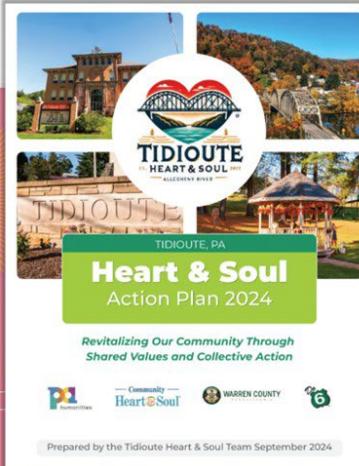
— Community —
Heart & Soul
Phase 4: Act
(2 months)

- Adopt Heart & Soul Statements at local governmental and organizational levels
- Publicly acknowledge and promote Heart & Soul Statements throughout the community
- Confirm support from key community institutions for the Community Action Plan and involvement in stewardship
- Finalize Community Action Plan and share with residents, government, and community organizations
- Develop implementation strategy for Community Action Plan
- Incorporate Heart & Soul Statements in municipal plans including comprehensive plans, economic development plans, recreational plans, and organizational plans, where appropriate
- Activate Heart & Soul Stewardship Team and Stewardship Plan that includes government representation
- Establish system for monitoring Heart & Soul progress and reporting to the community
- Continue to integrate Heart & Soul values and practices into the community's planning and decision-making culture
- **Reflect and celebrate!**



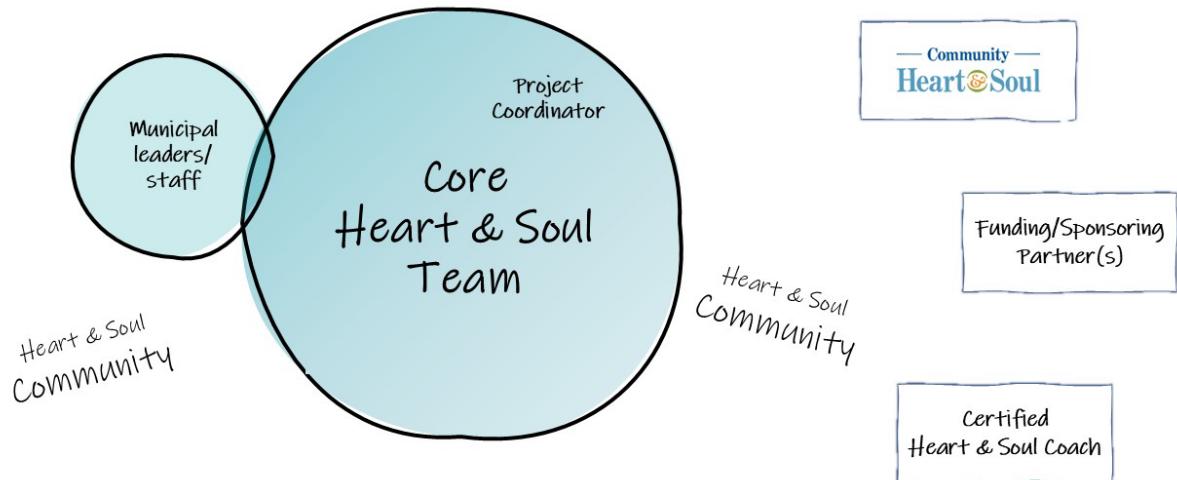
Throughout Community Heart & Soul

- Apply the principles of Involve Everyone, Focus on What Matters, and Play the Long Game
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- Promote and celebrate Community Heart & Soul achievements



Community Action Plans

CommunityHeart & Soul
Guided by What Matters Most



Roles & Relationships in Heart & Soul

Community Heart & Soul
Guided by What Matters Most

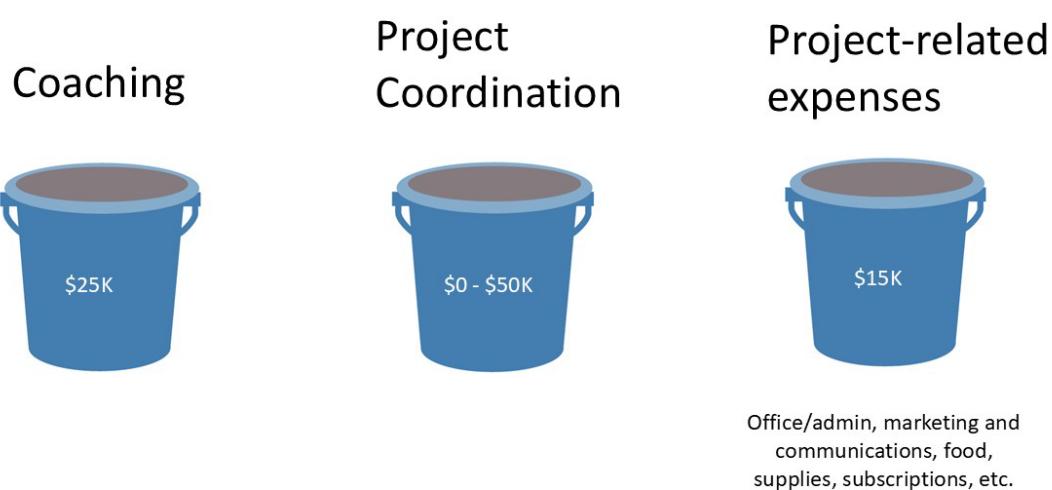
Timeline

Getting Started series = 3 months

After launch (4 phases) = 14-19 months

Ongoing Practice





3 categories of investment

Community Heart & Soul
Guided by What Matters Most

If Scappoose launches Heart & Soul,
Community Heart & Soul provides...



\$10,000 Seed Grant to
launch Heart & Soul in your
community + Coaching
support (\$25K value)



Community Heart &
Soul Project Roadmap



Support through
national peer
network



All the materials,
resources, and
toolkits to
complete the
step-by-step
process!

Certified Heart & Soul Coach *provides...*



**Training
Workshops for
each Phase**

**Strategy sessions
and mentoring
throughout**



**Feedback and
guidance to keep
the project on
track**



**Help with using the
Heart & Soul
materials, resources,
and toolkits**

Scappoose community provides...

**Matching
\$10,000 for Seed
Grant + other
fundraising for
project expenses**

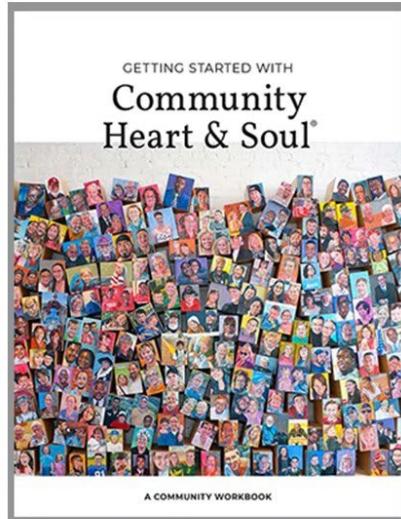


**Energy, effort, and
enthusiasm to
determine
“what matters most”
to those in
Scappoose**

**Core Team of volunteers
to steer the project and
engage the community**

**Local Project
Coordinator**

Next Step: Schedule Getting Started



Scan the QR code to access the electronic version of *Getting Started with Community Heart & Soul*.

What is
Getting
Started?

Series of exploratory conversations and activities within your community to determine...

- if Heart & Soul is **right** for your community
- if your community is **ready** to do Heart & Soul

What's involved in Getting Started activities?



- Community members gather to participate in a series of workshops facilitated by a Coach
- Activities can be repeated on your own—instructions are in Community Workbook
- Community has “Homework” to do between workshops

Goals:

- Explore together, generate interest, and build momentum
- Increase the number of community members participating over time
- Complete milestones and make a decision about launching Heart & Soul

What will your
community gain
from the
Getting Started series?



Who should participate?

Anyone interested in getting to know their community better and contributing to making it a thriving one! Including...

- Arts and cultural groups
- Community and civic groups
- Faith-based groups
- Local non-profits
- Municipal officials
- Municipal services staff
- Neighborhood associations and groups
- Major employers, business owners, and organizations supporting businesses
- Schools, educators, and youth organizations



- Workshops and conversations facilitated by a Heart & Soul Certified Coach
- Digital copies of *Getting Started* Community Workbook to guide conversations and activities
- \$500 for Getting Started activities (refreshments and materials to bring people together!)

Getting Started support

Community Heart & Soul™

An Introduction City of Scappoose City Council

December 15, 2025

Spirit Lake Idaho Community Heart & Soul Phase 2 – Story Gathering

<https://vimeo.com/1118894700/5c1e2a5482?share=copy>



Mayor Backus and Council thanked Bill.

This will be on the Council Retreat agenda.

The Non-Partisan Pledge of Civility and Stewardship Discussion

Councilor Haugen explained this is a continuation from last month's meeting and he is increasingly convinced that we need to do something along those lines to kind of move the ball forward. He explained the 30,000 some communities that have city councils in our Country, they're the foundation of the Country and we need to do the modeling for that because we're not getting modeling from certain sectors of our system. He thinks it's incumbent on us in our communities to do the modeling. The civility and stewardship concept he thinks is really essential and important to push forward and he is not all that concerned at what this precise language is. He explained Councilor Holmes provided a South Carolina example of their pledge, which is fine. It's probably not as pointed as the one that he prefers, but that's fine too. At least

we're making a statement that we're going to be models for our communities and by extension for the Nation and for the citizens that come in to visit with us, especially on public comments. He is hoping that whether we use the one that he presented or the one that comes out of South Carolina that we get this done sooner than later.

Councilor Holmes stated she doesn't particularly love the one from South Carolina. She just presented an alternative, and she would say they took the approach of a resolution. She thinks that is an important distinction for us to consider in this conversation. Are we looking to ask Council members to do individual personal pledge and then how does that bind future Councils potentially and what if a Councilor does want to do it versus a resolution which would just be a majority kind of one-time action statement.

Councilor Haugen explained if we can't get unanimity on this, he wouldn't even do it. If everyone doesn't agree to this he wouldn't want to proceed with it because that doesn't send the right message.

City Manager Burgener explained there are some pieces in the team agreement that reflect the civility pledge, that is why it was in the packet. He asked Councilor Haugen if he wants to reconstruct the team agreement or does he think this should be completely separate?

Councilor Haugen replied he thinks the team agreement is separate because that is for the Council. To him this is sending the message to the broader community and everywhere else that we believe in civility and stewardship going forward. He stated regardless to what happens everywhere else we here in Scappoose believe in this and we stand behind it with unanimous consent that we support this and he thinks it is a powerful statement.

Council President Miller moved, and Councilor Santiago seconded the motion to extend the meeting past 9:00pm. Motion Passed (7-0). Mayor Backus, aye; Councilor President Miller, aye; Councilor Santiago, aye; Councilor Holmes, aye; Councilor Haugen, aye; Councilor Marquis, aye; and Councilor Riutta, aye.

Councilor Haugen stated since our goal setting session is in January, let's put this as an item that we are going to resolve during the goal setting as a Council.

Mayor Backus and Council will send feedback to City Manager Burgener prior to the goal setting session.

Councilor Holmes would like to see what is the merit of this and what additional value does this bring.

The consensus of the Council is to go forward with this at the Council retreat.

Resolution Discussion

Councilor Santiago explained she brought this up at the last meeting as well. She explained the reason why she brought this up is because even though we haven't had any documentation of ICE related incidents within the City of Scappoose, it's obviously on the news and on the radio, it is all around. She stated it is impacting our residents and it is a safety issue, a sense of safety. She explained our role as local leaders is to foster trust, transparency, and then also proactive language. Fear is not, it just doesn't say okay, here in Scappoose that's where the fear stops, right? She is hearing and seeing that people are scared to go to school to go to work, to go shopping. There are many residents that work here that travel from their homes or live here and then go to work, so they're afraid to go to work there. She is sure this is not news to any of you. Over the past few months, she has heard, like she said, directly from families. She has asked them to come and present during the Council, she can't make them, but they have shared their concerns with her about not feeling safe, and about not being able to do their work. A lot of them do work in the trades and they're afraid. So, this fear does exist and so it's important for us. We should really be concerned because this is also a mental health issue. She doesn't know if we have heard school attendance is down. It has affected the businesses economically, and it has affected a lot of institutes. As local leaders, it's our responsibility to respond to what's happening. She likes to be proactive instead of reactive, so we need to foster trust, provide clarity and dialogue before the fear turns into harm. She wants to be clear she understands the limits and that we cannot change Federal Law. We have to abide by Federal Law, she gets that, but that doesn't mean that we're powerless, right? She fully understands that any resolution, any statement must respect the boundaries of Federal Law. She is not trying to interfere with any Federal operations. Last time when she was asking, she did pass out a resolution and guidelines and it is more to highlight what's already in place. She is not asking for any policies to pass. This is how we as local leaders can be transparent. Since she last brought this up, she came across a letter dated November 24th of this year, the Attorney General Dan Rayfield, issued a formal notice of the US Department of Justice and Department of Homeland Security regarding reported patterns of excessive force by Federal Officers within Oregon. The letter makes clear that the Oregon Department of Justice, along with local attorneys, are actively monitoring the Federal conduct within the State asserting Oregon interest in protecting the safety and constitutional rights of our residents. She is not raising to debate the Federal authority, but to underscore the concern about community safety and that it's not hypothetical. It's being addressed at the highest level at State government, so local community organizations, including the Columbia County Coalition of Human Dignity, are raising questions and asking for clarity. As part of that, Chief Fluellen, her, and others met. She thanked Chief Fluellen for answering their questions. She explained it's just a lot of information, a lot of people have questions, but just reiterating it just helps people process things. We are clearly informed that Oregon is a longstanding sanctuary State and that the law enforcement does not participate in civil immigration enforcement and that neither Scappoose Police Department nor Columbia County Sheriff's Office is aware of ICE operations in our City or County. That clarity is important, but it does not erase the fear that people are feeling. One of the most urgent gaps to identify is the residents do not know where to turn for accurate information. That's where we can help. So last time she did give out resources and so we should be able to publicly trust these resources and put that out into our social media for residents. The Portland Immigration Rights Coalition, the IRC Echo, it's ACO are doing great work for families. She explained if you have any questions, feel free to reach out to her. These resources

provide guidance, rapid response, and coordination to support families who are scared and impacted. This information should be accessible not only tonight, but throughout the City's communication channels so residents know where to turn if needed. She stated tonight she is asking Council not to change any laws or adopt new enforcement policy. What she is asking is to discuss how, as a City, we can formally, clearly communicate the commitment to being safe. Inclusive community, one where one resident knows that the local government sees them, hears them and will help direct them to the appropriate resources. That could take the form of a statement, a resolution, another legal sound approach the Council is comfortable with. So doing nothing is basically neutral and it's not very helpful for our community. So, this is about transparency, this is about trust, this is about being proactive instead of reactive. Every resident in Scappoose deserves to feel safe going to school, going to work, going to the grocery store regardless of their background and or immigration status. She is bringing this forward because our community is asking for reassurance and because local leaders matter when the people feel uncertain and afraid. Thank you for hearing her out and she looks forward to your thoughts moving forward.

Legal Counsel Ashleigh Dougill explained she heard a question to the extent that the City can share resources for Community Members. If you're simply sharing information that is accurate or resources that are, you know, publicly available in the Community and potentially publishing that just on the city website, for example, she would direct you to Hillsboro and the web page they have on immigration resources on their government website, if you're doing something along those lines, that wouldn't be an issue from her perspective. It becomes more of a potential legal issue where you are giving targeted advice or targeted assistance to specific Community members. With the caveat that she would suggest having legal counsel review the resources that you're sharing and then vetting internally before formally publishing them.

Mayor Backus and Council discussed this agenda item further and will add this discussion to the Council Retreat agenda.

Announcements – information only

Calendar

Mayor Backus went over the calendar.

Updates: City Manager; Police Chief; Councilors; and Mayor

City Manager Burgener explained we are looking at January 31 or February 7 for the Council Retreat. He stated he appreciates all of the staff and everything they do.

Chief Fluellen gave an update on the Police Department. He gave an update on the Annual Donut Day and explained they collected \$4,000 and about 800 pounds of food. He gave an overview on the Flock system and how it would help our City.

Student Representative Ferreira gave an update on the Youth Advisory Council within the school.

Councilor Riutta would like to extend his deep appreciation to all who put on the Whoville 2025. He and his family enjoyed it, and it was absolutely fantastic. It is a shining star in our community that everyone should come and enjoy.

Councilor Marquis explained there are a lot of people who don't share any of our values in the community and he doesn't think that it behooves us to represent some of the worst instincts in the community. He knows that we are representing Scappoose but he thinks we need to be representing the best instincts in Scappoose. He stated half the City might be a bigot and he doesn't want to be a bigot just because half the City is a bigot. Same thing with civility. There might be a lot of people who are into divisiveness or trying to ruffle feathers and stuff. That's not the kind of representative he wants to be, so he just wanted to say that because it was on his heart.

Councilor Haugen gave an update from the School Board meeting. Enrollment is now over 4,000, primarily because of the online academy. He handed out his annual calendar.

Councilor Holmes wished everyone who observes Hanukkah Happy Hanukkah, we're in the midst of that and a wonderful Christmas as well. She will not be able to join the City for the end of year celebration, as it is her son's birthday. She wished him a Happy birthday! She does want to offer her gratitude. This is wrapping up her third year serving as a City Councilor and she would say this is the most successful year that she's been engaged in with the City. The future looks bright for Scappoose and it's because of all the hard work that the City staff and Council does. She stated thank you all and she wants to wish everyone a wonderful holiday season. She explained she had the opportunity to attend the very first CERT meeting and it was very exciting. CERT (Community Emergency Response Team) is a Federally supported program that helps bring community members together to ensure that the community is prepared for a variety of natural disasters or other incidents that might require support so that we can be resilient as a community. She applauded Chief Fluellen's efforts to bring the group together. She thinks we have a solid initial core group and look forward to seeing the group grow.

Councilor Santiago explained she unfortunately won't be able to attend the celebration, but she wants to wish everyone a Merry Christmas, Happy Hanukkah. Speaking of Hanukkah, she is so sorry and pray for all the people that were affected by the tragedy in Australia. She stated we should be better and even though there's hard times we should be sharing the holiday cheer to everyone. She explained she volunteered on Sunday night as a greeter at Whoville and it was a great experience because you get to hear about how amazing it is. They had people drive from Seattle to come and see the display. They had 360 people show up on a Sunday night. Even people that live here were just in awe. Janet and her team do an amazing job. There was a conversation just briefly on how we can expand Whoville, and maybe we can have that conversation at the Council Retreat.

Council President Miller thanked everyone on Council for sharing their different viewpoints that they have on these difficult conversations and just being able to engage in respectful dialogue. It has already been said but thank you to Janet Williams and the rest of the Historic Society team. If you don't know, Janet starts working on Whoville months in advance, and it's amazing to see. If you ever stop and see some of the things she's working on it is quite amazing. He wished everyone Happy Holidays!

Mayor Backus stated thank you, Tyler, for the Youth Council information, that'll be exciting. Whoville has been great. He has gone a couple of times. He thinks the Mayor of Whoville might show up again. He thanked staff. He stated he is finishing up his third year and it's been great. He stated be safe this holiday season and we'll see you next year.

Adjournment

Mayor Backus adjourned the meeting at 9:50pm.

Mayor Joseph A. Backus

Attest:

City Recorder/HR Susan M. Reeves, MMC

City of Scappoose Council Action & Staff Report

Date Submitted: December 30, 2025

Agenda Date Requested: January 5, 2026

To: Scappoose City Council

From: N.J. Johnson, City Planner/Assistant to City Manager

Subject: Request by Sherman & Marsha Garver and Richard & Christianne Watt for Annexation, Zone Change, and Minor Partition

Type of Action Requested:

| | | | |
|--------------------------|----------------------|-------------------------------------|--------------------|
| <input type="checkbox"/> | Resolution | <input checked="" type="checkbox"/> | Ordinance |
| <input type="checkbox"/> | Formal Action | <input type="checkbox"/> | Report Only |

Issue

Sherman & Marsha Garver and Richard & Christianne Watt have requested approval of a consolidated application for Annexation to annex the subject site into City limits, Zone Change to rezone the subject site from Single-Family Residential (R-10) County zoning to Low Density Residential (R-1) City zoning, and a Minor Partition to partition Tax Lot 3000 into two lots. The site contains two parcels addressed 34094 and 34102 SE Elm Street, south of the SE Elm Street and SE Tussing Way intersection, on properties described as Columbia County Assessor Map #3107-CC-03000 and #3107-CC-03100.

Annexation and Zone Change require City Council approval. While staff would normally be the approval authority for Minor Partition, the applicant requested a consolidated decision and so the City Council will be the approval authority for the entire consolidated application.

The Planning Commission held a public hearing regarding this request on December 11, 2025. Written comments for this hearing were due December 10, 2025, the day prior to the Planning Commission hearing, at 5:00 pm. Members of the public also had the opportunity to provide verbal testimony at the December 11, 2025 Planning Commission hearing. No written or verbal comments were submitted by the public prior to or during the Planning Commission hearing and none have been submitted prior to the Council hearing, as of the date of this report.

Analysis

The applicant is requesting the Annexation and Zone Change because they desire to connect to City utilities (Tax Lot 3000 to water and sewer, Tax Lot 3100 to sewer), which requires properties to be in city limits in most circumstances, including this case. The site is in the urban growth boundary (UGB) and is designated as Suburban Residential (SR) on the Comprehensive Plan Map. UGB sites with an SR designation are scheduled to be zoned Low Density Residential (R-1) upon annexation. The applicant is requesting for the site to be zoned R-1 upon annexation, in conformance with the Comprehensive Plan and Development Code.

All findings related to the approval criteria for the consolidated land use applications, including the recommended conditions of approval, are contained within the Planning Commission staff report, dated December 4, 2025 (**Exhibit C**). Ordinance 924 to approve the consolidated applications is included as **Exhibit A**.

After considering the facts in the record, the Planning Commission voted unanimously to forward a recommendation of approval to the City Council, subject to conditions of approval as outlined in the Planning Commission staff report (**Exhibit C**).

Interested parties have the opportunity to submit written testimony prior to the City Council hearing as well as oral testimony during the City Council hearing. Written comments for the Council proceedings are due Friday, January 2, 2026 at 5:00 pm and oral testimony will be heard at the January 5, 2026 City Council meeting. Written and oral testimony submitted as part of the Council proceedings may only provide argument but may not introduce new evidence. The opportunity for the public to submit new evidence concluded during the December 11, 2025 Planning Commission hearing.

Recommendation

Staff recommends that Council adopt Ordinance 924, thereby approving ANX 1-25, ZC 2-25, MiP 1-25 and accepting the findings and conditions of approval within the Planning Commission staff report, dated December 4, 2025, as written.

Suggested Motion

I move Council adopt Ordinance 924, thereby approving ANX 1-25, ZC 2-25, MiP 1-25 and accepting the findings and conditions of approval within the Planning Commission staff report, dated December 4, 2025, as written.

Exhibits

Ordinance 924 with attachments:

- A. Annexation (legal) description, completed by KLS Surveying Inc.
- B. Annexation area, completed by KLS Surveying Inc.
- C. Planning Commission Staff Report, dated December 4, 2025 with all exhibits

ORDINANCE NO. 924

**AN ORDINANCE APPROVING ANNEXATION OF PROPERTY TO THE CITY OF SCAPPOOSE,
AMENDING THE ZONING MAP, AND MINOR PARTITION**

WHEREAS, Sherman & Marsha Garver and Richard & Christianne Watt filed an application to annex property described in **Exhibit A** and depicted in **Exhibit B**, attached hereto and incorporated herein, to the City of Scappoose, as well as for Minor Partition; and

WHEREAS, the property described in **Exhibit A** would automatically be zoned Low Density Residential (R-1) upon annexation due to the property's Suburban Residential Comprehensive Plan designation, pursuant to Section 17.136.070 of the Scappoose Municipal Code; and

WHEREAS, public notice pursuant to ORS 222.120 and Scappoose Municipal Code Chapters 17.22, 17.136, and 17.162 was given; and

WHEREAS, the Planning Commission held a hearing on the consolidated application on December 11, 2025 and voted unanimously to recommend that the City Council approve the entire consolidated application subject to the conditions of approval and findings contained within the ANX 1-25, ZC 2-25, MiP 1-25 Planning Commission staff report dated December 4, 2025 (**Exhibit C**); and

WHEREAS, the City Council held a hearing on the consolidated application on January 5, 2026; now therefore,

THE CITY OF SCAPPOOSE ORDAINS AS FOLLOWS:

Section 1. The property described in **Exhibit A** is hereby annexed into the city limits of the City of Scappoose, Oregon.

Section 2. The property described in **Exhibit A** is hereby rezoned from Single-Family Residential (R-10) Columbia County zoning to Low Density Residential (R-1) City of Scappoose zoning. The City Planner is directed to conform the Zoning Map to the provisions of this Ordinance.

Section 3. In support of the proposed Annexation, Zone Change, and Minor Partition, the City Council hereby adopts the findings, conditions of approval, and recommendations made by the Scappoose Planning Commission outlined in the ANX 1-25, ZC 2-25, MiP 1-25 Planning Commission staff report, dated December 4, 2025 (**Exhibit C**), attached hereto and incorporated herein.

Section 4. This Ordinance is effective 30 days after passage if not appealed. If appealed, this Ordinance is effective upon the resolution of all appeals.

PASSED AND ADOPTED by the City Council this _____ day of January, 2026, and signed by the Mayor and City Recorder in authentication of its passage.

CITY OF SCAPPOOSE, OREGON

Joseph A. Backus, Mayor

First Reading: January 5, 2026

Second Reading:

Attest: _____

Susan M. Reeves, MMC, City Recorder/HR

DRAFT



KLS Surveying Inc.

1224 Alder Street
Vernonia, OR 97064

Phone: (503) 429-6115
Fax: (866) 297-1402
Email: don@klssurveying.com

Exhibit A Annexation description

A tract of land lying in the Southwest quarter of Section 7, Township 3 North, Range 1 West of the Willamette Meridian, Washington County, Oregon being more particularly described as follows:

- Beginning at a point North 211.12 feet and East 206.73 feet from the Southwest corner of said Section 7;
- thence South $64^{\circ}14'40''$ East 170.63 feet to the West line of Cascade Meadow, Columbia County Survey Records;
- thence North $20^{\circ}05'49''$ East along said West line 526.03 feet to the South line of SE Elm Street;
- thence North $64^{\circ}19'50''$ West along said South line 170.54 feet;
- thence South $20^{\circ}06'15''$ West 525.76 feet to the point of beginning.

Containing 2.05 acres more or less.

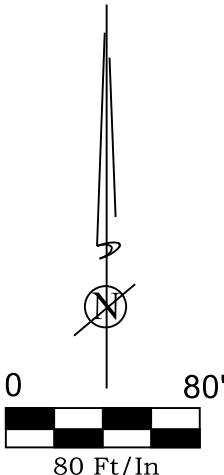
The basis of bearings for this description is Oregon Coordinate Reference System Columbia River West Zone, NAD83 (2011) EPOCH 2010.00.

REGISTERED
PROFESSIONAL
LAND SURVEYOR

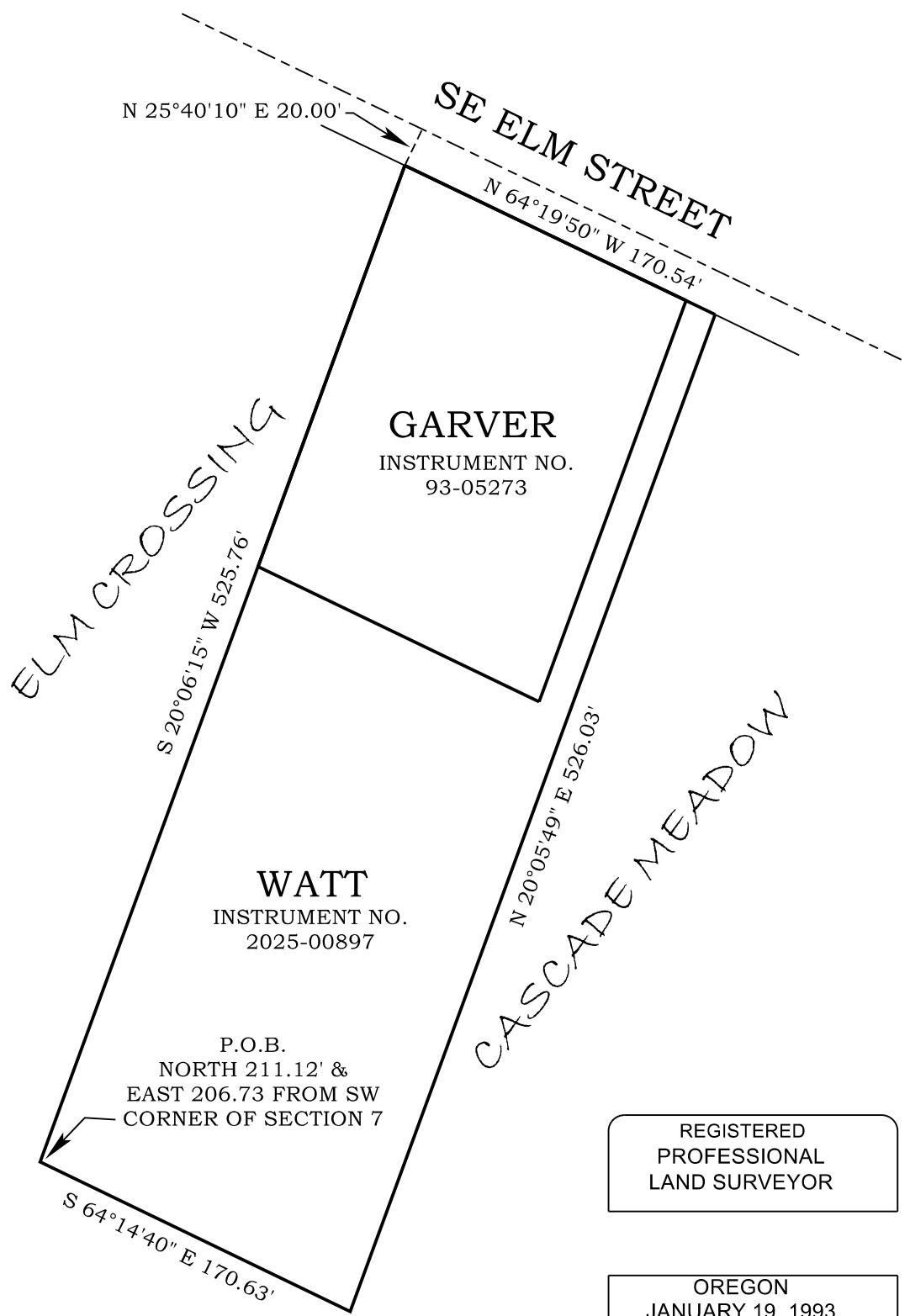
OREGON
JANUARY 19, 1993
DONALD D WALLACE, JR
2601

RENEWS 6/30/26

EXHIBIT B
ANNEXATION AREA
IN THE SW 1/4 OF SECTION 7,
T3N, R1W, W.M.,
COLUMBIA COUNTY, OREGON



DWG NO. 25-151 EXHIBIT
JOB NO. SCAPPOOSE
10-15-2025



K.L.S. SURVEYING INC.
1224 ALDER STREET
VERNONIA, OREGON 97064
(503) 429-6115

CITY OF SCAPPOOSE
STAFF REPORT & DECISION

Request: Approval of an application for Annexation to annex the subject site into City limits, Zone Change to rezone the subject site Single-Family Residential (R-10) County zoning to Low Density Residential (R1) City zoning, and Minor Partition to partition Tax Lot 3000 into two lots.

Location: The site contains two parcels addressed 34094 and 34102 SE Elm Street, south of the SE Elm Street and SE Tussing Way intersection, on properties described as Columbia County Assessor Map #3107-CC-03000 and #3107-CC-03100. See Vicinity Map (**Exhibit 1**).

Applicant: Sherman & Marsha Garver and Richard & Christianne Watt

Owner(s): Sherman & Marsha Garver (Tax Lot 3000) and Richard & Christianne Watt (Tax Lot 3100)

EXHIBITS

1. Vicinity Map
2. Application Form
 - A. Annexation
 - B. Minor Partition
3. Land Use Narrative
4. Preliminary Development Plans
 - A. Cover Sheet (Sheet G-1)
 - B. Existing Conditions Plan (Sheet C-1)
 - C. Proposed Conditions Plan (Sheet C-2)
 - D. Frontage Improvement Plan (Sheet C-3)
 - E. Standard Details (Sheet D-1)
 - F. Topographic Survey
 - G. Preliminary Partition Plat
5. Annexation Legal Description, completed by KLS Surveying Inc., dated October 15, 2025
6. Shared Driveway Agreement, recorded February 21, 2025
7. Referral comment from City of Scappoose Public Works Director, dated November 6, 2025
8. Referral comment from Columbia County Building Official, dated November 10, 2025
9. Referral comment from Columbia River PUD, dated November 10, 2025
10. Referral comment from Columbia County Planning Director, dated November 19, 2025
11. Referral comment from Columbia County Public Works, dated December 2, 2025

SUBJECT SITE

- The subject site consists of two existing tax lots (TL) where TL 3000 is 0.75 acres and TL 3100 is 1.3 acres, totaling 2.05 acres (see **Exhibit 4B**). TL 3000 contains a single-family residence, manufactured home, shop, two paved driveway paths connecting to gravel paths, and some trees (see **Exhibit 4B**). TL 3100 contains a single-family residence, shop, gravel driveway path, and some trees (see **Exhibit 4B**).
- Neither TL 3000 or 3100 are currently connected to municipal utilities. TL 3100 is experiencing a failing septic system and desires to connect to City sewer as a remedy to this (see **Exhibit 3, p. 4**).
- North of the subject site is SE Elm Street and to the north of that is SE Tussing Way and two rows of single-family homes, making up a portion of the Tussing Subdivision. West and east of the subject site are single-family homes platted as Elm Crossing Subdivision and Cascade Meadow Subdivision, respectively. South of the subject site is a large residential property with a house and several outbuildings.
- The subject site is currently in the urban growth boundary (UGB), zoned by Columbia County as Single-Family Residential (R-10), and designated by the City of Scappoose Comprehensive Plan Map as Suburban Residential (SR). The properties to the north and east of the site are in City limits, zoned Moderate Density Residential (R-4), and designated as SR on the Comprehensive Plan Map. The properties to the west and south of the site are in City limits, zoned R-1, and designated as SR on the Comprehensive Plan Map.
- The subject site is not located within the Special Flood Hazard Area (commonly referred to as the 100-year floodplain) and there are no wetlands, slope hazards, or watercourses on or near the site (see **Exhibit 4B**).

OBSERVATIONS

ANNEXATION/ZONE CHANGE

- The applicant is requesting approval of an Annexation (**Exhibit 2A**) that would annex both parcels into City limits. The properties are currently in the UGB and are eligible for annexation according to the Development Code and Comprehensive Plan.
- The subject site is currently zoned R-10 by Columbia County. With an SR Comprehensive Plan Map designation, the subject site will automatically be zoned R-1 upon annexation.
- The subject site is currently in the UGB but surrounded by properties in city limits in each direction. Annexing this site would eliminate an island of unincorporated property, promote desirable urban form, and continue the organic progression of the city.

PARTITION

- The applicant is requesting approval of a Preliminary Partition Plat (**Exhibit 4G**) to partition TL 3000 into two lots to establish separate parcels for the existing single-family home and manufactured home. The lots are proposed to be 24,503 square feet and 6,500 square feet, respectively (see **Exhibit 4G**).
- The Partition request does not create any new streets (see **Exhibit 4G**) so it is not considered

a Major Partition and it is less than 4 lots (see **Exhibit 4G**) so it is not considered a Subdivision. Therefore, this request will be processed as a Minor Partition.

- Given that the Minor Partition proposal is consolidated with the Annexation proposal, the Minor Partition will be processed by the City, subject to City standards since the parcel will be in City limits if the Annexation is approved.

RIGHT-OF-WAY/PUBLIC IMPROVEMENTS

- All proposed parcels of the subject site have frontage on SE Elm Street (see **Exhibit 4B**). This section of SE Elm Street is classified by the 2016 Transportation System Plan (TSP)¹ as a Neighborhood Route, which requires 60 feet of right-of-way width comprised of 36 feet of vehicular travel way & parking, two 5.5-foot planter strips, two 6-foot sidewalks, and two 6-inch utility areas. The SE Elm Street right-of-way is primarily 50 feet wide with a ~29-foot paved width (see **Exhibit 4B**). There are sidewalks on the north side of SE Elm Street and to the east and west of the subject frontage but no sidewalk or curb on the subject site's frontage. The applicant is proposing to provide a 10-foot dedication along TL 3000 to bring the right-of-way width to standard, install a 6-foot sidewalk with curb and gutter along the entire frontage including two new driveway approaches, pave between the existing asphalt concrete and the new curb, and plant street trees (see **Exhibit 4D**). The applicant did not propose a dedication along the frontage of TL 3100 (see **Exhibit 4C**) since it is not part of the Minor Partition proposal, which is the element of the consolidated application that requires the dedication along the frontage of TL 3000. However, the applicant is proposing to install a sidewalk that would go through what is currently and proposed to remain as private property along the frontage of TL 3100 (see **Exhibit 4D**). The applicant will be required by the recommended conditions of approval to dedicate 10 feet of TL 3100 as right-of-way or record a public sidewalk easement in the area that would be dedicated that allows for the public to use the sidewalk as a public walkway as if it were in the right-of-way.
- The applicant will be required to install one new streetlight on their frontage. The applicant has proposed to install a streetlight to the west of the eastern driveway in the right-of-way (see **Exhibit 4D**).

UTILITIES

- There is currently a 15-inch polyvinyl chloride (PVC) sewer main in SE Elm Street as well as a 12-inch concrete sewer main that extends south from the 15-inch PVC main into and through the rear yards of the parcels to the west of the subject site (see **Exhibit 4B**). The applicant proposes installing three new sewer laterals from the SE Elm Street main to serve each of the existing homes (see **Exhibit 4C**). The applicant also indicated on their Proposed Conditions Plan (**Exhibit 4C**) that the contractor may evaluate the feasibility of serving the home on TL 3100 with the sewer main to the west of the subject site that runs through the rear yards of the adjacent homes in the Elm Crossing Subdivision instead of the main in SE Elm Street.
- There is an existing 24-inch PVC stormwater main in SE Elm Street (see **Exhibit 4B**). The applicant is proposing to relocate and install a new catch basin to the west of the driveway

¹ City of Scappoose, Transportation System Plan: Volume 1, 2016, Figures 12 & 13b.

serving Parcel 2 (see **Exhibit 4C**) as required. Runoff will be directed towards this relocated catch basin (see **Exhibit 4C**).

- There is currently an 8-inch cast iron water main in SE Elm Street. The applicant proposes to install two new water laterals from the main to serve Parcels 1 and 2 (see **Exhibit 4C**). TL 3100 will continue to use its existing well as its water source (see **Exhibit 3, p. 24**).

LANDSCAPING, SCREENING, AND STREET TREES

- The applicant is proposing to plant Autumn Brilliance Serviceberry street trees (see **Exhibit 3, p. 13**), which is a species on the Approved Street Trees list². Autumn Brilliance Serviceberries are 25 feet tall and 20 feet wide at maturity, requiring them to be spaced at 20 feet apart on center with a planting width greater than the required 6 feet, which is what the applicant is proposing (see **Exhibit 3, p. 13**).

PUBLIC AND PRIVATE AGENCIES

- The City of Scappoose City Manager, Public Works Director, and Police Chief; Columbia County Public Works Director, Planning Director, and Building Official; Scappoose Rural Fire Protection District; and Columbia River PUD have been provided an opportunity to review and comment on this proposal. The City Engineer's comments are incorporated throughout this report.
- The City of Scappoose Public Works Director provided a referral comment (**Exhibit 7**) stating that they have reviewed the application and have no objection to its approval, provided it meets all criteria set forth in the Scappoose Municipal Code, zoning criteria, and the Scappoose Public Works Design Standards (PWDS).
- The Columbia County Building Official provided a referral comment (**Exhibit 8**) stating that they have reviewed the application and have no objection to its approval as submitted.
- The Columbia River PUD provided a referral comment (**Exhibit 9**) stating that they have reviewed the application and have no objection to its approval as submitted.
- The Columbia County Planning Director provided a referral comment (**Exhibit 10**) stating that they have reviewed the application and have no objection to its approval as submitted.
- The Columbia County Public Works Department provided a referral comment (**Exhibit 11**) requiring that each parcel created through this land use action obtain an access permit from Columbia County. Additionally, the County is requiring that the applicant improve the SE Elm Street frontage to City of Scappoose standards and obtain a right-of-way permit from Columbia County for any work completed in the right-of-way. The County's comments clarified that these requirements will only be enforced if this section of SE Elm Street is found to be a County-owned road currently and after the parcels are annexed into city limits. These requirements have been incorporated into the recommended conditions of approval.
- Notice of the application and hearings was mailed to property owners within 300 feet of the subject site on December 1, 2025, posted in the November 28, 2025 edition of the Columbia

² City of Scappoose, Approved Street Trees, 2023, page 10, available at:

https://www.scappoose.gov/sites/default/files/fileattachments/building/page/667/scappoose_street_tree_list_updated_2023-06-21.pdf.

County Spotlight, and posted on the subject site in accordance with Chapter 17.162. As of the date of this report, no public comments have been submitted.

FINDINGS OF FACT

The following sections of the Scappoose Development Code are applicable to this request:

Chapter 12.10
VISUAL CLEARANCE AREAS

12.10.020 Visual clearance—Required.

A. A visual clearance area shall be maintained on the corners of all property adjacent to an unregulated intersection of two streets, a street and a railroad, or a driveway providing access to a public or private street.

Finding: Though directly south of the SE Elm Street and SE Tussing Way intersection, the intersection is only a three-way intersection and the subject site is not a corner lot so visual clearance on the site's corners are not required. The applicant is proposing to install two driveways (see **Exhibit 4D**). Visual clearance will need to be maintained next to these driveways. Section 12.10.020 is satisfied.

B. A visual clearance area shall contain no vehicle, recreational vehicle, watercraft, parts designed to be affixed to a vehicle of any type, hedge, planting, fence, wall structure, sign, or temporary or permanent obstruction that would impede visibility between a height of three feet and ten feet above the center line grades of the intersecting streets or railroad.

Finding: The required visual clearance areas (VCA) do not contain any of the items listed above (see **Exhibit 4D**). Section 12.10.020(B) is satisfied.

C. Where the crest of a hill or vertical curve conditions contribute to the obstruction of visual clearance areas at a street, driveway or railroad intersection, hedges, plantings, fences, walls, wall structures and temporary or permanent obstructions shall be further reduced in height or eliminated to comply with the intent of the required visual clearance area.

Finding: The areas of the two driveways have negligible slopes in terms of their impact on visual clearance (see **Exhibit 4D**). Without any slopes causing a visual clearance hazard, stricter visual clearance requirements will not be established. Section 12.10.020(C) is satisfied.

D. The preceding provisions shall not apply to the following:

[...]

Finding: The VCAs are free of any objects that could impede visual clearance (see **Exhibit 4D**). Therefore, no exemptions are necessary to satisfy the requirements of this chapter. Section

12.10.020(D) is satisfied.

12.10.030 Visual clearance area dimensions.

A visual clearance area shall consist of a triangular area, two sides of which are lot lines for distances specified in this section, or, where the lot lines have rounded corners, the lot lines extended in a straight line to a point of intersection and so measured, and the third side of which is a line across the corner of the lot joining the nonintersecting ends of the other two sides. The following measurements shall establish the visual clearance areas:

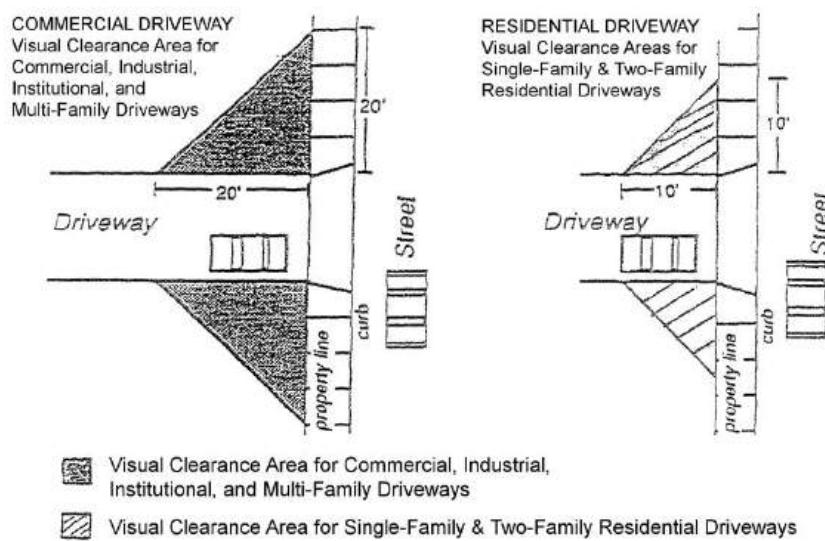
[...]

B. Driveway Intersections

[...]

2. Single-Family and Two-Family Developments. Driveways to public or private streets shall have a minimum visual clearance area formed by the intersection of the edges of the driveway, the street right-of-way line, and a straight line joining said lines through points ten feet from their intersection. No off-street parking area shall be located in a driveway visual clearance area.

Figure 12.10.2 Visual Clearance Areas for Driveways



Finding: The applicant is providing a 10-foot triangulation of visual clearance on both sides of both driveways (see **Exhibit 4D**). Section 12.10.030(B) is satisfied.

Chapter 17.01
INTRODUCTION

17.01.060 Right-of-way dedications and improvements.

Upon approval of any development permit or any land use approval of any property which abuts or is served by an existing substandard street or roadway, the applicant shall make the necessary right-of-way dedications for the entire frontage of the property to provide for minimum right-of-

way widths according to the city's public works design standards and shall improve the abutting portion of the street or roadway providing access to the property in accordance with the standards in Chapter 17.154.

Finding: Both parcels of the subject site have frontage on SE Elm Street (see **Exhibit 4B**). This section of SE Elm Street is classified by the TSP³ as a Neighborhood Route, which requires 60 feet of right-of-way width. The SE Elm Street right-of-way is primarily 50 feet wide (see **Exhibit 4B**). The applicant is proposing to provide a 10-foot dedication along TL 3000 to bring the right-of-way width to standard, install a 6-foot sidewalk along the entire frontage including two new driveway approaches, and plant street trees (see **Exhibit 4D**). The applicant did not propose a dedication along the frontage of TL 3100 (see **Exhibit 4C**) since it is not part of the Minor Partition proposal, which is the element of the consolidated application that requires the dedication along the frontage of TL 3000. However, the applicant is proposing to install a sidewalk that would go through what is currently and proposed to remain as private property along the frontage of TL 3100 (see **Exhibit 4D**). The applicant will be required by the recommended conditions of approval to dedicate 10 feet of TL 3100 as right-of-way or record a public sidewalk easement in the area that would be dedicated that allows for the public to use the sidewalk as a public walkway as if it were in the right-of-way. Section 17.01.060 is satisfied.

Chapter 17.22

AMENDMENTS TO THE TITLE, COMPREHENSIVE PLAN, AND MAPS

17.22.040 Approval criteria.

Planning commission review and recommendation, and Council approval, of an ordinance amending the comprehensive plan, the zoning map, or this title shall be based on the following criteria:

[...]

B. The proposal is consistent with the comprehensive plan (although the comprehensive plan may be amended concurrently with proposed changes in zoning or this title), the standards of this title, or other applicable implementing ordinances;

Finding: The subject site proposed for annexation is designated as SR on the Comprehensive Plan Map and is proposed to adopt R-1 zoning upon annexation (see **Exhibit 3, p. 7**), as scheduled for SR designated properties. This Zone Change is therefore consistent with the Comprehensive Plan and Development Code. Section 17.22.040(B) is satisfied.

C. The change will not adversely affect the health, safety, and welfare of the community;

Finding: The proposed Zone Change will not negatively impact the health, safety, or welfare of the community as the subject site will adopt R-1 zoning, as scheduled by the Comprehensive Plan and Development Code. The zoning and uses will remain residential. Section 17.22.040(C) is

³ City of Scappoose, Transportation System Plan: Volume 1, 2016, Figures 12 & 13b.

satisfied.

D. The proposal either responds to changes in the community or it corrects a mistake or inconsistency in the comprehensive plan, the zoning map, or this title; and

Finding: The applicant is requesting that the site be zoned R-1 upon annexation (see **Exhibit 3, p. 7**), in conformance with the requirements of SR designated properties in Section 17.136.070. There is, therefore, no mistake or inconsistency to correct. Section 17.22.040(D) is not applicable.

E. The amendment conforms to Section 17.22.050.

Finding: The proposal conforms to Section 17.22.050, as discussed in the staff report finding to Section 17.22.050. Section 17.22.040(E) is satisfied.

17.22.050 Transportation planning rule compliance.

Proposals to amend the comprehensive plan or zoning map shall be reviewed to determine whether they significantly affect a transportation facility pursuant to Oregon Administrative Rule (OAR) 660-012-0060 (Transportation Planning Rule - TPR). Where the City, in consultation with the applicable roadway authority, finds that a proposed amendment would have a significant effect on a transportation facility, the City shall work with the roadway authority and the applicant to modify the request or mitigate the impacts in accordance with the TPR and applicable law.

Finding: See findings to OAR 660-012-0060 for this analysis. Section 17.22.050 is satisfied.

Chapter 17.44
R-1 LOW DENSITY RESIDENTIAL

17.44.030 Permitted and Conditional uses.

| <i>Use</i> | |
|---|---------------------------------------|
| <i>Single-family detached residential dwelling unit</i> | <i>Permitted outright¹</i> |
| <i>Manufactured homes on individual lots subject to Section 17.94.030</i> | <i>Permitted outright¹</i> |
| <i>[...]</i> | <i>[...]</i> |

1 These uses and their accessory uses are permitted outright outside of the Scappoose Creek Flood Plain. In the R-1 zone within the Scappoose Creek Flood Plain, only uses listed in Section 17.84.040 shall be permitted. Additional requirements shall include any applicable section of this title.
[...]

Finding: The applicant is proposing to annex the subject site into city limits (see **Exhibit 2A**). The subject site currently contains two single-family detached residential dwelling units, a manufactured home, and accessory structures, which are all outright permitted in the R-1 zone the site will adopt upon annexation. The subject site is not within the 100-year floodplain and so

the standard regulations apply. Section 17.44.030 is satisfied.

17.44.050 Dimensional requirements.

| Dimensional Requirements | Requirement |
|---|--|
| <i>Minimum lot area</i> | |
| Single-family detached | <i>Six thousand (6,000) square feet outside the Scappoose Creek Flood Plain</i> |
| | [...] |
| <i>Minimum lot width</i> | <i>Not be less than fifty feet, except the minimum lot width at front property line on the arc of an approved full cul-de-sac shall not be less than thirty feet</i> |
| | <i>Flag lots shall provide a minimum of twenty-five feet of frontage along a public right-of-way</i> |
| <i>Minimum setback</i> | |
| Front Yard | <i>Twenty feet</i> |
| Front of garages or carports | <i>Twenty feet from the property line where access occurs</i> |
| Side yard | <i>Total a minimum of fifteen feet with one setback not less than ten feet, which shall be on the street side for corner lots</i> |
| Rear yard | <i>Twenty feet</i> |
| <i>Setbacks for accessory building behind a residence</i> | |
| Side | <i>Five feet each</i> |
| Rear | <i>Five feet</i> |
| <i>Maximum height</i> | <i>Thirty-five feet</i> |
| Accessory Building | <i>Twenty-two feet</i> |
| <i>Principal building per lot</i> | <i>One</i> |
| <i>Maximum building coverage</i> | <i>Thirty-five percent of the lot area</i> |

Finding: The subject site currently encompasses two parcels, TL 3000 and TL 3100, and the applicant is proposing a Minor Partition (**Exhibits 2B & 4G**) to partition TL 3000 into two lots. The table below will demonstrate each lot's compliance based on the applicant's Proposed Conditions Plan (**Exhibit 4C**).

| Category | Proposed | Determination |
|-----------------|---|---|
| Lot area | TL 3000: Parcel 1: 24,503 sq. ft. Parcel 2: 6,500 sq. ft. TL 3100: 1.3 acres | Satisfied |
| Lot width | TL 3000: Parcel 1: 80.43 ft. | Not outright satisfied; see discussion below. |

| | | |
|----------------------------------|--|---|
| | Parcel 2: 74.1 ft. TL 3100: 16 ft. | |
| Front yard setback | TL 3000: Parcel 1: 23.1 ft. Parcel 2: 16.8 ft. TL 3100: ~93 ft. | Not outright satisfied; see discussion below. |
| Front of garages setback | TL 3000: Parcel 1: 27.6 ft. Parcel 2: N/A TL 3100: N/A | Satisfied |
| Principal side yard setback | TL 3000: Parcel 1: 19.5 ft., 8.3 ft. Parcel 2: 15.4 ft., 10.4 ft. TL 3100: ~59 ft., ~53 ft. | Satisfied |
| Principal rear yard setback | TL 3000: Parcel 1: ~147 ft. Parcel 2: 16.8 ft. TL 3100: ~141 ft. | Satisfied |
| Accessory side/rear yard setback | TL 3000: Parcel 1: 7 ft. Parcel 2: N/A TL 3100: 11 ft. | Satisfied |
| Principal building height | TL 3000: Parcel 1: ~10 ft. Parcel 2: ~10 ft. TL 3100: ~10 ft. | Satisfied |
| Accessory building height | TL 3000: Parcel 1: <22 ft. Parcel 2: <22 ft. TL 3100: <22 ft. | Satisfied |
| Principal buildings | TL 3000: Parcel 1: 1 Parcel 2: 1 TL 3100: 1 | Satisfied |
| Building coverage | TL 3000: Parcel 1: 11.4% Parcel 2: 20.7% TL 3100: 6.2% | Satisfied |

All dimensional requirements are outright satisfied with the exception of the lot width of existing TL 3100 and the front setback of the home on Parcel 2. The substandard lot width of TL 3100 is existing (see **Exhibit 4B**) and is not created or worsened by the proposed Minor Partition (see **Exhibit 4G**). The structure in what is proposed to be Parcel 2 currently meets the R-1 standards

for front setbacks; however, this development also requires a 10-foot right-of-way dedication, making the distance from the front lot line to the structure substandard. The City is requiring that the applicant fully comply with right-of-way width standards and so the applicant will be held harmless for noncompliant front setbacks on Parcel 2. Section 17.44.050 is satisfied.

Chapter 17.104
STREET TREES

17.104.020 Applicability.

A. The provisions of this chapter shall apply to all development as defined in Scappoose Municipal Code Chapter 17.26, Definitions, except a building permit to add to or remodel an existing single family residence.

B. All development shall be required to plant street trees. Street trees shall be defined as trees located on land lying between the property lines on either side of all streets, avenues or public rights-of-way within the city or within easements defined on a recorded plat as street tree easements.

C. All street trees required under this chapter shall be subject to the requirements of Scappoose Municipal Code Chapter 17.140 Public Land Tree Removal.

Finding: The applicant is proposing an Annexation (**Exhibit 2A**), Zone Change, and Minor Partition (**Exhibit 2B**), which collectively fall under the definition of development; see Section 17.26.030. Therefore, street trees are required. If any street trees are removed as part of this development, the provisions of Chapter 17.140 will be applied. Section 17.104.020 is satisfied.

17.104.040 Standards for street trees.

A. Street trees shall be selected from the approved street tree list on file with the Planning Department.

Finding: The applicant is proposing to plant Autumn Brilliance Serviceberries (see **Exhibit 3, p. 13**), which is a species on the Approved Street Trees list⁴. Section 17.104.040(A) is satisfied.

B. At the time of planting, street trees shall not be less than ten feet high for deciduous trees and five feet high for evergreen trees.

Finding: As Autumn Brilliance Serviceberries are deciduous trees, the Conditions of Approval will require them to be planted at no less than 10 feet tall. Section 17.104.040(B) is satisfied.

C. Spacing and minimum planting areas for street trees shall be as follows:

1. Street trees under twenty-five feet tall and less than sixteen feet wide at maturity shall

⁴ City of Scappoose, Approved Street Trees, 2023, page 10, available at:

https://www.scappoose.gov/sites/default/files/fileattachments/building/page/667/scappoose_street_tree_list_updated_2023-06-21.pdf.

be spaced no further than fifteen feet apart in planting areas containing no less than sixteen square feet of porous surface and not less than four feet wide;

2. Street trees under twenty-five feet tall and greater than sixteen feet wide at maturity shall be spaced no further than twenty feet apart in planting areas containing no less than sixteen square feet of porous surface and not less than four feet wide;

3. Street trees between twenty-five feet to forty feet tall and less than twenty-five feet wide at maturity shall be spaced no greater than twenty-five feet apart in planting areas containing no less than twenty-four square feet of porous surface and not less than six feet wide;

4. Street trees between twenty-five feet to forty feet tall and greater than twenty-five feet wide at maturity shall be spaced no greater than thirty feet apart in planting areas containing no less than twenty-four square feet of porous surface and not less than six feet wide;

5. Street trees greater than forty feet tall at maturity shall be spaced no greater than forty feet apart in planting areas containing not less than thirty-six square feet of porous surface and not less than eight feet wide.

[...]

Finding: Autumn Brilliance Serviceberries are 25 feet tall and 20 feet wide at maturity, triggering the spacing criteria of subsection 3. The applicant is proposing to space the streets 20 feet apart on center with a planting width greater than the required 6 feet (see **Exhibit 3, p. 13**). Section 17.104.040(C) is satisfied.

E. Street trees shall be planted in accordance with the requirements of Scappoose Municipal Code Section 13.28.020(C).

Finding: The Conditions of Approval will require the applicant to plant street trees in accordance with Section 13.28.020(C). Section 17.104.040(E) is satisfied.

17.104.060 Maintenance of street trees.

A. The adjacent owner, tenant, and their agent, if any, shall be jointly and severally responsible for the maintenance of all street trees which shall be maintained in good condition so as to present a healthy, neat and orderly appearance and tree wells shall be kept free from refuse and debris.

B. All street trees shall be controlled by pruning to National Arborist Association Pruning Standards for Shade Trees included as Appendix B of the Scappoose Comprehensive Urban Forestry Plan.

C. Every owner of any tree overhanging any street or right-of-way within the city shall prune the branches so that such branches shall not severely obstruct the light from any street lamp or obstruct the view of any street intersection and so that there shall be a clear space of thirteen feet above street surface or eight feet above the sidewalk surface. Such owners shall remove all dead, diseased or dangerous trees, or broken or decayed limbs which constitute a menace to the safety of the public. The city shall have the right to prune any tree or shrub on private property

when it interferes with the proper spread of light along the street from a street light, or interferes with visibility of any traffic-control device or sign or sight triangle at intersections as defined in Scappoose Municipal Code 12.10, Visual Clearance Areas. Tree limbs that grow near high voltage electrical conductors shall be maintained clear of such conductors by the electric utility company in compliance with any applicable franchise agreements.

D. The city shall have the right to plant, prune, and otherwise maintain trees, plants and shrubs within the lines of all streets, alleys, avenues, lanes, as may be necessary to insure public safety or to preserve or enhance the symmetry and beauty of such public grounds.

E. It is unlawful as a normal practice for any person, firm or city department to top any street tree. Topping is defined as the severe cutting back of limbs within the tree's crown to such a degree so as to remove the normal canopy and disfigure the tree. Trees severely damaged by storms or other causes, or certain trees under utility wires or other obstructions where other pruning practices are impractical may be exempted from this chapter at the determination of the city manager after consultation with a registered arborist or certified forester.

17.104.070 Excavation approval required.

Written approval of the city manager is required prior to any excavation within the dripline of a street tree.

17.104.080 Penalties for damage or removal of street trees.

Any activity that results in injury, mutilation or death of a street tree is prohibited. If such injury, mutilation or death of a street tree shall occur, the cost of the repair or replacement shall be borne by the party performing the activity. The replacement value of street trees shall be determined in accordance with the latest revision of the Council of Tree and Landscape Appraisers evaluation method.

Finding: The applicant has stated in their Narrative (**Exhibit 3, pp. 13-14**) that they acknowledge and accept the responsibilities and regulations above. Sections 17.104.060, 17.104.070, and 17.104.080 are satisfied.

Chapter 17.136
ANNEXATIONS

17.136.020 Policy.

Annexations shall be considered on a case-by-case basis, taking into account the goals and policies in the Scappoose comprehensive plan, long range costs and benefits of annexation, statewide planning goals, this title and other ordinances of the city and the policies and regulations of affected agencies' jurisdictions and special districts.

A. It is the city's policy to encourage and support annexation where:

- 1. The annexation complies with the provisions of the Scappoose comprehensive plan;*

Finding: The subject site is currently designated as SR on the Comprehensive Plan Map and is therefore scheduled to be zoned R-1 upon annexation. The applicant is requesting for the site to

be zoned R-1 upon annexation (see **Exhibit 3, p. 7**), in conformance with the Comprehensive Plan. The applicant's proposal is also in conformance with the Comprehensive Plan Policies for the Urban Growth Boundary as discussed in the staff report findings to those policies. Section 17.136.020(A)(1) is satisfied.

2. The annexation would provide a logical service area, straighten boundaries, eliminate or preclude islands of unincorporated property, and contribute to a clear identification of the city;

Finding: The subject site is currently in the UGB but surrounded by properties in city limits in each direction. Annexing this site would eliminate an island of unincorporated property, promote desirable urban form, and continue the organic progression of the city. Section 17.136.020(A)(2) is satisfied.

3. The annexation would benefit the city by addition to its revenues of an amount that would be at least equal to the cost of providing services to the area;

Finding: Upon annexation, the property owners will begin paying property taxes to the City of Scappoose. Additionally, they will pay the applicable system development charges, connection fees, and monthly utility fees upon connection to City utilities. These fees are anticipated to be equal to or greater than the cost of providing these services. Section 17.136.020(A)(3) is satisfied.

4. The annexation would be clearly to the city's advantage in controlling the growth and development plans for the area.

Finding: Having the subject site annexed into city limits allows the City to manage growth and development plans for the area since annexation requires future development of this site to adhere to City standards of the R-1 zone and other applicable City standards for development. Section 17.136.020(A)(4) is satisfied.

B. It is the city's policy to discourage and deny annexation where:

1. The annexation is inconsistent with the provisions of the Scappoose comprehensive plan;

Finding: See findings to Section 17.136.020(A)(1). The proposed Annexation is consistent with the Comprehensive Plan. Section 17.136.020(B)(1) is satisfied.

2. The annexation would cause an unreasonable disruption or distortion of the current city boundary or service area;

Finding: The subject site is an island of unincorporated land in the middle of the city. Annexing the site would promote desirable urban form. Section 17.136.020(B)(2) is satisfied.

3. *The annexation would severely decrease the ability of the city to provide services to an area either inside or outside of the city;*

Finding: SE Elm Street fronts the subject site and has adequate utilities to service the site (see **Exhibit 4B**) without disruption to other utility users. Section 17.136.020(B)(3) is satisfied.

4. *Full urban services could not be made available within a reasonable time.*

Finding: The applicant is proposing to connect all three parcels to City sewer and two parcels to City water (see **Exhibit 4D**). City water could be provided to all three parcels but the applicant is not requesting water connection to TL 3100 at this time. Section 17.136.020(B)(4) is satisfied.

17.136.040 Approval standards.

A. *The decision to approve, approve with modification or deny, shall be based on the following criteria:*

1. *All services and facilities are available to the area and have sufficient capacity to provide service for the proposed annexation area;*

Finding: Existing municipal police and utility services can be made available to the site immediately and the applicant is proposing connection to City water and sewer upon annexation (see **Exhibit 4D**). The property is already located within the Scappoose Rural Fire Protection District, Scappoose School District, and Scappoose Library District service boundaries. Telecommunication and electric services are currently serving the subject site, demonstrating that adequate capacity exists to do so. No new uses or structures are proposed on the subject site at this time (see **Exhibit 4C**). Section 17.136.040(A)(1) is satisfied.

2. *The impact upon public services which include but are not limited to police and fire protection, schools and public transportation to the extent that they shall not be unduly compromised;*

Finding: The annexation would bring in three residences on a total of 2.05 acres (see **Exhibit 4C**), which is a minimal impact to public service providers. Municipal police services can be made available to the current and future residents of these properties and the site is already in the service district of the Scappoose Rural Fire Protection District and Scappoose School District. The Scappoose Police Department, Scappoose Rural Fire Protection District, and Scappoose School District all had the opportunity to provide comment on the proposal but none submitted comment. Section 17.136.040(A)(2) is satisfied.

3. *The need for housing, employment opportunities and livability in the city and surrounding areas;*

Finding: The subject site has three existing residences on it (see **Exhibit 4B**) and while no additional units are proposed to be built at this time (see **Exhibit 4C**), the subject site as a whole

would have capacity to provide more housing units if developed under City standards rather than County standards. The Annexation would also allow TL 3100 to connect to City sewer, resolving the failing septic system issue (see **Exhibit 3, p. 4**) and improving livability in the city. Therefore, the consideration of the city's need for housing and livability supports annexation. The subject site is not adopting commercial zoning so it will not provide employment opportunities. Section 17.136.040(A)(3) is satisfied.

4. The location of the site in relation to efficient provision of public facilities, services, transportation, energy conservation, urbanization and social impacts.

Finding: The subject site is adjacent to city limits in all directions. SE Elm Street fronts the subject site (see **Exhibit 4B**) and allows users of the site to efficiently connect to City utilities and the street network. The site is conveniently located for a short commute to all public schools in Scappoose, is already in the Scappoose Rural Fire Protection District service boundary, and can be provided police services immediately upon annexation. The subject site is primarily surrounded by residential uses developed at urban densities. Continued urbanization of this site and area is consistent with the Comprehensive Plan. Section 17.136.040(A)(4) is satisfied.

17.136.070 Zoning upon annexation.

Upon annexation, the area annexed shall be automatically zoned to the corresponding land use zoning classification as shown in the table below. The zoning designation shown on the table below is the city's zoning district which most closely implements the city's comprehensive plan map designation.

Comprehensive Plan Designation

SR, Suburban Residential

[...]

Zoning Classification

R-1, Low Density Residential

[...]

Finding: The subject site is currently designated as SR on the Comprehensive Plan Map and is therefore scheduled to be zoned R-1 upon annexation. The applicant is requesting for the site to be zoned R-1 upon annexation (see **Exhibit 3, p. 7**), in conformance with the Comprehensive Plan and this section. Section 17.136.070 is satisfied.

Chapter 17.152

*LAND DIVISION–MAJOR AND MINOR LAND PARTITIONS AND PROPERTY LINE
ADJUSTMENTS*

17.152.020 Partition review required.

A. A major land partition review is required when a division of land creates a street or road (public or private), within one calendar year.

B. A minor land partition review is required when three lots or fewer are created without the creation of a street or road, within one calendar year.

C. A property line adjustment is any adjustment to a property line by the relocation of a common

boundary where an additional parcel of land is not created.

Finding: The applicant is proposing a 2-lot partition that will not create a new public street (see **Exhibit 4G**). Therefore, this request will be processed as a Minor Partition. Section 17.152.020 is satisfied.

17.152.030 General provisions.

A. An application for a major or minor partition shall be processed through a two-step process: (1) the tentative plan, and (2) the final plat:

- 1. The tentative plan for a major partition shall be approved by the planning commission before the final plat can be submitted for approval consideration; the tentative plan for a minor partition shall be approved by the planner before the final plat can be submitted for approval consideration; and*
- 2. The final plat shall reflect all conditions of approval of the tentative plan.*

Finding: The applicant has submitted a Preliminary Partition Plat (**Exhibit 4G**). As a Minor Partition, staff would be the approval authority if it were processed on its own. However, since it is in a consolidated application with an Annexation and Zone Change, City Council will be the approval authority for the Minor Partition. The Conditions of Approval will require the applicant to submit a final partition plat reflective of all applicable Conditions of Approval. Section 17.152.030(A) is satisfied.

B. All partition and property line adjustment proposals shall be in conformity with all state regulations set forth in ORS Chapter 92, Subdivisions and Partitions.

Finding: The Conditions of Approval will require that the final partition plat conforms to the requirements of ORS Chapter 92. Section 17.152.030(B) is satisfied.

C. When partitioning tracts into large lots, the approval authority shall require that the lots be of such size and shape as to facilitate future redivision in accordance with the requirements of the zoning district and this title.

[...]

Finding: The proposed partition is designed around two existing residential structures as well as an accessory building (see **Exhibit 4B & 4C**). Parcel 1 would remain a large lot after the proposed partition; however, future division of the site would require the removal of all structures and cooperation with TL 3100 (part of annexation but not partition) in order to extend future right-of-way through the entire subject site no matter the size and shape of this partition. Section 17.152.030(C) is satisfied.

F. All partition proposals shall have adequate drainage provided to reduce exposure to flood damage.

Finding: There is an existing 24-inch PVC stormwater main in SE Elm Street (see **Exhibit 4B**). The applicant is proposing to relocate and install a new catch basin to the west of the driveway serving Parcel 2 (see **Exhibit 4C**) as required. Runoff will be directed towards this relocated catch basin (see **Exhibit 4C**). Section 17.152.030(F) is satisfied.

G. All land partition proposals shall include neighborhood circulation plans that conceptualize future street plans and lot patterns to parcels within five hundred feet of the subject site. Circulation plans address future vehicular/bicycle/pedestrian transportation systems including bike lanes, sidewalks, bicycle/pedestrian paths and destination points. A circulation plan is conceptual in that its adoption does not establish a precise alignment. An applicant for a partition is required to submit a circulation plan unless the applicant demonstrates to the planner one of the following:

- 1. An existing street or proposed new street need not continue beyond the land to be divided in order to complete or extend an appropriate street system or to provide access to adjacent parcels within five hundred feet of the proposed development; or*
- 2. The proposed street layout is consistent with a street pattern adopted as part of the city's transportation system plan, or a previously adopted circulation plan.*

Finding: No circulation plan will be required since no new streets are proposed or warranted at this time in the vicinity of the subject site (see **Exhibit 4G**). Section 17.152.030(G) is not applicable.

17.152.070 Partition approval criteria.

A request to partition land shall meet all of the following criteria:

- A. The proposed partition complies with all statutory and ordinance requirements and regulations;*

Finding: Future review and approval of a final partition plat by the City staff will assure that the proposed partition complies with all statutory and ordinance requirements. The Conditions of Approval will ensure these standards are reviewed and met. Section 17.152.070(A) is satisfied.

- B. Adequate public facilities are available to serve the proposal;*

Finding: The applicant is proposing to utilize the existing 8-inch cast iron water main in SE Elm Street for water services, the 15-inch PVC sewer main in SE Elm Street for sewer services, and the 24-inch PVC stormwater main in SE Elm Street for stormwater management (see **Exhibit 4D**). SE Elm Street is an existing public street that will provide access to each lot (see **Exhibit 4C**). Section 17.152.070(B) is satisfied.

- C. All proposed lots conform to the size and dimensional requirements of this title; and*

Finding: See findings to Section 17.44.050. Section 17.152.070(C) is satisfied.

D. All proposed improvements meet city and applicable agency standards.

Finding: All proposed improvements will meet City standards, as discussed throughout the findings of the staff report and ensured by the Conditions of Approval. The City of Scappoose Public Works Director, Columbia County Building Official, Scappoose Rural Fire Protection District, and Columbia River PUD have been provided an opportunity to review and comment on the proposal. The applicable requirements issued by these agencies are reflected in the Conditions of Approval. Section 17.152.070(D) is satisfied.

E. Streets or roads are laid out so as to conform to the plats of subdivisions and maps of major partitions already approved for adjoining property as to width, general direction and in all other respects, including conformance with submitted neighborhood circulation plans, unless the city determines it is in the public interest to modify the street or road pattern.

Finding: This proposal would not create any new streets (see **Exhibit 4G**) nor does it conflict with the TSP's⁵ projected locations for future streets. Section 17.152.070(E) is satisfied.

17.152.080 Special provisions for lots created through partition process.

A. The minimum width of the building envelope area shall meet the lot requirement of the applicable zoning district.

B. The lot area shall be as required by the applicable zoning district. In the case of a flag lot, the accessway may not be included in the lot area calculation.

Finding: See findings to Section 17.44.050. The proposed partition meets the dimensional requirements of the R-1 zone. Section 17.152.080(A-B) is satisfied.

C. Each lot created through the partition process shall front a public right-of-way as specified by the zoning designation. All flag lots shall be considered to be major variances and shall be subject to planning commission review and approval.

Finding: Both lots would front SE Elm Street (see **Exhibit 4C**). Although TL 3100 is a flag lot, it is existing and not part of this partition so it will not trigger a Major Variance. Section 17.152.080(C) is satisfied.

D. Setbacks shall be as required by the applicable zoning district.

[...]

Finding: See findings to Section 17.44.050. All setback requirements are outright satisfied with the exception of the front setback of the home on Parcel 2. The structure in Parcel 2 currently meets the R-1 standards for front setbacks; however, this development also requires a 10-foot right-of-way dedication, making the distance from the front lot line to the structure substandard.

⁵ City of Scappoose, Transportation System Plan: Volume 1, 2016, Figure 9.

The City is requiring that the applicant fully comply with right-of-way width standards and so the applicant will be held harmless for noncompliant front setbacks on Parcel 2. Section 17.152.080(D) is satisfied.

F. Screening to the standards included in Section 17.100.090, may be required along the property line of a lot of record where the paved drive of an accessway is located within ten feet of an abutting lot. Screening to the standards included in Section 17.100.090 may also be required to maintain privacy for abutting lots and to provide usable outdoor recreation areas for proposed development.

Finding: Both driveways are proposed to be installed within 10 feet of the nearest shared property line (see **Exhibit 4D**). Both shared property lines also have existing fences, already meeting the screening requirements of Chapter 17.100.090. Section 17.152.080(F) is satisfied.

G. The Scappoose fire district may require the installation of a fire hydrant where the length of an accessway would have a detrimental effect on firefighting capabilities and may require provision of an emergency vehicle turnaround.

Finding: The Scappoose Rural Fire Protection District was provided an opportunity to comment on the proposal but did not provide comment. Section 17.152.080(G) is satisfied.

H. No greater than three single-family detached dwelling units may be served by a common drive. Use of a common drive for access to more than three dwelling units other than single-family detached may be required and shall be subject to the approval of the planner, public works director and the planning commission. Where a common drive is to be provided, a reciprocal easement which will ensure access and maintenance rights shall be recorded with the approved partition map.

Finding: The applicant is proposing a shared driveway between Parcel 2 and TL 3100 (see **Exhibit 4D**). The two existing parcels currently share a driveway as well and have a Shared Driveway Agreement (**Exhibit 6**) between the two parties recorded. The recommended conditions of approval will require the applicant to update the Shared Driveway Agreement with updated property information and applicable requirements. Section 17.152.080(H) is satisfied.

I. Any access way shall be paved and shall comply with the standards set forth in public works design standards.

[...]

Finding: The PWDS⁶ allows for residential driveways to be up to 24 feet wide or 28 feet wide if it serves a 3-car garage. The applicant is proposing to install an 18-foot driveway to serve Parcel 1 and a shared 29-foot driveway to serve Parcel 2 and TL 3100 (see **Exhibit 4D**). The Conditions of

⁶ City of Scappoose, Public Works Design Standards, 2014, Section 5.0070, Table 5-1.

Approval will require the applicant to install the proposed driveway in compliance with the PWDS, including width. Section 17.152.080(I) is satisfied.

Chapter 17.154
STREET AND UTILITY IMPROVEMENT STANDARDS

17.154.030 Streets.

A. No development shall occur unless the development has frontage or approved access to a public street:

- 1. Streets within a development and streets adjacent to a development shall be improved in accordance with this title and the public works design standards and specifications.*
- 2. Any new street or additional street width planned as a portion of an approved street plan shall be dedicated and improved in accordance with this title and the public works design standards and specifications.*
- 3. Subject to approval of the city engineer and the planner, the planner may accept and record a non-remonstrance agreement in lieu of street improvements if two or more of the following conditions exist:*
 - a. A partial improvement is not feasible due to the inability to achieve a cohesive design for the overall street;*
 - b. A partial improvement may create a potential safety hazard to motorists or pedestrians;*
 - c. Due to the nature of existing development on adjacent properties it is unlikely that street improvements would be extended in the foreseeable future and the improvement associated with the project under review does not, by itself, provide a significant improvement to street safety or capacity;*
 - d. The improvement would be in conflict with an adopted capital improvement plan;*
 - e. Additional planning work is required to define the appropriate design standards for the street and the application is for a project which would contribute only a minor portion of the anticipated future traffic on the street.*

[...]

Finding: All proposed parcels of the subject site have frontage on SE Elm Street (see **Exhibit 4B**). This section of SE Elm Street is classified by the TSP⁷ as a Neighborhood Route, which requires 60 feet of right-of-way width comprised of 36 feet of vehicular travel way & parking, two 5.5-foot planter strips, two 6-foot sidewalks, and two 6-inch utility areas. The SE Elm Street right-of-way is primarily 50 feet wide with a ~29-foot paved width (see **Exhibit 4B**). There are sidewalks on the north side of SE Elm Street and to the east and west of the subject frontage but no sidewalk or curb on the subject site's frontage. The applicant is proposing to provide a 10-foot dedication along TL 3000 to bring the right-of-way width to standard, install a 6-foot sidewalk with curb and gutter along the entire frontage including two new driveway approaches, pave between the

⁷ City of Scappoose, Transportation System Plan: Volume 1, 2016, Figures 12 & 13b.

existing asphalt concrete and the new curb, and plant street trees (see **Exhibit 4D**). The applicant did not propose a dedication along the frontage of TL 3100 (see **Exhibit 4C**) since it is not part of the Minor Partition proposal, which is the element of the consolidated application that requires the dedication along the frontage of TL 3000. However, the applicant is proposing to install a sidewalk that would go through what is currently and proposed to remain as private property along the frontage of TL 3100 (see **Exhibit 4D**). The applicant will be required by the recommended conditions of approval to dedicate 10 feet of TL 3100 as right-of-way or record a public sidewalk easement in the area that would be dedicated that allows for the public to use the sidewalk as a public walkway as if it were in the right-of-way. Section 17.154.030(A) is satisfied.

C. The planning commission may approve an access easement established by deed without full compliance with this title provided such an easement is the only reasonable method by which a lot large enough to develop can develop:

[...]

Finding: The applicant is not proposing an access easement (see **Exhibit 4G**) nor will one be required since all parcels front on a public road. Section 17.154.030(C) is satisfied.

D. The location, width and grade of all streets shall conform to an approved street plan and shall be considered in their relation to existing and planned streets, to topographic conditions, to public convenience and safety, and in their appropriate relation to the proposed use of the land to be served by such streets:

1. *Street grades shall be approved by the public works director in accordance with the city's public works design standards; and*
2. *Where the location of a street is not shown in an approved street plan, the arrangement of streets in a development shall either:*
 - a. *Provide for the continuation or appropriate projection of existing streets in the surrounding areas, or*
 - b. *Conform to a plan adopted by the council, if it is impractical to conform to existing street patterns because of particular topographical or other existing conditions of the land. Such a plan shall be based on the type of land use to be served, the volume of traffic, the capacity of adjoining streets and the need for public convenience and safety.*
3. *New streets shall be laid out to provide reasonably direct and convenient routes for walking and cycling within neighborhoods and accessing adjacent development.*

E. The street right-of-way and roadway widths shall not be less than the minimum widths described in the city's public works design standards.

[...]

Finding: All proposed parcels of the subject site have frontage on SE Elm Street (see **Exhibit 4B**).

This section of SE Elm Street is classified by the TSP⁸ as a Neighborhood Route, which requires 60 feet of right-of-way width and a 36-foot paved section. The SE Elm Street right-of-way is primarily 50 feet wide with a ~29-foot paved width (see **Exhibit 4B**). The applicant is proposing to provide a 10-foot dedication along TL 3000 to bring the right-of-way width to standard and complete half-street improvements to provide a 19-foot paved section on the southern half of SE Elm Street (see **Exhibit 4D**). The applicant did not propose a dedication along the frontage of TL 3100 (see **Exhibit 4C**) since it is not part of the Minor Partition proposal, which is the element of the consolidated application that requires the dedication along the frontage of TL 3000. However, the applicant is proposing to install a sidewalk that would go through what is currently and proposed to remain as private property along the frontage of TL 3100 (see **Exhibit 4D**). The applicant will be required by the recommended conditions of approval to dedicate 10 feet of TL 3100 as right-of-way or record a public sidewalk easement in the area that would be dedicated that allows for the public to use the sidewalk as a public walkway as if it were in the right-of-way. Section 17.154.030 (D-E) is satisfied.

H. Concrete vertical curbs, curb cuts, wheelchair, bicycle ramps and driveway approaches shall be constructed in accordance with standards specified in this chapter and the city's public works design standards. Concrete curbs and driveway approaches are required and shall be built to the city's configuration standards.

[...]

Finding: The PWDS⁹ allows for residential driveways to be up to 24 feet wide or 28 feet wide if it serves a 3-car garage. The applicant is proposing to install an 18-foot driveway to serve Parcel 1 and a shared 29-foot driveway to serve Parcel 2 and TL 3100 (see **Exhibit 4D**). The Conditions of Approval will require the applicant to install the proposed driveways in compliance with the PWDS, including width. Section 17.154.030(H) is satisfied.

K. Upon completion of a street improvement and prior to acceptance by the city, it shall be the responsibility of the developer's registered professional land surveyor to provide certification to the city that all boundary and interior monuments shall be established or re-established, protected and recorded.

[...]

Finding: In addition to street improvements, the applicant is providing a right-of-way dedication that will adjust the boundary lines and therefore, the property pins (see **Exhibit 4D**). The applicant will be required by the recommended conditions of approval to have their surveyor verify that the property pins are appropriately located following the completion of street improvements and the dedication. Section 17.154.030(K) is satisfied.

O. The developer shall install all street signs, relative to traffic control and street names, as specified by the public works director for any development. The cost of signs shall be the

⁸ City of Scappoose, Transportation System Plan: Volume 1, 2016, Figures 12 & 13b.

⁹ City of Scappoose, Public Works Design Standards, 2014, Section 5.0070, Table 5-1.

responsibility of the developer.

Finding: The Conditions of Approval state that any street signs required by the City Engineer will be installed at the cost and labor of the applicant. Section 17.154.030(O) is satisfied.

P. Joint mailbox facilities shall be provided in all residential developments, with each joint mailbox serving at least two dwelling units.

1. *Joint mailbox structures shall be placed adjacent to roadway curbs and shall comply with provisions of the Americans with Disabilities Act and implementing federal and state regulations;*
2. *Proposed locations of joint mailboxes shall be designated on a copy of the tentative plan, and shall be approved by the U.S. Post Office prior to plan approval; and*
3. *Plans for the joint mailbox structures to be used shall be submitted for approval by the planner prior to final approval.*

Finding: The applicant has proposed to establish a new shared mailbox in front of Parcel 2 to be used by all three parcels (see **Exhibit 4D**). The final location is subject to the approval of the Scappoose Post Office. Section 17.154.030(P) is satisfied.

Q. The location of traffic signals shall be noted on approved street plans, and where a proposed street intersection will result in an immediate need for a traffic signal, a city-approved signal shall be installed. The cost shall be included as a condition of development.

Finding: The TSP¹⁰ does not indicate the need for a traffic signal at the SE Elm Street and SE Tussing Way intersection so none will be required. Section 17.154.030(Q) is satisfied.

R. Street lights shall be installed in accordance with the city's public works design standards.

Finding: This development requires the installation of one streetlight. The applicant is proposing to install one streetlight in front of Parcel 2 (see **Exhibit 4D**). The applicant will be required to install the streetlight using Columbia River PUD approved luminaires in accordance with PWDS and IES standards. Section 17.154.030(R) is satisfied.

S. A Transportation Impact Study (TIS) must be submitted with a land use application if the conditions in (1) or (2) apply in order to determine whether conditions are needed to protect and minimize impacts to transportation facilities, consistent with Section 660-012-0045(2)(b) and (e) of the State Transportation Planning Rule.

[...]

Finding: Applications for Annexation, Zone Change, or Minor Partition do not require a TIS; see Sections 17.136.050 and 17.152.100. There are no new uses or trips that will be generated as

¹⁰ City of Scappoose, Transportation System Plan: Volume 1, 2016, pp 25-35.

part of this development (see **Exhibit 4C**). Section 17.154.030(S) is not applicable.

17.154.050 Easements.

A. Easements for sewers, drainage, water mains electric lines or other public utilities shall be either dedicated or provided for in the deed restrictions, and where a subdivision is traversed by a watercourse, drainageway, channel or stream, there shall be provided a stormwater easement or drainage right-of-way conforming substantially with lines of such watercourse and such further width as will be adequate for conveyance and maintenance.

B. A property owner proposing a development shall make arrangements with the city, the applicable district and each utility franchise for the provision and dedication of utility easements necessary to provide full services to the development.

Finding: The applicant is proposing an 8-foot public utility easement (PUE) on the TL 3000 frontage (see **Exhibit 4G**). The Conditions of Approval will require the applicant to depict an 8-foot PUE on the final partition plat. If the applicant does not provide a dedication of TL 3100, they will be required by the recommended conditions of approval to record a public sidewalk easement in the area that would be dedicated that allows for the public to use the sidewalk as a public walkway as if it were in the right-of-way. Section 17.154.050 is satisfied.

17.154.070 Sidewalks.

A. Sidewalks are required and shall be constructed, replaced or repaired in accordance with the city's public works design standards.

B. Maintenance of sidewalks and curbs in the continuing obligation of the adjacent property owner.

C. Subject to approval by the public works director and planner, planner may accept and record a nonremonstrance agreement for the required sidewalks from the applicant for a building permit for a single-family residence when the public works director determines the construction of the sidewalk is impractical for one or more of the following reasons:

- 1. The residence is an in-fill property in an existing neighborhood and adjacent residences do not have sidewalks;*
- 2. Sidewalk grades have not and will not be established for the property in question within a one-year period;*
- 3. Topography or elevation of the sidewalk base area makes construction of a sidewalk impractical.*

D. In the event one or more of the following situations are found by the council to exist, the council may adopt a resolution to initiate construction of a sidewalk in accordance with city ordinances:

- 1. A safety hazard exists for children walking to or from school and sidewalks are necessary to eliminate the hazard;*
- 2. A safety hazard exists for pedestrians walking to or from a public building, commercial area, place of assembly or other general pedestrian traffic, and sidewalks are necessary to eliminate the hazard;*
- 3. Fifty percent or more of the area in a given block has been improved by the construction of dwellings, multiple dwellings, commercial buildings or public buildings and/or parks.*

Finding: SE Elm Street is classified by the TSP¹¹ as a Neighborhood Route, which requires 6-foot-wide sidewalks. The applicant is proposing to install 6-foot sidewalks with curb and gutter along the entire frontage of Parcels 1 and 2 and TL 3100 (see **Exhibit 4D**). The maintenance of sidewalks and curbs will be the continuing obligation of the property owner. The applicant is not proposing a non-remonstrance agreement. Section 17.154.070 is satisfied.

17.154.090 Sanitary Sewers.

- A. Sanitary sewers shall be installed to serve each new development and to connect developments to existing mains in accordance with the provisions set forth by the city's public works design standards and the adopted policies of the comprehensive plan.*
- B. The public works director shall approve all sanitary sewer plans and proposed systems prior to issuance of development permits involving sewer service.*
- C. Proposed sewer systems shall include consideration of additional development within the area as projected by the comprehensive plan and the wastewater treatment facility plan and potential flow upstream in the sewer sub-basin.*
- D. Applications shall be denied by the approval authority where a deficiency exists in the existing sewer system or portion thereof which cannot be rectified within the development and which if not rectified will result in a threat to public health or safety, surcharging of existing mains, or violations of state or federal standards pertaining to operation of the sewage treatment system.*

Finding: There is currently a 15-inch PVC sewer main in SE Elm Street as well as a 12-inch concrete sewer main that extends south from the 15-inch PVC main into and through the rear yards of the parcels to the west of the subject site (see **Exhibit 4B**). The applicant proposes installing three new sewer laterals from the SE Elm Street main to serve each of the existing homes (see **Exhibit 4C**). The applicant also indicated on their Proposed Conditions Plan (**Exhibit 4C**) that the contractor may evaluate the feasibility of serving the home on TL 3100 with the sewer main to the west of the subject site that runs through the rear yards of the adjacent homes in the Elm Crossing Subdivision instead of the main in SE Elm Street. The City of Scappoose Public Works Director provided a referral comment (**Exhibit 7**) stating that they have reviewed the application and have no objection to its approval as submitted provided it meets the applicable criteria. Section 17.154.090 is satisfied.

17.154.100 Storm Drainage.

- A. The planner and public works director shall issue permits only where adequate provisions for stormwater and floodwater runoff have been made, and:*
 - 1. The stormwater drainage system shall be separate and independent of any sanitary sewage system.*
 - 2. Where possible, inlets shall be provided so surface water is not carried across any intersection or allowed to flood any street.*
 - 3. Surface water drainage patterns shall be shown on every development proposal plan.*
 - 4. All stormwater analysis and calculations shall be submitted with proposed plans for*

¹¹ City of Scappoose, Transportation System Plan: Volume 1, 2016, Figures 12 & 13b.

public works directors review and approval.

5. All stormwater construction materials shall be subject to approval of the public works director.

B. Where a subdivision is traversed by a watercourse, drainageway, channel or stream, there shall be provided a stormwater easement or drainage right-of-way conforming substantially with the lines of such watercourse and such further width as will be adequate for conveyance and maintenance.

C. A culvert or other drainage facility shall, and in each case be, large enough to accommodate potential runoff from its entire upstream drainage area, whether inside or outside the development. The public works director shall determine the necessary size of the facility.

D. Where it is anticipated by the public works director that the additional runoff resulting from the development will overload an existing drainage facility, the planner and engineer shall withhold approval of the development until provisions have been made for improvement of the potential condition or until provisions have been made for storage of additional runoff caused by the development.

Finding: There is an existing 24-inch PVC stormwater main in SE Elm Street (see **Exhibit 4B**). The applicant is proposing to relocate and install a new catch basin to the west of the driveway serving Parcel 2 (see **Exhibit 4C**) as required. Runoff will be directed towards this relocated catch basin (see **Exhibit 4C**). The City of Scappoose Public Works Director provided a referral comment (**Exhibit 7**) stating that they have reviewed the application and have no objection to its approval as submitted provided it meets the applicable criteria. Section 17.154.100 is satisfied.

17.154.105 Water System.

The planner and public works director shall issue permits only where provisions for municipal water system extensions have been made, and:

A. Any water system extension shall be designed in compliance with the comprehensive plan existing water system plans.

B. Extensions shall be made in such a manner as to provide for adequate flow and gridding of the system.

C. The public works director shall approve all water system construction materials.

Finding: There is currently an 8-inch cast iron water main in SE Elm Street. The applicant proposes to install two new water laterals from the main to serve Parcels 1 and 2 (see **Exhibit 4C**). TL 3100 will continue to use its existing well as its water source (see **Exhibit 3, p. 24**). The City of Scappoose Public Works Director provided a referral comment (**Exhibit 7**) stating that they have reviewed the application and have no objection to its approval as submitted provided it meets the applicable criteria. Section 17.154.105 is satisfied.

17.154.110 Bikeways.

A. Developments adjoining proposed bikeways shall include provisions for the future extension of such bikeways through the dedication of easements or rights-of-way.

B. Where possible, bikeways should be separated from other modes of travel including

pedestrians.

C. Minimum width for bikeways is four paved feet per travel lane.

Finding: According to the TSP¹², there are no proposed bicycle routes along the applicable section of SE Elm Street so bikeways will not be required. Section 17.154.110 is satisfied.

17.154.120 Utilities.

A. All utility lines including, but not limited to those required for electric, communication, lighting and cable television services and related facilities shall be placed underground, except for surface mounted transformers, surface mounted connection boxes and meter cabinets which may be placed above ground, temporary utility service facilities during construction, high capacity electric lines operating at fifty thousand volts or above, and:

- 1. The applicant shall make all necessary arrangements with the serving utility to provide the underground services;*
- 2. The city reserves the right to approve location of all surface mounted facilities;*
- 3. All underground utilities, including sanitary sewers, water lines, and storm drains installed in streets by the applicant, shall be constructed prior to the surfacing of the streets; and*
- 4. Stubs for service connections shall be long enough to avoid disturbing the street improvements when service connections are made.*

B. The applicant for a subdivision shall show on the development plan or in the explanatory information, easements for all underground utility facilities, and:

- 1. Plans showing the location of all underground facilities as described herein shall be submitted to the public works director for review and approval; and*
- 2. Above ground equipment shall not obstruct visual clearance areas for vehicular traffic.*

Finding: There are no overhead utility lines running along the subject site's frontage. The applicant is proposing an 8-foot PUE on the TL 3000 frontage (see **Exhibit 4G**). The Conditions of Approval will require the applicant to depict an 8-foot PUE on the final partition plat. Section 17.154.120 is satisfied.

Chapter 17.162
PROCEDURES FOR DECISION MAKING–QUASI-JUDICIAL

17.162.021 Consolidation of proceedings.

A. Except as provided in subsection C of this section, whenever an applicant requests more than one approval and more than one approval authority is required to decide the applications, the proceedings shall be consolidated so that one approval authority shall decide all applications in one proceeding.

B. In such cases as stated in subsection A of this section, the hearings shall be held by the approval authority having original jurisdiction over one of the applications under Section 17.164.110, in the

¹² City of Scappoose, Transportation System Plan: Volume 1, 2016, Figure 11.

following order of preference: the council, the commission, or the planner.

C. Where there is a consolidation of proceedings:

- 1. The notice shall identify each action to be taken;*
- 2. The decision on a plan map amendment shall precede the decision on the proposed zone change and other actions. Plan map amendments are not subject to the one hundred twenty-day decision making period prescribed by state law and such amendments may involve complex issues. Therefore, the planner shall not be required to consolidate a plan map amendment and a zone change or other permit applications requested unless the applicant requests the proceedings be consolidated and signs a waiver of the one hundred twenty-day time limit prescribed by state law for zone change and permit applications; and*
- 3. Separate actions shall be taken on each application.*

D. Consolidated Permit Procedure.

- 1. Use of the consolidated permit procedures described in this section shall be at the election of the applicant.*
- 2. When the consolidated procedure is elected, application and fee requirements shall remain as provided by resolution approved by the council. If more than one permit is required by this title or other ordinance to be heard by the planning commission or city council, each such hearing shall be combined with any other permit also requiring such hearing. The standards applicable to each permit by this or any other ordinance shall be applied in the consolidated procedures to each application.*
- 3. In a consolidated proceeding, the staff report and recommendation provided by the planner shall be consolidated into a single report.*
- 4. All rules and ordinances of the city not in conflict with this section shall apply in a consolidated permit procedure.*

Finding: The applicant has requested a consolidated application for Annexation (**Exhibit 2A**), Zone Change (**Exhibit 3, p. 7**), and Minor Partition (**Exhibit 2B**). On their own, these application types would have different approval authorities with MiP being reviewed by the Planner and ANX and ZC being reviewed by the City Council. However, since the applicant submitted a consolidated application, the entire consolidated application will be reviewed for approval by the City Council, as the higher approval authority. Approving MiP is a limited land use decision subject to Chapter 17.164 and Oregon Revised Statutes (ORS) 197.195. Approving the other application types are quasi-judicial decisions subject to Chapter 17.162. The consolidated application will be processed as a quasi-judicial decision since it provides more opportunities for public participation and the procedures for quasi-judicial decisions necessitate public hearings, which are not allowed in limited land use decisions. Section 17.162.021 is satisfied.

17.162.025 Noticing Requirements

A. Notice of a pending quasi-judicial public hearing shall be given by the planner in the following manner:

- 1. At least twenty days prior to the scheduled hearing date, or if two or more hearings are scheduled, ten days prior to the first hearing, notice shall be sent by mail to:*

- a. The applicant and all owners or contract purchasers of record of the property which is the subject of the application;*
 - b. All property owners of record or the most recent property tax assessment roll within three hundred feet of the property which is the subject of the notice plus any properties abutting proposed off-site improvements.*
 - c. Any governmental agency or utility whose property, services or facilities may be affected by the decision. The reviewing City Staff shall determine the extent of notice to public agencies or utilities based on perceived interest or impact; noticed agencies may include:*
 - i. Columbia County Land Development Services;*
 - ii. Columbia County Road Department;*
 - iii. Oregon Department of Transportation (ODOT);*
 - iv. ODOT Rail Division;*
 - v. Portland & Western Railroad;*
 - vi. Scappoose Rural Fire Protection District;*
 - vii. Port of St. Helens;*
 - viii. Oregon Department of Aviation;*
 - ix. Scappoose School District;*
 - x. Columbia County Soil Conservation District;*
 - xi. Scappoose Drainage Improvement Company; or*
 - xii. Any other affected agencies as identified by the planner;*
 - d. Acknowledged neighborhood planning organizations, if active;*
 - e. Any person who requests, in writing; and*
 - f. The appellant and all parties to an appeal.*
- 2. At least thirty-five days before the initial hearing on adoption of any proposal to amend the comprehensive plan map or zoning map, notice shall be sent to the Department of Land Conservation and Development;*
- 3. Notice of a hearing on a proposed zone change for a manufactured home park shall be given to tenants of that manufactured home park at least twenty days but no more than forty days prior to the hearing; and*
- 4. The planner shall cause an affidavit of mailing of notice to be filed and made a part of the administrative record.*
- B. For all quasi-judicial decisions requiring a public hearing, the applicant shall post signs provided by the planner displaying notice of the pending hearing at least fourteen days prior to the date of the hearing. One sign shall be required for each three hundred feet, or part thereof, of frontage of the subject property on any street. The content, design, size and location of the signs shall be as determined by the planner to assure that the information is legible from the public right-of-way. As a precondition to a hearing, the applicant shall file an affidavit of such posting with the planner no less than ten days prior to the hearing.*
- C. For all quasi-judicial decisions requiring a public hearing, at least ten days prior to the hearing, notice shall be given in a newspaper of general circulation in the city. An affidavit of publication shall be made part of the administrative record.*

Finding: Using the most recent property tax assessment roll, notice of this application was mailed to every property owner within 300 feet of the entire subject site on December 1, 2025. The applicant has provided a signed affidavit certifying that onsite noticing has been posted as of November 10, 2025, consistent with the requirements of this section. Notice of the hearing was published in the November 28, 2025 edition of the Columbia County Spotlight. The public has until December 10, 2025 at 5:00 pm to provide a written public comment. As of the date of this report, no members of the public have submitted written comment. A land use action referral was sent to the City of Scappoose City Manager, Public Works Director, and Police Chief; Columbia County Public Works Director, Planning Director, and Building Official; Scappoose Rural Fire Protection District; and Columbia River PUD. The City Engineer's comments are incorporated throughout this report. Section 17.162.025 is satisfied.

17.162.090 Approval authority responsibilities.

[...]

D. Upon appeal or recommendation, the city council shall conduct a public hearing in the manner prescribed by this chapter and shall have the authority to approve, deny or approve with conditions the following development applications:

- 1. Annexations and the formal imposition of plan and zone designations made to lands annexed to the city;*
- 2. Quasi-judicial plan and zone amendments, including overlay zones;*

[...]

Finding: See findings to Section 17.162.021. Since this application includes a proposal for ANX and ZC, City Council will be the approval authority for the entire consolidated application. Staff has written this staff report and will present it to the Planning Commission, who will make a recommendation to the City Council, who will be the final decision maker. Section 17.162.090 is satisfied.

17.162.140 Decision process.

A. The decision shall be based on:

- 1. Proof by the applicant that the application fully complies with:*
 - a. Applicable policies of the city comprehensive plan; and*
 - b. The relevant approval standards found in the applicable chapter(s) of this title, the public works design standards, and other applicable implementing ordinances.*

B. Consideration may also be given to:

- 1. Proof of a substantial change in circumstances or a mistake in the comprehensive plan or zoning map as it relates to the property which is the subject of the development application; and*
- 2. Factual oral testimony or written statements from the parties, other persons and other governmental agencies relevant to the existing conditions, other applicable standards and criteria, possible negative or positive attributes of the proposal or factors in subsections (A) or (B)(1) of this section.*

[...]

Finding: The applicant has submitted a complete proposal for Annexation, Zone Change, and Minor Partition. Findings related to the approval criteria have been addressed within this staff report. The recommended conditions of approval are included to ensure the satisfaction of all applicable approval criteria and the requirements of other governmental agencies. Section 17.162.140 is satisfied.

The following sections of the Scappoose Comprehensive Plan are applicable to this request:

POLICIES FOR THE URBAN GROWTH BOUNDARY

[...]

6) *Approve annexations of residential lands, except in the cases of health hazards, when:*

- A) *There is sufficient capacity in the sewer, water, street, school, police and fire systems to service the potential additional populace.*

Finding: Existing municipal police and utility services can be made available to the site immediately and the applicant is proposing connection to City water and sewer upon annexation (see **Exhibit 4D**). The property is already located within the Scappoose Rural Fire Protection District, Scappoose School District, and Scappoose Library District service boundaries. Telecommunication and electric services are currently serving the subject site, demonstrating that adequate capacity exists to do so. Being in the UGB, the subject site has already been considered in all the City's master plans. The City of Scappoose Public Works Director, Scappoose School District, Scappoose Police Department, and Scappoose Rural Fire Protection District, all had the opportunity to provide comment on the proposal. The City of Scappoose Public Works Director stated in their referral comment (**Exhibit 7**) that they have no objections to this proposal, indicating that the requested services are available to serve the site. The other agencies did not provide a referral comment. The Police Department and Fire District did not provide comment. Comprehensive Plan Policies for the Urban Growth Boundary #6(A) is satisfied.

- B) *Sufficient in-filling of vacant land has occurred to warrant an expansion.*

[...]

Finding: The area surrounding the subject site to its immediate north, west, and east are all in city limits and fully built out at urban densities. Several vacant residential sites throughout the city are currently under construction or in development review, providing further infilling within city limits. Approving this annexation supports the pattern of growth in the immediate vicinity and the city at large and allows for future redevelopment of the annexation area at urban densities. Comprehensive Plan Policies for the Urban Growth Boundary #6(B) is satisfied.

The following sections of the Oregon Administrative Rules are applicable to this request:

Chapter 660, Division 12
TRANSPORTATION PLANNING

660-012-0060 Plan and Land Use Regulation Amendments

(1) If an amendment to a functional plan, an acknowledged comprehensive plan, or a land use regulation (including a zoning map) would significantly affect an existing or planned transportation facility, then the local government must put in place measures as provided in section (2) of this rule, unless the amendment is allowed under section (3), (9) or (10) of this rule. A plan or land use regulation amendment significantly affects a transportation facility if it would:

(a) Change the functional classification of an existing or planned transportation facility (exclusive of correction of map errors in an adopted plan);

Finding: The proposed Annexation and Zone Change will not necessitate changes to the functional classification of existing or planned transportation facilities. OAR 660-012-0060(1)(a) is satisfied.

(b) Change standards implementing a functional classification system; or

Finding: The proposed Annexation and Zone Change will not change any standards implementing the functional classification system. OAR 660-012-0060(1)(b) is satisfied.

(c) Result in any of the effects listed in paragraphs (A) through (C) of this subsection based on projected conditions measured at the end of the planning period identified in the adopted TSP. As part of evaluating projected conditions, the amount of traffic projected to be generated within the area of the amendment may be reduced if the amendment includes an enforceable, ongoing requirement that would demonstrably limit traffic generation, including, but not limited to, transportation demand management. This reduction may diminish or completely eliminate the significant effect of the amendment.

(A) Types or levels of travel or access that are inconsistent with the functional classification of an existing or planned transportation facility;

(B) Degrade the performance of an existing or planned transportation facility such that it would not meet the performance standards identified in the TSP or comprehensive plan; or

(C) Degrade the performance of an existing or planned transportation facility that is otherwise projected to not meet the performance standards identified in the TSP or comprehensive plan.

[...]

(9) Notwithstanding section (1) of this rule, a local government may find that an amendment to a zoning map does not significantly affect an existing or planned transportation facility if all of the following requirements are met.

(a) The proposed zoning is consistent with the existing comprehensive plan map designation and the amendment does not change the comprehensive plan map;

(b) The local government has an acknowledged TSP and the proposed zoning is consistent with the TSP; and

(c) The area subject to the zoning map amendment was not exempted from this rule at the time

of an urban growth boundary amendment as permitted in OAR 660-024-0020(1)(d), or the area was exempted from this rule but the local government has a subsequently acknowledged TSP amendment that accounted for urbanization of the area.

Finding: The City's TSP assumed that development on this site would be under the City's Suburban Residential and Low Density Residential Comprehensive Plan and zoning designations, respectively, and street functional classifications were established accordingly. This proposal does not "significantly affect" an existing or planned transportation facility (as defined by the Transportation Planning Rule) because the annexation and zoning are consistent with the TSP's traffic assumptions and meet criteria above. Accordingly, the City can conclude that the proposed Zone Change does not have a significant effect on the existing transportation system and a TPR analysis will not be required by the applicant. OAR 660-012-0060(1(c)-9) is satisfied.

RECOMMENDATION

Based on the Findings of Fact and the materials submitted by the applicant, staff recommends that the Planning Commission recommend that the City Council **approve ANX 1-25, ZC 2-25, MiP 1-25**, subject to the following conditions:

GENERAL

1. Approval of the consolidated decision will be effective 30 days after the passage of the ordinance.
2. Approval of this preliminary partition plat shall be effective for a period of one year from the date of City Council approval.

PRIOR TO CONSTRUCTION

3. The applicant shall submit construction documents (Plans) in accordance with the Conditions of Approval for streets, utilities, and other public infrastructure that have been prepared and stamped by a registered professional engineer, licensed in the State of Oregon. The Plans shall adhere to the applicable Scappoose Municipal Codes, utility Master Plans, and Scappoose Public Works Design Standards. All applicable improvements shown in the construction documents shall be referenced vertically to the NAVD 88 datum and horizontally to the NAD 1983 HARN State Plane Oregon North FIPS 3601 (Intl Feet).
4. The applicant shall obtain a National Pollutant Discharge Elimination System (NPDES) 1200-C permit from the Oregon Department of Environmental Quality, a Right of Way Permit, and a Grading Permit from the City of Scappoose, if applicable, and attend a Pre-construction Meeting with the City prior to any work. A copy of the approved NPDES permit shall be submitted to the City Engineer prior to approval of the Right-of-Way and Grading Permits, and prior to issuance of the Notice to Proceed for construction. Provide erosion control measures meeting the requirements of the City of Scappoose Public

Works Design Standards, Section 2.0051.

5. The applicant shall provide computations to the Building Official demonstrating adequate domestic water lateral sizing for the development.
6. The applicant shall obtain a Right of Way permit prior to being issued the Notice to Proceed for Type III work including the contact information of the owner and general contractor and pay inspection fees to the City of Scappoose.
7. The applicant shall obtain a Grading permit prior to issuing the Notice to Proceed listing the quantities of the cuts and fills and pay the associated fees to the City of Scappoose.
8. The applicant shall pay all system development charges, connection fees, building permit fees, and any other applicable fees at the time of building permit (for the utility connections) issuance.
9. The applicant shall sign an Improvement Agreement with the City for the proposed improvements and submit a performance bond and maintenance bond in accordance with Chapter 17.154.

CONSTRUCTION REQUIREMENTS

10. The applicant shall submit to the City Engineer a stormwater memorandum, documenting the improvements and analysis regarding the proposed treatment and conveyance in accordance with the Scappoose Public Works Design Standards. Clean Water Services or City of Portland standards are acceptable treatment methods. Any new or existing drywells (if utilized) will be required to be registered with the DEQ.
11. The applicant shall complete half-street improvements along the subject site's frontage, including sidewalks, curb/gutter, street trees, and asphalt concrete paving between the existing asphalt concrete and the new curb. All work shall be completed in conformance with the Scappoose Public Works Design Standards and Scappoose Development Code.
12. The applicant shall install, upgrade, or remove any street signs as required by the Manual on Uniform Traffic Control Devices, Scappoose Municipal Code, or the City Engineer, at the applicant's cost and labor.
13. The applicant shall install one new streetlight on the subject site's frontage, in accordance with the Scappoose Public Works Design Standards and Illuminating Engineering Society standards.
14. The applicant shall plant street trees on their SE Elm Street frontage in accordance with Section 13.28.010(C) and Chapter 17.104. The street trees shall be a species listed on the Approved Street Tree list on file with the Planning Department. The final construction plans shall provide a detail for root guard to protect sidewalks and other surroundings. At

the time of planting, all street trees shall have a 2-inch minimum caliper, a height no less than 10 feet if they are deciduous and 5 feet if they are evergreen, and be spaced as appropriate for the selected species, as specified in the Approved Street Tree list. Street trees shall not be planted in areas where there is a conflict with any below ground utility line. All street trees shall be of good quality, with appropriate staking and conform to the American Standard for Nursery Stock (ANSI Z60.1). The Planner reserves the right to reject any plant material that does not meet this standard.

15. The applicant shall submit final asbuilt plans based on any contractor markups including electronic (AutoCAD) files for GIS updates for the City's review and approval in accordance with the Scappoose Public Works Design Standards prior to final signoff and release of the performance bond.
16. The applicant shall comply with Scappoose Rural Fire Protection District code regarding site access to Tax Lot 3100.

PRIOR TO FINAL PLAT RECORDING

17. The applicant shall have all street improvements, including sidewalks, curb/gutter, street trees, and asphalt concrete paving between the existing asphalt concrete and the new curb, completed to the satisfaction of the City.
18. The applicant shall install municipal water and sewer services to serve Parcels 1 and 2 to the satisfaction of the City.
19. The applicant shall prepare a final partition plat that demonstrates full compliance with the dimensional requirements specified in Section 17.44.050 and that conforms to the requirements of Oregon Revised Statutes Chapter 92, Subdivisions and Partitions. The plat shall include a note that states "This Plat is subject to the Conditions of Approval imposed by the City of Scappoose for local file # MiP 1-25."
20. The applicant shall depict on the final partition plat an 8-foot public utility easement along the northern frontage of Parcels 1 and 2.
21. The applicant shall depict easements for any utilities (public or franchise) that run through multiple properties and cross property lines on the final plat. Any easements which allow access and maintenance of private drainage lines or other common elements, and their associated appurtenances shall meet the applicable requirements of the developer and the Oregon Structural Specialty Code and Oregon Plumbing Specialty Code, whichever is a higher standard.
22. The applicant shall provide a 10-foot right-of-way dedication of Tax Lot 3000 along the SE Elm Street frontage to bring the right-of-way width to a Neighborhood Route standard. This dedication shall be reflected and referenced on the final partition plat.

23. The applicant shall provide a 10-foot right-of-way dedication of Tax Lot 3100 along the SE Elm Street frontage to bring the right-of-way width to a Neighborhood Route standard or record a public sidewalk easement in the area that would be dedicated that allows for the public to use the sidewalk as a public walkway as if it were in the right-of-way.
24. The applicant shall update the Shared Driveway Agreement with updated property information and applicable requirements.
25. The applicant shall have their professional land surveyor verify that the property pins are appropriately located following the completion of street improvements and the right-of-way dedication.
26. The applicant shall submit an electronic copy of the draft Final Partition Plat to the City for review and approval prior to submitting the Plat to Columbia County. After City approval of the Final Plat, the Plat shall be recorded with Columbia County. An electronic copy of the recorded plat shall be provided to the City within 15 days of recording.

Garver & Watt Annexation & Partition Vicinity Map

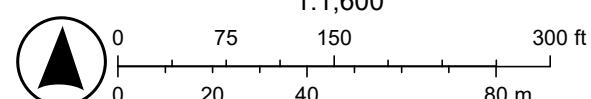


Taxlots with labels

Streets

City Limits Boundary

Subject site



GeoTerra, Frontier, Sources: Esri, TomTom, Garmin, FAO, NOAA, USGS, © OpenStreetMap contributors, and the GIS User Community



Scappoose Planning Department
 33568 E. Columbia Ave. Scappoose, OR 97056
 Phone: 503-543-7184 Fax: 503-543-7182
www.scappoose.gov

ANNEXATION APPLICATION

NOTICE TO APPLICANT: On original application form, please print legibly using black/dark blue ink or type. Applicants are advised to review the list of submittal requirements and recommendations indicated on each land use application form and in the applicable code section prior to submitting an application. Applicants are also advised to schedule a pre-application meeting with staff prior to submitting final application. **INCOMPLETE APPLICATIONS WILL NOT BE PROCESSED UNTIL THE PLANNING DEPARTMENT RECEIVES ALL REQUIRED SUBMITTAL MATERIALS. REFER TO SUBMITTAL CHECKLIST PAGE.**

TRACKING INFORMATION (For Office Use Only)

| | | |
|---|---|------------------------------|
| Application Submittal Includes: | | |
| <input type="checkbox"/> 2 Hard Copies Required (Initial Submittal) | <input type="checkbox"/> Electronic Submittal | <input type="checkbox"/> Fee |
| <input type="checkbox"/> 7 Hard Copies Required (Final Submittal, once deemed complete by City Planner) | | |
| Date Submitted with payment: _____ | | Receipt #: _____ |
| File # _____ | Hearing Date _____ | |

SITE LOCATION & DESCRIPTION

Tax Map #(s) 3107-CC Tax Lot #(s) 3000, 3100

Frontage Street or Address 34094 SE Elm St. (lot 3000); 34102 SE Elm St. (lot 3100)

Nearest Cross Street SE Tussing Way, SE 9th Street

Current County Zoning R-10 City Comprehensive Plan Designation SR, Suburban Residential
 .75 (3000) Lot 3000: 155' x 202'
 Site Size 1.3 (3100) acres sq. ft. Dimensions Lot 3100: 1701' x 304' + flag portion (16' x 212')

REASON FOR REQUEST (If for utility connection, cite which utility.) Lot 3100: To connect to city sewer (failing septic)
Lot 3000: To connect to city sewer and water, and to partition into two smaller lots

OWNERSHIP AND APPLICANT INFORMATION (Property owner signature must be a wet-ink signature. If the property is under-going a change of ownership, proof of purchase or purchase contract must be provided if property owner of record is not the signing party.)

Property Owner(s): Name(s) Lot 3000: Marsha and Sherman Garver

Business Name _____

Mailing Address 34094 SE Elm St. City Scappoose State OR Zip 97056

Phone # _____ Fax # _____ Email Address _____

Does the owner of this site also own any adjacent property? Yes No (If yes, please list tax map and tax lots)

Property Owner(s) Signature(s) Sherman R. Garver Date: 6-25-25

Property Owner(s) Signature(s) Marsha Garver Date: 06-25-25

Annexation Application Rvs. 2025-May Page 1 of 8

OWNERSHIP AND APPLICANT INFORMATION (Property owner signature must be a wet-ink signature. If the property is under-going a change of ownership, proof of purchase or purchase contract must be provided if property owner of record is not the signing party.)

Property Owner(s): Name(s) Lot 3100: Christianne and Richard Watt

Business Name _____

Mailing Address 34102 SE Elm St. City Scappoose State OR Zip 97056

Phone # _____ Fax # _____ Email Address _____

Does the owner of this site also own any adjacent property? Yes No (If yes, please list tax map and tax lots)

Property Owner(s) Signature(s) Richard M. Watt Date: 8-6-25

Property Owner(s) Signature(s) Christanne Watt Date: 8-6-25

Applicant: Name Brad Hendrickson
Business Name _____
Mailing Address 134 River Dr. City St. Helens State OR Zip 97051
Phone # (503) 310-0235 Fax # _____ Email Address 3232brad@gmail.com
Applicant's Signature B. Hendrickson Date: 06/26/2025
Applicant's interest in property Developer (frontage improvements)

DETAILED SITE INFORMATION

Are any of the following present on site? If so, please specify the number of acres and/or percentage of site affected.

Floodplain _____ Wetlands _____ Significant Natural Resources _____

Cultural Resources _____ Airport Noise Contours _____ Slopes greater than 15% _____

Water Provider: City of Scappoose Well

Does the site have access to a City street? Yes No (Please explain): Frontage on SE Elm St. (this portion of SE Elm St. has been confirmed to be within City's jurisdiction)

Does the site have access to County road(s)? Yes No (Please explain): _____

Are there existing structures on the site? Yes No (If Yes, briefly explain future status of structures.) No changes

STRUCTURES: Are any of the following structures present on the site? (If so, please specify the number of each type of building, and if any of the buildings are in the 100-year floodplain, please write FLOODPLAIN)

Single Family Residence(s) #: 1 (lot 3100)
 Barn or Other Agricultural Building(s) #: _____
 Industrial Building(s) #: _____
 None

Accessory Building(s) #: 1 (lot 3100)
 Commercial Building(s): _____
 Other _____

BUSINESSES: Is any business being operated on the property to be annexed?

Yes No (If Yes, describe) _____

COMPLETENESS CHECK (For Office Use Only)

Received by _____ Date _____

Accepted as complete by _____ Date _____

Additional reviews pending? Yes No If yes, File # _____

Receipt # _____ Fee(s) Paid _____



PETITION FOR ANNEXATION TO THE CITY OF SCAPPOOSE, OREGON

TO: The Council of the City of Scappoose, Oregon

We, the undersigned property owners of and/or registered voters in the area described below, hereby petition for, and give our consent to, annexation of the area to the City of Scappoose.

| I AM A:* | | | | | | | | | |
|-------------------------|------------------|----|----|----|--|---------|---------|--------------|------|
| SIGNATURE | PRINTED NAME | PO | RV | OV | ADDRESS | TAX MAP | TAX LOT | PRECINCT NO. | DATE |
| <i>Marsha Garver</i> | Marsha Garver | | | | 34094 SE Elm Street Scappoose, OR 97056 | 3107-CC | 3000 | n/a | |
| <i>Sherman Garver</i> | Sherman Garver | | | | 34094 SE Elm Street Scappoose, OR 97056 | 3107-CC | 3000 | n/a | |
| <i>Christianne Watt</i> | Christianne Watt | | | | 34102 SE Elm Street Scappoose, OR 97056 | 3107-CC | 3100 | n/a | |
| <i>Richard M. Watt</i> | Richard Watt | | | | 34102 SE Elm Street Scappoose, OR 97056 | 3107-CC | 3100 | n/a | |
| | | | | | | | | | |
| | | | | | | | | | |
| | | | | | | | | | |

*PO = PROPERTY OWNER

RV = REGISTER VOTER

OV = OWNER VOTER



Scappoose Planning Department

33568 E. Columbia Ave. Scappoose, OR 97056

Phone: 503-543-7146

www.scappoose.gov

PARTITION APPLICATION

NOTICE TO APPLICANT: On original application form, please print legibly using black/dark blue ink or type. Applicants are advised to review the list of submittal requirements and recommendations indicated on each land use application form and in the applicable code section prior to submitting an application. Applicants are advised to schedule a pre-application meeting with the staff prior to submitting final application. **INCOMPLETE APPLICATIONS WILL NOT BE PROCESSED UNTIL THE PLANNING DEPARTMENT RECEIVES ALL REQUIRED SUBMITTAL MATERIALS.**

TRACKING INFORMATION (For Office Use Only)

Application Submittal Includes:

2 Hard Copies Required (Initial Submittal) Electronic Submittal

Fee

7 Hard Copies Required (Final Submittal, once deemed complete by City Planner)

Date Submitted with payment: _____

Receipt #: _____

File #: _____

Final Submittal Date: _____

SITE LOCATION & DESCRIPTION

Tax Map #(s) 3107-CC Tax Lot #(s) 3000

Frontage Street or Address 34094 SE Elm St.

Nearest Cross Street SE Tussing Way and SE 9th Street

Plan Designation SR Zoning: R-1, City of Scappoose (proposed) Site Size 0.75 _____ acres sq. ft.

Dimensions 155' x 202'

SUMMARY OF REQUEST

Proposed Project Name SE Elm Street Partition

Project Type/Narrative Summary: (Provide a brief summary and specify project type: Single Family Residential (SFR), Multi-family Residential (MFR), Commercial (C), Industrial (I)) Already developed with single family residential (SFR).

There are two (2) existing manufactured homes on the property (allowed by Columbia County hardship permit). Proposal is to partition off area with the second manufactured home so it can be sold.

Please indicate the dimensions of proposed parcels in feet:

| | Parcel Area | Parcel Width | Parcel Depth |
|-----------------------|----------------------|------------------|--------------|
| Parcel 1 measurements | 24,503 SF (L-shaped) | 80.4' and 154.6' | 201.6' |
| Parcel 2 measurements | 6,500 SF | 74.1' | 88.1' |
| Parcel 3 measurements | | | |

Note: If a residential project is proposed, a Residential Density Calculation Worksheet (page attached) must be submitted.

Is a Variance Requested? Yes No If Yes, identify type of request: Minor Variance Major Variance

Note: Procedures and applicable criteria for variances may be found in SDC Chapter 17.134

**PRELIMINARY PARTITION
(CONTINUED)**

Detailed Site Information

Are any of the following present on the site? (NOTE: If any of the below are present on-site, specify number of acres and/or percentage of site affected.)

Flood Plain _____ Wetlands _____ Significant Natural Resources _____
Cultural Resources _____ Airport Noise Contours _____ Slopes Greater Than 15% _____

Water Provider: City of Scappoose Well Other: Will connect to city water upon annexation

Does the site have access to City street(s) Yes No (Please explain) Frontage on SE Elm Street (a portion that has been confirmed to be within City limits)

Does the site have access to County road(s) Yes No (Please explain)

Are street/road improvements requested or required? Yes No (Please explain) 10-foot wide ROW dedication + half-street improvements to Neighborhood Collector standards (see drawings and narrative for detail)

Are there existing structures on the site? Yes No (If Yes, briefly explain future status of structures.)

No changes proposed

Are there existing wells or septic drain fields on the site? Yes No (If Yes, briefly explain future status.)

Existing septic drain field to be decommissioned. See Sheet C-1 - Existing Conditions Plan for location.

OWNERSHIP AND APPLICANT INFORMATION (Property owner signature must be a wet-ink signature. If the property is under-going a change of ownership, proof of purchase or purchase contract must be provided if property owner of record is not the signing party.)

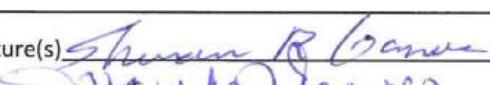
Property Owner(s): Name(s) Marsha and Sherman Garver

Business Name _____

Mailing Address 34094 SE Elm St. City Scappoose State OR Zip 97056

Phone # (503) 438-8477 Fax # _____ Email Address _____

Does the owner of this site also own any adjacent property? Yes No (If Yes, please list tax map and tax lots)

Property Owner(s) Signature(s)  Date: 6-25-25

Property Owner(s) Signature(s)  Date: 06-25-25

Applicant: Name Brad Hendrickson

Business Name _____

Mailing Address 134 River Dr., St. Helens, OR 97051

Phone # (503) 310-0235 Fax # _____ Email Address 3232brad@gmail.com

Applicant's Signature  Date: 06/26/2025

Applicant's interest in property Developer (frontage improvements)

RESIDENTIAL DENSITY CALCULATION WORKSHEET

To monitor compliance with State regulations, the City must track the net densities of new residential developments in the City. This worksheet must be completed by the applicant and submitted with the preliminary application for any residential or mixed-use subdivision, planned unit development, partition, or development review approval.

Project Name SE Elm Street Partition

Developer / Applicant Brad Hendrickson

Project Site Address 34094 SE Elm Street

Tax Map #(s) 3107-CC Tax Lot #(s) 3000

Plan Designation SR (Suburban Residential) Zoning R-1, City of Scappoose (proposed)
R-10, Columbia County existing)

Net residential density is calculated on net acreage, the area on a site which is eligible for development. Net acreage is calculated by subtracting undevelopable land from gross acreage.

Residential Density Calculations: Fill in the blanks below to calculate the net residential density.

Total Gross Area of Subject Site (1 acre = 43,560 sq. ft.): 32,497 square feet

Less “undevlopable land”: (as applicable)

| | | |
|---|-------|-------------------------------|
| Public street right-of-way dedication | 1,494 | Elm St. ROW |
| Public or private access easements | 1,236 | Public Utility Easement (PUE) |
| Public or private access easements | | |
| Private street tracts | | |
| Required internal fire access drive areas | | |
| Storm water treatment and detention areas | | |
| Wetlands and required CWS vegetated corridors | | |
| Areas with 20% or greater slopes | | |
| Areas within the 100-year floodplain | | |
| Land dedicated to the City for parks or greenways | | |
| Maneuvering area for truck loading docks | | |
| Electrical transformer platforms, industrial chemical and/or gas storage areas, or other hazardous area where occupancy is Not Permitted for safety reasons | | |

| | | |
|---|--------|--------------------|
| Total Net Area (total gross area minus undevelopable land): | 29,767 | square feet |
| Net Acreage of Subject Site (total net area divided by 43,560): | .68 | acres |
| Total Number of Residential Units Proposed: | 2.0 | units |
| Net Residential Density (proposed units divided by net acreage): | 2.9 | units per net acre |



Brad Hendrickson

SE Elm Street Annexation, Zone Change and Minor Partition

Land Use Narrative

Prepared by Lower Columbia Engineering
Submitted to City of Scappoose
Planning Department
June 2025



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List of Drawings

- G-1 Cover Sheet
- C-1 Existing Conditions Plan
- C-2 Proposed Conditions Plan
- C-3 Frontage Improvement Plan

List of Exhibits

- Exhibit A: Preliminary Plat
- Exhibit B: Legal Descriptions



Project Summary

Internal File No: 3693

Pre-Application Date: 4/3/25

Applicant:
Brad Hendrickson
134 N. River St.
St. Helens, OR 97051
(503) 310-0235
3232brad@gmail.com

Applicant Representative:
Chase Berg
Lower Columbia Engineering
58640 McNulty Way
St. Helens, OR 97051
(503) 366-0399
chase@lowercolumbiaengr.com

Current Zoning Designation: Single Family Residential (R-10), Columbia County (tax lots 3000 & 3100)

Proposed Zoning Designation: Low Density Residential (R-1), City of Scappoose (tax lots 3000 & 3100)

Request: Annexation (ANX), Zone Change (ZC) & Minor Partition (MiP)

Location, Size and Ownership:

| Tax Lot ID | Site Address | Property Owners | Size | Request |
|---------------|---|----------------------------|------------|---|
| 3107-CC-03000 | 34094 SE Elm St. Scappoose, OR 97056 | Marsha & Sherman Garver | 0.75 acres | Annexation, Zone Change & Minor Partition |
| 3107-CC-03100 | 34102 SE Elm St. Scappoose, OR 97056 | Christianne & Richard Watt | 1.3 acres | Annexation & Zone Change |

Project Overview

Annexation and Zone Change

The applicant is seeking approval to annex two tax lots (3000 and 3100) into the City and to rezone both lots from Columbia County Zone R-10 (Single Family Residential) to City of Scappoose Zone R-1 (Low-Density Residential). The purpose of the annexation is to allow the properties to connect to the public sewer system in light of a failing septic system on lot 3100 and to allow for the partition of lot 3000 into two smaller lots. Both of the new lots will connect to City water. Lot 3100 will continue using its well as a water source.

Both lots are already developed with residential uses and are inside the Urban Growth Boundary (UGB). No new development is proposed beyond utility connections and right-of-way improvements required by the City. Extension of public services to the subject properties is both efficient and logical. They are adjacent to the City limits on all four sides. Public utilities are available for connections in the adjacent SE Elm Street right-of-way and it has been determined that there is sufficient capacity to serve these sites. The properties are already being served by the Scappoose Fire District, School District, Police and local road system.

Lot 3100 is a 1.3-acre flag lot with one stick-built dwelling and one general purpose accessory building. The existing septic system is failing and annexation/rezoning is required for a new connection to the city sewer and decommissioning of the failing septic. Lot 3000 is 0.75 acres and has two manufactured homes and one detached garage. The placement of the second manufactured home was allowed under a Columbia County hardship permit.

Minor Partition

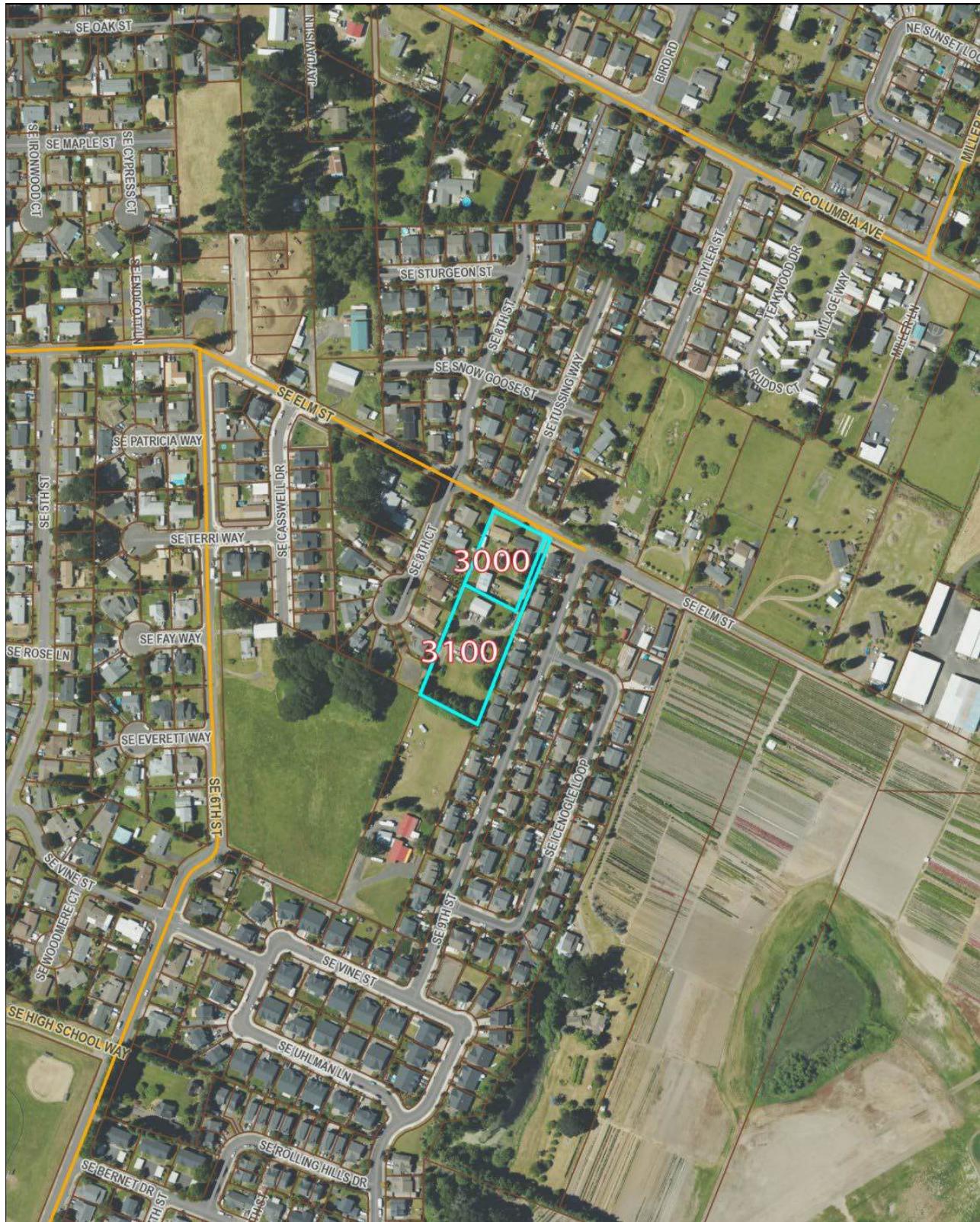
As mentioned above, the applicant also seeks approval of a minor partition to divide tax lot 3000 into two smaller lots. Annexation and partition will allow the part of the property with the second manufactured home to be split off from the parent lot and sold. Both lots would then be legally conforming under the proposed R-1 zoning.

As required by partition criteria, Lot 3000 will dedicate a 10-foot-wide strip along its entire frontage to the adjacent SE Elm Street public right-of-way. With this, the applicant will also improve the southern half of the right-of-way to meet the City's Neighborhood Route standard.

Consolidated Application

The two lots have different owners who have opted to consolidate their land use applications for the sake of efficiency and to coordinate utility and frontage improvements. No Comprehensive Plan amendment is needed as the properties are already designated Suburban Residential under the current Comp Plan and the existing development on the lots meets the criteria for that designation.

Vicinity Map



Compliance with Scappoose Municipal and Development Codes

This section of the narrative demonstrates the project's compliance with all applicable provisions of Chapters 12 and 17 of the Scappoose Development Code. All text in *italics* are direct quotes from the code, which are followed by applicant responses in blue.

12.10 Visual Clearance Areas

12.10.020 Visual clearance – Required.

A. *A visual clearance area shall be maintained on the corners of all property adjacent to an unregulated intersection of two streets, a street and a railroad, or a driveway providing access to a public or private street.*

Response: Please see **Sheet C-3 – Frontage Improvement Plan** for proposed visual clearance areas at the intersections of SE Elm Street and the residential driveways.

12.10.030 Visual clearance area dimensions.

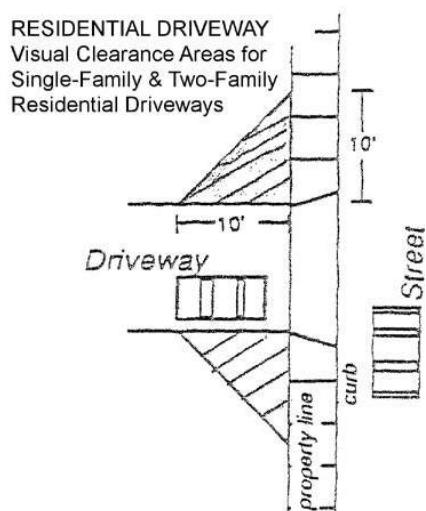
[...]

B. *Driveway Intersections (see also Figure 12.10.2):*

[...]

2. *Single-Family and Two-Family Developments. Driveways to public or private streets shall have a minimum visual clearance area formed by the intersection of the edges of the driveway, the street right-of-way line, and a straight line joining said lines through points **ten feet from their intersection**. No off-street parking area shall be located in a driveway visual clearance area. (Ord. 820 § 2, 2012)*

Figure 12.10.2. Visual Clearance Areas for Driveways



 Visual Clearance Area for Commercial, Industrial, Institutional, and Multi-Family Driveways

 Visual Clearance Area for Single-Family & Two-Family Residential Driveways

Response: Please see **Sheet C-3 – Frontage Improvement Plan** for visual clearance areas at the intersections of SE Elm Street and the residential driveways. The dimensions of each visual clearance area will conform to the requirements for residential driveways, covering a triangular area extending 10 feet perpendicular and parallel to the property line from the intersection of the street-fronting property line with the public right-of-way. There will be no off-street parking in the driveway visual clearance areas. The visual clearance areas will be free of all structures, vehicles, plantings, etc. that could impede visibility. These criteria are met.

17.01 Introduction.

17.01.060 Right-of-way dedications and improvements.

Upon approval of any development permit or any land use approval of any property which abuts or is served by an existing substandard street or roadway, the applicant shall make the necessary right-of-way dedications for the entire frontage of the property to provide for minimum right-of-way widths according to the city's public works design standards and shall improve the abutting portion of the street or roadway providing access to the property in accordance with the standards in Chapter [17.154](#). (Ord. 634 § 1 Exh. A, 1995)

Response: See **Sheet C-3 – Frontage Improvement Plan**. The existing SE Elm Street right-of-way is 50 feet wide in the portion adjacent to the subject properties. Neighborhood Routes, as designated by the City's TSP, are required to be 60 feet wide. As the property across the street has already provided a dedication to establish centerline, the City is requiring a 10-foot-wide dedication from the proposed Lots 1 and 2 along their frontage to bring this portion of SE Elm Street to standard. The frontage will be improved to the Neighborhood Route standard with half-street improvements, including up to 18 feet of paving from the centerline as needed (depending on condition of existing paving), new curb and gutter, new 6-foot-wide attached sidewalks, new street trees and one new streetlight. This criterion is met.

17.22 Amendment to the Title, Comprehensive Plan, and Maps

17.22.040 Approval criteria.

Planning commission review and recommendation, and Council approval, of an ordinance amending the comprehensive plan, the zoning map, or this title shall be based on the following criteria:

A. If the proposal involves an amendment to the comprehensive plan, the amendment is consistent with the Statewide Planning Goals and relevant Oregon Revised Statutes and Administrative Rules;

Response: No amendment to the Comprehensive Plan is proposed. The proposal is consistent with applicable Oregon Revised Statutes and Administrative Rules (namely, the Transportation Planning Rule). This criterion is met.

B. The proposal is consistent with the comprehensive plan (although the comprehensive plan may be amended concurrently with proposed changes in zoning or this title), the standards of this title, or other applicable implementing ordinances;

Response: The subject properties are designated as "Suburban Residential" in the Comprehensive Plan Map. The proposed zone change to R-1 (Low-Density Residential) aligns with that designation. Therefore, the proposal is consistent with the Comprehensive Plan.

Comprehensive Plan – Policies for the Urban Growth Boundary – Policy 6 (pg. 152)

6) Approve annexations of residential lands, except in the cases of health hazards, when:

A) There is sufficient capacity in the sewer, water, street, school, police and fire systems to service the potential additional populace.

Response: It has been determined that there is sufficient capacity in sewer, water, street, school, police and fire systems to service the potential additional populace. The subject properties are already served by the Scappoose Rural Fire Protection District and Scappoose School District. There is public water, sewer and storm available for connection, per the description below, in SE Elm Street, adjacent to the subject properties. The adjacent right-of-way will be improved to City's Neighborhood Route standard. The properties are already served by the local street system. The proposed annexation and zone change would not increase the density of existing development on the subject properties.

Storm: There is an existing 24" storm system along the SE Elm Street frontage and two catch basins on the southside of SE Elm Street. As the subject properties are already developed there will be no additional contributions to the stormwater systems apart from what is redirected from the public right-of-way after construction of the new curb and sidewalk. As part of the frontage improvements the applicant will relocate and build a new catch basin to current PWDS.

Sewer: There is an existing 15" PVC sewer line along the frontage that conveys flows to the west and then north on Tussing Way toward E Columbia Ave. According to input from the City Engineer, this line has enough depth for all lots to be able to convey their flow in accordance with PWDS and Oregon Plumbing Specialty Code (OPSC).

Water: There is an existing 8" cast iron waterline along frontage the south side of the street and is available for all three lots to connect to.

This criterion is met.

B) Sufficient in-filling of vacant land has occurred to warrant an expansion.

Response: Scappoose's 2022 Housing Capacity Analysis identifies a projected deficit of land inside the UGB zoned for medium- and high-density housing over the next 20 years (Housing Capacity Analysis pg. 32). This indicates there has been sufficient in-filling of vacant land to warrant annexation of additional residential land. Although already developed, annexation would provide the potential for the subject sites to be up-zoned and infilled or redeveloped at a higher density in the future (with the appropriate approval processes). This criterion is met.

C. The change will not adversely affect the health, safety, and welfare of the community;

Response: The proposed annexation and zone change does not change the existing uses or intensity of uses of the subject properties. Annexation allows for the properties to connect to sewer, which will decrease the hazard posed by the failing septic system on lot 3100. In addition, annexation will lead to public improvements that will benefit the surrounding community, such as a new sidewalk, street trees, curb, gutter and streetlight. This criterion is met.

D. The proposal either responds to changes in the community or it corrects a mistake or inconsistency in the comprehensive plan, the zoning map, or this title; and

Response: The proposal responds to changes in the community such as the need to connect to City sewer due to a failing septic system and the desire to partition due to increased property values. Annexation would also increase the level of consistency with the Comprehensive Plan given that the properties abut City limits on all four sides. This criterion is met.

E. The amendment conforms to Section 17.22.050. (Ord. 828, 2013)

Response: Consistency with Section 17.22.050 (compliance with the Transportation Planning Rule) is demonstrated below. This criterion is met.

17.22.050 Transportation planning rule compliance.

Proposals to amend the comprehensive plan or zoning map shall be reviewed to determine whether they significantly affect a transportation facility pursuant to Oregon Administrative Rule (OAR) 660-012-0060 (Transportation Planning Rule - TPR). Where the City, in consultation with the applicable roadway authority, finds that a proposed amendment would have a significant effect on a transportation facility, the City shall work with the roadway authority and the applicant to modify the request or mitigate the impacts in accordance with the TPR and applicable law. (Ord. 828, 2013)

OAR 660-012-0060 Transportation Planning Rule

(1) If an amendment to a functional plan, an acknowledged comprehensive plan, or a land use regulation (including a zoning map) would significantly affect an existing or planned transportation facility, then the local government must put in place measures as provided in section (2) of this rule, unless the amendment is allowed under section (3), (9) or (10) of this rule. A plan or land use regulation amendment significantly affects a transportation facility if it would:

(a) Change the functional classification of an existing or planned transportation facility (exclusive of correction of map errors in an adopted plan);

Response: The proposed annexation and zone change will not necessitate changes to the functional classification of existing or planned transportation facility.

(b) Change standards implementing a functional classification system; or

Response: The proposed annexation and zone change will not change any standards implementing the functional classification system.

(c) Result in any of the effects listed in paragraphs (A) through (C) of this subsection based on projected conditions measured at the end of the planning period identified in the adopted TSP. As part of evaluating projected conditions, the amount of traffic projected to be generated within the area of the amendment may be reduced if the amendment includes an enforceable, ongoing requirement that would demonstrably limit traffic generation, including, but not limited to, transportation demand management. This reduction may diminish or completely eliminate the significant effect of the amendment.

(A) Types or levels of travel or access that are inconsistent with the functional classification of an existing or planned transportation facility;

(B) Degrade the performance of an existing or planned transportation facility such that it would not meet the performance standards identified in the TSP or comprehensive plan; or

(C) Degrade the performance of an existing or planned transportation facility that is otherwise projected to not meet the performance standards identified in the TSP or comprehensive plan.

[...]

Response: The proposed annexation will not result in any of the effects listed in paragraphs (A) through (C). The functional classification of SE Elm Street in the City's TSP assumed a Suburban Residential-level of development as designated by the Comprehensive Plan. Upon annexation the existing development on the subject properties will comply with R-1 (Suburban Residential) zoning and will not increase traffic levels beyond the capacity of the existing transportation facility or impact its meeting of performance standards identified in the TSP. Thus, no measure to reduce projected traffic generation is needed. This criterion is met.

(9) Notwithstanding section (1) of this rule, a local government may find that an amendment to a zoning map does not significantly affect an existing or planned transportation facility if all of the following requirements are met.

(a) The proposed zoning is consistent with the existing comprehensive plan map designation and the amendment does not change the comprehensive plan map;

(b) The local government has an acknowledged TSP and the proposed zoning is consistent with the TSP; and

(c) The area subject to the zoning map amendment was not exempted from this rule at the time of an urban growth boundary amendment as permitted in OAR 660-024-0020 (Adoption or Amendment of a UGB)(1)(d), or the area was exempted from this rule but the local government has a subsequently acknowledged TSP amendment that accounted for urbanization of the area.

Response: The proposed zoning is consistent with the existing Comprehensive Plan designation of Suburban Residential and the functional classification of SE Elm Street as a Neighborhood Route in the City's TSP, which assumes Suburban Residential development of the subject properties. The area subject to the zoning map amendment was not exempted from this rule at the time of the related urban growth boundary amendment. Therefore, this amendment would not significantly affect an existing/planned transportation facility (as defined by the Transportation Planning Rule). This criterion is met.

17.44 R-1 Low Density Residential

17.44.030

| Use | |
|---|---------------------------------|
| Single-family detached residential dwelling | Permitted outright ¹ |
| [...] | |

1 *These uses and their accessory uses are permitted outright outside of the Scappoose Creek Flood Plain. In the R-1 zone within the Scappoose Creek Flood Plain, only uses listed in Section 17.84.040 shall be permitted. Additional requirements shall include any applicable section of this title*

2 *These uses and their accessory uses may be permitted in the R-1 zone when authorized by the planning commission in accordance with the requirements of Chapter 17.130, other relevant sections of this title and any conditions imposed by the Planning Commission when such uses are located outside of Scappoose Creek Flood Plain.*

(Ord. 740 §§ 2, 3, 2004; Ord. 705 § 1, 2001; Ord. 634 § 1 Exh. A, 1995)

Response: The existing use of the two subject properties as detached single-family residences is allowed outright in the R-1 zone. Although lot 3000 currently has two principal buildings (both detached, single-family dwellings), allowed under a hardship permit granted by the County, upon partition each resulting lot will have only one principal building and will thus be in conformance with R-1 zoning requirements. The subject properties are not within the floodplain (FIRM panel 41009C0463D, effective 11/26/2010). This criterion is met.

17.44.050 Dimensional requirement.

| Dimensional Requirements | Requirement ¹ |
|--|--|
| Minimum lot area | |
| Single-family detached | <i>Six thousand (6,000) square feet outside the Scappoose Creek Flood Plain</i> |
| Response: See Sheet C-2 – Proposed Conditions Plan. | The subject properties are located outside the floodplain. Not including the flag portion, Lot 3100 is 53,267 square feet. The two lots proposed to be created out of parent lot 3000 will be 24,503 square feet (Lot 1) and 6,500 square feet (Lot 2). These dimensions do not include the approximately 1,5450-square-foot area to be dedicated to the SE Elm Street right-of-way. These three lot sizes all exceed the 6,000 square foot minimum lot area. This criterion is met. |
| [...] | |
| Minimum lot width | <i>Not be less than fifty feet, except the minimum lot width at front property line on the arc of an approved full cul-de-sac shall not be less than thirty feet</i> |
| Response: See Sheet C-2 – Proposed Conditions Plan. | Lot 3100 is 170.6 feet wide (except at the flag portion, which is 15.9 feet wide). Proposed Lot 1 will be 80.4 feet wide and proposed Lot 2 will be 74.1 feet wide. These exceed the 50-foot minimum. This criterion is met. |
| | <i>Flag lots shall provide a minimum of twenty-five feet of frontage along a public right-of-way</i> |

Response: See **Sheet C-2 – Proposed Conditions Plan**. Lot 3100, a flag lot, has 15.9 feet of frontage along SE Elm Street. Although this is less than the stated minimum of 25 feet, this proposal will not worsen the existing degree of noncompliance.

| | |
|------------------------|--------------------|
| <i>Minimum setback</i> | |
| <i>Front Yard</i> | <i>Twenty feet</i> |

Response: See **Sheet C-2 – Proposed Conditions Plan**. Upon partition, proposed Lot 1 will have a front setback of 23.1 feet, exceeding the 20-foot minimum.

Proposed Lot 2 will have a front setback of 16.8 feet after the required dedication of 10 feet of frontage to the SE Elm Street public right-of-way. This substandard setback is deemed acceptable as it allows for the development of a full 60-foot-wide street section per the City's TSP and public works design standards.

Lot 3100 has a front setback of 324 feet for the principal building and 221.9 feet for the accessory building, as measured from its frontage on SE Elm Street. This criterion is met.

| | |
|-------------------------------------|---|
| <i>Front of garages or carports</i> | <i>Twenty feet from the property line where access occurs</i> |
|-------------------------------------|---|

Response: See **Sheet C-2 – Proposed Conditions Plan**. Upon partition, the front setback of the detached garage setback on proposed Lot 1 will be 142 feet. Proposed Lot 2 will not have a garage. On lot 3100, the front setback for the attached garage is 342 feet. This criterion is met.

| | |
|------------------|---|
| <i>Side yard</i> | <i>Total a minimum of fifteen feet with one setback not less than ten feet, which shall be on the street side for corner lots</i> |
|------------------|---|

Response: See **Sheet C-2 – Proposed Conditions Plan**. Proposed Lot 1 will have side setbacks of 19.5 feet (west side) and 8.3 feet (east side). Proposed Lot 2 will have side setbacks of 10.4 feet (west side) and 15.4 feet (east side). On lot 3100 the side setbacks are 58.8 feet (west side) and 52.7 feet (east side) for the principal building. There are no corner lots. This criterion is met.

| | |
|------------------|--------------------|
| <i>Rear yard</i> | <i>Twenty feet</i> |
|------------------|--------------------|

Response: See **Sheet C-2 – Proposed Conditions Plan**. The rear setback (principal building) for proposed Lot 1 is 147.0 feet. The rear setback for proposed Lot 2 is 39.2 feet. On lot 3100, the rear setback (principal building) is 146.1 feet. These all exceed the 20-foot minimum. This criterion is met.

| | |
|---|-----------------------|
| <i>Setbacks for accessory building behind a residence</i> | |
| <i>Side</i> | <i>Five feet each</i> |

Response: See **Sheet C-2 – Proposed Conditions Plan**. On proposed Lot 1 the detached garage will have side setbacks of 36.5 feet (west) and 88.0 feet. Proposed lot 2 will have no accessory buildings. On lot 3100, the accessory building has side setbacks of 37.1 feet (west) and 80.8 feet (east side). These all exceed the five-foot minimum. This criterion is met.

| | |
|-------------|------------------|
| <i>Rear</i> | <i>Five feet</i> |
|-------------|------------------|

Response: See **Sheet C-2 – Proposed Conditions Plan**. Proposed Lot 1 will have a rear setback of 7 feet for the detached garage. Proposed Lot 2 will have no accessory buildings. Lot 3100 has an accessory building with a rear setback of 265.4 feet. This criterion is met.

| | |
|-----------------------|-------------------------|
| <i>Maximum height</i> | <i>Thirty-five feet</i> |
|-----------------------|-------------------------|

Response: The dwellings on the subject properties are all single-story and approximately 10 feet tall, less than the 35-foot maximum. This criterion is met.

| | |
|---------------------------|------------------------|
| <i>Accessory Building</i> | <i>Twenty-two feet</i> |
|---------------------------|------------------------|

Response: All accessory structures are under 22 feet tall. This criterion is met.

| | |
|-----------------------------------|------------|
| <i>Principal building per lot</i> | <i>One</i> |
|-----------------------------------|------------|

Response: See **Sheet C-2 – Proposed Conditions Plan**. Lot 3000 currently has two principal buildings, allowed under a hardship permit issued by Columbia County. Upon partition, each new lot will have only one principal building. Lot 3100 has only one principal building. This criterion is met.

| | |
|----------------------------------|--|
| <i>Maximum building coverage</i> | <i>Thirty-five percent of the lot area</i> |
|----------------------------------|--|

Response: Upon partition, proposed Lot 1 will have a total building footprint of 2,792 square feet*, covering 11.4% of the 24,503-square foot lot. Proposed Lot 2 will have a total building footprint of 1,344 square feet*, covering 20.7% of the 6,500-square foot lot.

Lot 3100 buildings have a total footprint of 3,512 square feet* (including the residential structure with attached garage and one accessory building), covering 6.2% of the 1.3-acre (approximately 56,628 square feet) lot. All lots have building coverage under the 35% maximum. This criterion is met.

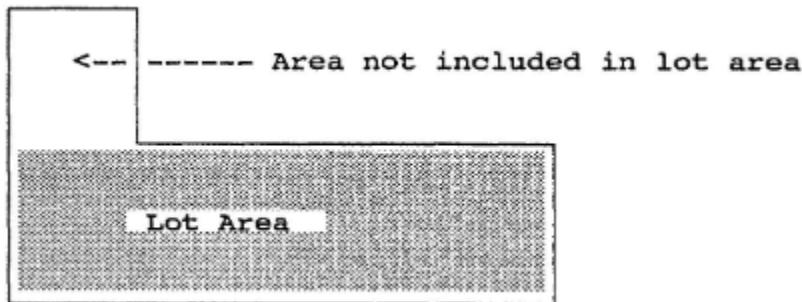
*Note: All building sizes are based on data from Columbia County Assessor website retrieved 6/18/2025.

17.96 Lots – Exceptions and Additional Setbacks.

[...]

17.96.090 Lot area for flag lots.

- A. *The lot area for a flag lot shall comply with the lot area requirements of the applicable zoning district.*
- B. *The lot area shall be provided entirely within the building site area exclusive of any accessway (see figure following).*



(Ord. 634 § 1 Exh. A, 1995)

Response: The area of flag lot 3100, exclusive of the accessway, is 53,267 square feet, exceeding the 6,000-square-foot minimum in the R-1 zone. These criteria are met.

17.104 Street Trees

17.104.040 Standards for street trees.

- A. *Street trees shall be selected from the approved street tree list on file with the Planning Department.*

Response: Please see **Sheet C-3 – Frontage Improvement Plan**. There will be four (4) new 'Autumn Brilliance' Serviceberry street trees planted at 20 feet on-center within the new landscape strip. This cultivar grows to 25 feet tall and 20 feet wide (according to the City of Scappoose approved street tree list) and is thus appropriate for the required 5.5-foot-wide planting strip. The applicant's contractor may need to choose a different species based on availability, but will only do so with prior approval from the City of Scappoose.

B. At the time of planting, street trees shall not be less than ten feet high for deciduous trees and five feet high for evergreen trees.

Response: The proposed street trees will be no less than 10 feet tall upon planting.

C. Spacing and minimum planting areas for street trees shall be as follows:

- 1. Street trees under twenty-five feet tall and less than sixteen feet wide at maturity shall be spaced no further than fifteen feet apart in planting areas containing no less than sixteen square feet of porous surface and not less than four feet wide;*
- 2. Street trees under twenty-five feet tall and greater than sixteen feet wide at maturity shall be spaced no further than twenty feet apart in planting areas containing no less than sixteen square feet of porous surface and not less than four feet wide;*
- 3. Street trees between twenty-five feet to forty feet tall and less than twenty-five feet wide at maturity shall be spaced no greater than twenty-five feet apart in planting areas containing no less than twenty-four square feet of porous surface and not less than six feet wide;*
- 4. Street trees between twenty-five feet to forty feet tall and greater than twenty-five feet wide at maturity shall be spaced no greater than thirty feet apart in planting areas containing no less than twenty-four square feet of porous surface and not less than six feet wide;*
- 5. Street trees greater than forty feet tall at maturity shall be spaced no greater than forty feet apart in planting areas containing not less than thirty-six square feet of porous surface and not less than eight feet wide.*

Response: See **Sheet C-3 – Frontage Improvement Plan**. The 'Autumn Brilliance' Serviceberry cultivar grows to 25 feet tall and 20 feet wide at maturity, according to the City of Scappoose approved street tree list. Therefore, they will be spaced 20 feet apart within a planting area that is at least 6 feet in width and 24 square feet in porous planting area per tree (in accordance with items (2) and (3) above). Although the proposed planting strip within the public right of way will be only 5.5 feet wide, it will be contiguous with the front yards of the subject properties and will thus have an effective planting area of at least 6 feet wide. These criteria are met.

D. Street trees located under or within ten feet of overhead utility lines shall be less than twenty-five feet tall at maturity.

Response: There are no overhead utility lines within 10 feet of the area where street trees are proposed to be planted. This criterion is not applicable.

E. Street trees shall be planted in accordance with the requirements of Scappoose Municipal Code Section 13.28.020(C). (Ord. 875, 2018; Ord. 659 § 3, 1997)

Response: Street trees will be planted in accordance with Scappoose Municipal Code Section 13.28.020(C).

17.104.060 Maintenance of street trees.

A. The adjacent owner, tenant, and their agent, if any, shall be jointly and severally responsible for the maintenance of all street trees which shall be maintained in good condition so as to present a healthy, neat and orderly appearance and tree wells shall be kept free from refuse and debris.

Response: The applicant understands that the adjacent owner shall be responsible for maintenance of all street trees in good condition.

B. All street trees shall be controlled by pruning to National Arborist Association Pruning Standards for Shade Trees included as Appendix B of the Scappoose Comprehensive Urban Forestry Plan.

Response: The applicant will control street trees through pruning to meet the National Arborist Association standards.

C. Every owner of any tree overhanging any street or right-of-way within the city shall prune the branches so that such branches shall not severely obstruct the light from any street lamp or obstruct the view of any street intersection and so that there shall be a clear space of thirteen feet above street surface or eight feet above the sidewalk surface. Such owners shall remove all dead, diseased or dangerous trees, or broken or decayed limbs which constitute a menace to the safety of the public. The city shall have the right to prune any tree or shrub on private property when it interferes with the proper spread of light along the street from a street light, or interferes with visibility of any traffic-control device or sign or sight triangle at intersections as defined in Scappoose Municipal Code 12.10, Visual Clearance Areas. Tree limbs that grow near high voltage electrical conductors shall be maintained clear of such conductors by the electric utility company in compliance with any applicable franchise agreements.

Response: The applicant will prune street trees such that the branches shall not severely obstruct the light from any street lamp or obstruct the view of any street intersection, leaving a clear space of 13 feet above the street surface or 8 feet above the sidewalk surface. They will remove any dead or damaged trees or limbs that constitute a menace to the safety of the public. There are no high voltage electrical conductors overhead in the vicinity.

D. The city shall have the right to plant, prune, and otherwise maintain trees, plants and shrubs within the lines of all streets, alleys, avenues, lanes, as may be necessary to insure public safety or to preserve or enhance the symmetry and beauty of such public grounds.

Response: The applicant understands the city has the right to maintain trees, plants and shrubs within streets, alleys, avenues and lanes to ensure public safety and preserve or enhance the symmetry and beauty of such public grounds.

E. It is unlawful as a normal practice for any person, firm or city department to top any street tree. Topping is defined as the severe cutting back of limbs within the tree's crown to such a degree so as to remove the normal canopy and disfigure the tree. Trees severely damaged by storms or other causes, or certain trees under utility wires or other obstructions where other pruning practices are impractical may be exempted from this chapter at the determination of the city manager after consultation with a registered arborist or certified forester. (Ord. 820 § 7, 2012; Ord. 659 § 3, 1997)

Response: The applicant understands that topping of street trees is not allowed unless through determination of the city manager.

17.136 Annexations

17.136.020 Policy.

Annexations shall be considered on a case-by-case basis, taking into account the goals and policies in the Scappoose comprehensive plan, long range costs and benefits of annexation, statewide planning goals, this title and other ordinances of the city and the policies and regulations of affected agencies' jurisdictions and special districts.

A. It is the city's policy to encourage and support annexation where:

1. The annexation complies with the provisions of the Scappoose comprehensive plan;

Response: The proposed annexation complies with the goals and policies of the Comprehensive Plan, as previously discussed in response to section 17.22.

2. The annexation would provide a logical service area, straighten boundaries, eliminate or preclude islands of unincorporated property, and contribute to a clear identification of the city;

Response: The subject properties are adjacent to the City limits to the north, south, east and west. Their annexation would therefore provide a logical service area and eliminate islands of unincorporated property.

3. *The annexation would benefit the city by addition to its revenues of an amount that would be at least equal to the cost of providing services to the area;*

Response: It is a reasonable assumption that revenues from annexation would cover the cost of providing services to the area.

4. *The annexation would be clearly to the city's advantage in controlling the growth and development plans for the area.*

Response: Annexation will allow the City to manage growth and development plans for the area.

B. *It is the city's policy to discourage and deny annexation where:*

1. *The annexation is inconsistent with the provisions of the Scappoose comprehensive plan;*
2. *The annexation would cause an unreasonable disruption or distortion of the current city boundary or service area;*
3. *The annexation would severely decrease the ability of the city to provide services to an area either inside or outside of the city;*
4. *Full urban services could not be made available within a reasonable time. (Ord. 691 § 1, 1999; Ord. 634 § 1 Exh. A, 1995)*

Response: The proposal is consistent with the provisions of the Scappoose Comprehensive Plan, as previously discussed. The annexation does not decrease the ability of the City to provide services and does not cause an unreasonable disruption of the current City boundary. Therefore, city policy supports annexation of the subject properties.

17.136.040 Approval standards.

A. *The decision to approve, approve with modification or deny, shall be based on the following criteria:*

1. *All services and facilities are available to the area and have sufficient capacity to provide service for the proposed annexation area;*

Response: The subject properties are already served by the Scappoose Fire District, School District, and Police. Public water, sewer and storm is available with sufficient capacity adjacent to the site in the SE Elm Street right-of-way. The subject properties already have electric and internet service. This criterion is met.

2. *The impact upon public services which include but are not limited to police and fire protection, schools and public transportation to the extent that they shall not be unduly compromised;*

Response: As discussed previously, the proposed annexation will have a minimal impact on the capacity of public service providers as the site is already being served by several of these providers as well as the local road system. No additional development is proposed at this time that would generate additional demand for services. This criterion is met.

3. *The need for housing, employment opportunities and livability in the city and surrounding areas;*

Response: Based on the City's 2022 Housing Capacity Analysis, there is a need for additional housing in the area.

Although additional housing on the subject properties is not proposed at this time, annexation into the City would allow for further partition and/or addition of new dwelling units under R-1 zoning, and potentially more in the future were the appropriate infrastructure developed to support a zone change to a denser residential land use designation.

Additionally, annexation will alleviate the health hazard posed by the failing septic system on Lot 3100, and avoid the potential future failure of the septic system on Lot 3000 by allowing connection to City's sewer system. This criterion is met.

4. *The location of the site in relation to efficient provision of public facilities, services, transportation, energy conservation, urbanization and social impacts. (Ord. 691 § 1, 1999; Ord. 634 § 1 Exh. A, 1995)*

Response: This site is contiguous to the existing City limits along its northern, southern, western and eastern boundaries. The Scappoose Fire District, Police and School District are already serving the subject properties. The site is also already being served by the local road system. Therefore, the location of the site is conducive to efficient provision of services upon annexation. This criterion is met.

17.136.070 Zoning upon annexation.

Upon annexation, the area annexed shall be automatically zoned to the corresponding land use zoning classification as shown in the table below. The zoning designation shown on the table below is the city's zoning district which most closely implements the city's comprehensive plan map designation.

| <u>Comprehensive Plan Designation</u> | <u>Zoning Classification</u> |
|--|-------------------------------------|
| SR, Suburban Residential | R-1, Low Density Residential |
| GR, General Residential | R-4, Moderate Density Residential |
| MH, Manufactured Home | MH, Manufactured Home Residential |
| C, Commercial | EC, Expanded Commercial |
| I, Industrial | LI, Light Industrial |
| AE, Airport Employment | PUA, Public Use Airport |

(Ord. 816 § 12, 2011; Ord. 691 § 1, 1999)

Response: Both subject properties have a Comprehensive Plan designation of SR, Suburban Residential. Upon annexation, the sites would automatically be zoned R-1. This criterion is met.

17.152 Land Division – Major and Minor Land Partitions and Property Line Adjustment

17.152.030 General Provisions.

Note: Only proposed Lots 1 and 2 are addressed in this section. There is no partition proposed for Lot 3100.

A. *An application for a major or minor partition shall be processed through a two-step process: (1) the tentative plan, and (2) the final plat:*

- 1. The tentative plan for a major partition shall be approved by the planning commission before the final plat can be submitted for approval consideration; the tentative plan for a minor partition shall be approved by the planner before the final plat can be submitted for approval consideration; and*
- 2. The final plat shall reflect all conditions of approval of the tentative plan.*

Response: The proposed minor partition will follow the two-step process described above through submittal of a preliminary plat for consideration by planning commission and then a final plat reflecting all conditions of approval for the tentative plan.

B. *All partition and property line adjustment proposals shall be in conformity with all state regulations set forth in ORS Chapter 92, Subdivisions and Partitions.*

Response: The proposed partition conforms with all regulations set forth in ORS Chapter 92, Subdivisions and Partitions. This criterion is met.

C. When partitioning tracts into large lots, the approval authority shall require that the lots be of such size and shape as to facilitate future redivision in accordance with the requirements of the zoning district and this title.

Response: The size and shape of the partitioned lots are such that Lot 1 could be further partitioned under current zoning, assuming creation of a new flag lot or access via flag lot 3100 were to be allowed.

D. Where landfill and/or development is allowed within the floodway fringe, the city may require the dedication of sufficient open land area for a greenway adjoining and within the floodplain. This area shall include portions at a suitable elevation for the construction of a pedestrian/bicycle pathway within the floodplain.

Response: The subject properties are not within the floodway fringe. This criterion is not applicable.

E. All partition proposals shall have public utilities and facilities such as sewer, gas, electrical, and water systems located and constructed to minimize flood damage.

Response: The subject properties are not within the floodplain or floodway fringe. All new utilities and frontage improvements will be constructed per Scappoose's PWDS and thus will minimize flood damage. This criterion is met.

F. All partition proposals shall have adequate drainage provided to reduce exposure to flood damage.

Response: There are no known drainage issues on the subject property, and no grading or additional development is proposed that would increase risk of flood damage. Frontage improvements will be constructed per PWDS standards to direct runoff from impervious surfaces (sidewalk and travel lanes) into the proposed relocated stormwater catch basin on the south side of SE Elm Street (see **Sheet C-3 – Frontage Improvement Plan**).

G. All land partition proposals shall include neighborhood circulation plans that conceptualize future street plans and lot patterns to parcels within five hundred feet of the subject site. Circulation plans address future vehicular/bicycle/pedestrian transportation systems including bike lanes, sidewalks, bicycle/pedestrian paths and destination points. A circulation plan is conceptual in that its adoption does not establish a precise alignment. An applicant for a partition is required to submit a circulation plan unless the applicant demonstrates to the planner one of the following:

1. *An existing street or proposed new street need not continue beyond the land to be divided in order to complete or extend an appropriate street system or to provide access to adjacent parcels within five hundred feet of the proposed development; or*
2. *The proposed street layout is consistent with a street pattern adopted as part of the city's transportation system plan, or a previously adopted circulation plan. (Ord. 828, 2013; Ord. 711 § 1 Exh. A, 2001; Ord. 634 § 1 Exh. A, 1995)*

Response: The subject property proposed for partition is adequately served by SE Elm Street and would not require new streets for future development.

17.152.070 Partition approval criteria.

Note: Only proposed Lots 1 and 2 are addressed in this section. There is no partition proposed for Lot 3100.

A request to partition land shall meet all of the following criteria:

- A. The proposed partition complies with all statutory and ordinance requirements and regulations;*

Response: The proposed partition complies with all statutory and ordinance requirements and regulations, including Oregon Administrative Rule (OAR) 660-012-0060 (Transportation Planning Rule - TPR).

B. Adequate public facilities are available to serve the proposal;

Response: As described in response to Section 17.22, there are adequate public facilities to serve the proposed partition.

It has been determined that there is sufficient capacity in sewer, water, street, school, police and fire systems to service the potential additional populace. The subject properties are already served by the Scappoose Rural Fire Protection District and Scappoose School District. There is public water, sewer and storm available for connection, per the description below, in SE Elm Street, adjacent to the subject properties. The adjacent right-of-way will be improved to City's Neighborhood Route standard. The properties are already served by the local street system. The proposed annexation and zone change would not increase the density of existing development on the subject properties.

Storm: There is an existing 24" storm system along the SE Elm Street frontage and two catch basins on the southside of SE Elm Street. As the subject properties are already developed there will be no additional contributions to the stormwater systems apart from what is redirected from the public right-of-way after construction of the new curb and sidewalk. As part of the frontage improvements the applicant will relocate and build a new catch basin to current PWDS.

Sewer: There is an existing 15" PVC sewer line along the frontage that conveys flows to the west and then north on Tussing Way toward E Columbia Ave. According to input from the City Engineer, this line has enough depth for all lots to be able to convey their flow in accordance with PWDS and Oregon Plumbing Specialty Code (OPSC).

Water: There is an existing 8" cast iron waterline along frontage the south side of the street and is available for all three lots to connect to.

This criterion is met.

C. All proposed lots conform to the size and dimensional requirements of this title; and

Response: Proposed lots 1 and 2 conform to the size and dimensional requirement of this title, as described in response to section 17.44 Low Density Residential (R-1). This criterion is met.

D. All proposed improvements meet city and applicable agency standards.

Response: All proposed improvement shave been designed to meet Title 17 criteria as well as Public Works Design Standards. This criterion is met.

E. Streets or roads are laid out so as to conform to the plats of subdivisions and maps of major partitions already approved for adjoining property as to width, general direction and in all other respects, including conformance with submitted neighborhood circulation plans, unless the city determines it is in the public interest to modify the street or road pattern. (Ord. 711 § 1 Exh. A, 2001; Ord. 634 § 1 Exh. A, 1995)

Response: The subject property proposed for partition is adequately served by SE Elm Street and would not require new streets for future development.

17.152.080 Special provisions for lots created through partition process.

Note: Only proposed Lots 1 and 2 are addressed in this section. There is no partition proposed for Lot 3100.

A. The minimum width of the building envelope area shall meet the lot requirement of the applicable zoning district.

Response: See **Sheet C-2 – Proposed Conditions Plan**. Proposed lots 1 and 2 are 80.4 and 74.1 feet wide, respectively, exceeding the minimum of 50 feet in the R-1 zone. This criterion is met.

B. The lot area shall be as required by the applicable zoning district. In the case of a flag lot, the accessway may not be included in the lot area calculation.

Response: See **Sheet C-2 – Proposed Conditions Plan**. Proposed new lots 1 and 2 will be 24,503 and 6,500 square feet, respectively. Therefore, they both meet the minimum lot size of 6,000 square feet in the proposed R-1 zone. This criterion is met.

C. Each lot created through the partition process shall front a public right-of-way as specified by the zoning designation. All flag lots shall be considered to be major variances and shall be subject to planning commission review and approval.

Response: The proposed new lots 1 and 2 both have frontage on the SE Elm Street right-of-way. This criterion is met.

D. Setbacks shall be as required by the applicable zoning district.

Response: See **Sheet C-2 – Proposed Conditions Plan**.

Proposed Lot 1 will have a front setback of 23.1 feet (exceeding the 20-foot minimum), side setbacks of 19.5 and 8.3 feet (totaling 27.8 feet, more than the 15-foot minimum, with one exceeding 10 feet), and a rear setback of 147 feet (exceeding the 20-foot minimum). The detached garage has side setbacks of 36.5 and 88.0 feet (exceeding the 5-foot minimum for accessory structures) and a rear setback of 7 feet (exceeding the 5-foot minimum for accessory structures).

Proposed Lot 2 will have a front setback of 16.8 feet after the required dedication of 10 feet of frontage to SE Elm Street public right-of-way. This substandard setback is deemed acceptable as it allows for the development of a full 60-foot-wide street section per the City's TSP and public works design standards. Proposed Lot 2 will have side setbacks of 10.4 and 15.4 feet, and a rear setback of 39.2 feet. These also exceed the minimum requirements in the R-1 zone. This criterion is met.

E. When the partitioned lot is a flag lot, the developer may determine the location of the front yard, provided that no side yard is less than ten feet. Structures shall generally be located so as to maximize separation from existing structures.

Response: There are no flag lots proposed to be partitioned or proposed to be created by a partition. This criterion is not applicable.

F. Screening to the standards included in Section 17.100.090, may be required along the property line of a lot of record where the paved drive of an accessway is located within ten feet of an abutting lot. Screening to the standards included in Section 17.100.090 may also be required to maintain privacy for abutting lots and to provide usable outdoor recreation areas for proposed development.

Response: The existing driveway on the proposed Lot 1 is setback 5 feet from the western property line. There is a fence on the adjacent lot to the west that screens the driveway from view. This criterion is met.

G. The Scappoose fire district may require the installation of a fire hydrant where the length of an accessway would have a detrimental effect on firefighting capabilities and may require provision of an emergency vehicle turnaround.

Response: The proposed partition will not increase the length of existing accessways and will not have a detrimental effect on firefighting capabilities, as the subject properties are already served by the Scappoose Fire District.

H. No greater than three single-family detached dwelling units may be served by a common drive. Use of a common drive for access to more than three dwelling units other than single-family detached may be required and shall be subject to the approval of the planner, public works director and the planning commission. Where a common drive is to be provided, a reciprocal easement which will ensure access and maintenance rights shall be recorded with the approved partition map.

Response: See **Sheet C-2 – Proposed Conditions Plan** and **Exhibit A – Preliminary Plat**. It is proposed that there be a shared driveway between lot 3100 and the new (eastern) lot created by the partition. An access easement will also be

provided for lot 3000 to continue use of the flag lot driveway to access the existing detached garage to the rear of the main house if one does not already exist. This criterion is met.

I. Any access way shall be paved and shall comply with the standards set forth in public works design standards.

Response: The access ways for proposed Lots 1 and 2 will be paved and will comply with the PWDS standards.

J. Where landfill and/or development is allowed within the floodway fringe, the city may require the dedication of sufficient open land area for greenway adjoining and within the floodplain. This area shall include portions at a suitable elevation for the construction of a pedestrian/bicycle pathway with the floodplain. (Ord. 634 § 1 Exh. A, 1995)

Response: The subject site is not within the floodway fringe. This criterion is not applicable.

17.154 Street and Utility Improvement Standards

17.154.030 Streets.

A. No development shall occur unless the development has frontage or approved access to a public street:

1. Streets within a development and streets adjacent to a development shall be improved in accordance with this title and the public works design standards and specifications.

Response: See **Sheet C-3 – Frontage Improvement Plan**. The site's frontage on SE Elm Street will be improved in accordance with PWDS for the Neighborhood Route classification. This criterion is met.

2. Any new street or additional street width planned as a portion of an approved street plan shall be dedicated and improved in accordance with this title and the public works design standards and specifications.

Response: See **Sheet C-2 – Proposed Conditions Plan**. Proposed lots 1 and 2 will dedicate 10 feet of frontage to the SE Elm Street public right-of-way to bring its width up to the standard specified by the City's TSP and PWDS.

[...]

B. Rights-of-way shall be created through the approval of a final subdivision plat or major partition; however, the council may approve the creation of a street by acceptance of a deed, provided that such street is deemed essential by the council for the purpose of general traffic circulation:

[...]

Response: No new right-of-way is proposed. These criteria are not applicable.

[...]

E. The street right-of-way and roadway widths shall not be less than the minimum widths described in the city's public works design standards.

Response: See **Sheet C-3 – Frontage Improvement Plan**. After dedication of 10 feet of frontage from proposed Lots 1 and 2, the adjacent section of SE Elm Street public right of way will be 60 feet wide and the travel lanes will be 18 feet wide, in accordance with the minimum widths described in the City's PWDS. This criterion is met.

[...]

H. Concrete vertical curbs, curb cuts, wheelchair, bicycle ramps and driveway approaches shall be constructed in accordance with standards specified in this chapter and the city's public works design standards. Concrete curbs and driveway approaches are required and shall be built to the city's configuration standards.

Response: See **Sheet C-3 – Frontage Improvement Plan**. Concrete vertical curbs, curb cuts and driveway approaches will be constructed in accordance with the standards specified in this chapter and the city's PWDS.

[...]

P. Joint mailbox facilities shall be provided in all residential developments, with each joint mailbox serving at least two dwelling units.

- 1. Joint mailbox structures shall be placed adjacent to roadway curbs and shall comply with provisions of the Americans with Disabilities Act and implementing federal and state regulations;*
- 2. Proposed locations of joint mailboxes shall be designated on a copy of the tentative plan, and shall be approved by the U.S. Post Office prior to plan approval; and*
- 3. Plans for the joint mailbox structures to be used shall be submitted for approval by the planner prior to final approval.*

Response: See **Sheet C-3 – Frontage Improvement Plan**. A joint mailbox structure will be placed within the right-of-way, with the exact location to be determined pending approval by USPS. This criterion is met.

[...]

R. Street lights shall be installed in accordance with the city's public works design standards.

Response: See **Sheet C-3 – Frontage Improvement Plan**. One new streetlight will be installed in the new planting strip just west of the access drive for proposed Lot 2, in accordance with the City's PWDS. This criterion is met.

S. A Transportation Impact Study (TIS) must be submitted with a land use application if the conditions in (1) or (2) apply in order to determine whether conditions are needed to protect and minimize impacts to transportation facilities, consistent with Section 660-012-0045(2)(b) and (e) of the State Transportation Planning Rule.

- 1. Applicability - TIS letter. A TIS letter shall be required to be submitted with a land use application to document the expected vehicle trip generation of the proposal. The expected number of trips shall be documented in both total peak hour trips and total daily trips. Trip generation shall be estimated for the proposed project using the latest edition of the Institute of Engineers Trip Generation Manual or, when verified with the City prior to use, trip generation surveys conducted at similar facilities.*
- 2. Applicability - TIS report. A TIS report shall be required to be submitted with a land use application if the proposal is expected to involve one or more of the following:*
 - a. The proposed development would generate more than 10 peak hour trips or more than 100 daily trips.*
 - b. The proposal is immediately adjacent to an intersection that is functioning at a poor level of service, as determined by the city engineer.*
 - c. A new direct approach to US 30 is proposed.*
 - d. A proposed development or land use action that the road authority states may contribute to operational or safety concerns on its facility(ies).*
 - e. An amendment to the Scappoose Comprehensive Plan or Zoning Map is proposed.*

3. *Consistent with the city's Traffic Impact Study (TIS) Guidelines, the city engineer will determine the project study area, intersections for analysis, scenarios to be evaluated and any other pertinent information concerning the study and what must be addressed in either a TIS letter or a TIS report.*
4. *Approval Criteria. When a TIS Letter or Report is required, a proposal is subject to the following criteria:*
 - a. *The TIS addresses the applicable elements identified by the city engineer, consistent with the Traffic Impact Study Guidelines;*
 - b. *The TIS demonstrates that adequate transportation facilities exist to serve the proposed development or, in the case of a TIS report, identifies mitigation measures that resolve identified traffic safety problems in a manner that is satisfactory to the city engineer and, when state highway facilities are affected, to ODOT;*
 - c. *For affected non-highway facilities, the TIS report establishes that mobility standards adopted by the city have been met; and*
 - d. *Proposed public improvements are designed and will be constructed consistent with Public Works Design Standards and access standards in the Transportation System Plan.*

Response: The proposed annexation, zone change and partition are not expected to generate any new trips as the properties are already developed. Although an amendment to the Scappoose Zoning Map is proposed, it does not change the level of traffic anticipated by the Comprehensive Plan designation and Transportation System Plan functional classification of SE Elm Street. Therefore, a TIS report should not be required.

[...]

17.154.050 Easements.

A. *Easements for sewers, drainage, water mains, electric lines or other public utilities shall be either dedicated or provided for in the deed restrictions, and where a subdivision is traversed by a watercourse, drainageway, channel or stream, there shall be provided a stormwater easement or drainage right-of-way conforming substantially with the lines of such watercourse and such further width as will be adequate for conveyance and maintenance.*

Response: See **Sheet C-2 – Proposed Conditions Plan** (keynote 16). An 8-foot-wide PUD easement along the SE Elm Street frontage of the subject properties will be dedicated/provided for in the deed restriction. This criterion is met.

B. *A property owner proposing a development shall make arrangements with the city, the applicable district and each utility franchise for the provision and dedication of utility easements necessary to provide full services to the development. (Ord. 634 § 1 Exh. A, 1995)*

Response: The property owners will make arrangements for utility franchises and dedication of utility easements. This criterion is met.

17.154.070 Sidewalks.

A. *Sidewalks are required and shall be constructed, replaced or repaired in accordance with the city's public works design standards.*

Response: See **Sheet C-3 – Frontage Improvement Plan**. New 6-foot-wide sidewalks will be constructed in accordance with PWDS specifications for a Neighborhood Collector.

B. *Maintenance of sidewalks and curbs is the continuing obligation of the adjacent property owner.*

Response: It is understood that maintenance of sidewalks and curbs is the continuing obligation of the adjacent property owner.

[...]

17.154.090 Sanitary sewers.

A. *Sanitary sewers shall be installed to serve each new development and to connect developments to existing mains in accordance with the provisions set forth by the city's public works design standards and the adopted policies of the comprehensive plan.*

B. *The public works director shall approve all sanitary sewer plans and proposed systems prior to issuance of development permits involving sewer service.*

C. *Proposed sewer systems shall include consideration of additional development within the area as projected by the comprehensive plan and the wastewater treatment facility plan and potential flow upstream in the sewer sub-basin.*

D. *Applications shall be denied by the approval authority where a deficiency exists in the existing sewer system or portion thereof which cannot be rectified within the development and which if not rectified will result in a threat to public health or safety, surcharging of existing mains, or violations of state or federal standards pertaining to operation of the sewage treatment system. (Ord. 634 § 1 Exh. A, 1995)*

Response: See **Sheet C-3 – Frontage Improvement Plan**. New sewer laterals will be installed for each lot and connected to the existing sanitary sewer main in SE Elm Street in accordance with the city's PWDS and comprehensive plan. This criterion is met.

17.154.100 Storm drainage.

A. *The planner and public works director shall issue permits only where adequate provisions for stormwater and floodwater runoff have been made, and:*

1. *The stormwater drainage system shall be separate and independent of any sanitary sewerage system.*
2. *Where possible, inlets shall be provided so surface water is not carried across any intersection or allowed to flood any street.*
3. *Surface water drainage patterns shall be shown on every development proposal plan.*
4. *All stormwater analysis and calculations shall be submitted with proposed plans for public works directors review and approval.*
5. *All stormwater construction materials shall be subject to approval of the public works director.*

B. *Where a subdivision is traversed by a watercourse, drainageway, channel or stream, there shall be provided a stormwater easement or drainage right-of-way conforming substantially with the lines of such watercourse and such further width as will be adequate for conveyance and maintenance.*

C. *A culvert or other drainage facility shall, and in each case be, large enough to accommodate potential runoff from its entire upstream drainage area, whether inside or outside the development. The public works director shall determine the necessary size of the facility.*

D. *Where it is anticipated by the public works director that the additional runoff resulting from the development will overload an existing drainage facility, the planner and engineer shall withhold approval of the development until provisions have been made for improvement of the potential condition or until provisions have been made for storage of additional runoff caused by the development. (Ord. 634 § 1 Exh. A, 1995)*

Response: See **Sheet C-3 – Frontage Improvement Plan**. The subject properties are not traversed by a watercourse, drainageway, channel or stream. As the subject properties are already developed, there will not be a significant addition

of stormwater to the public storm system. Runoff from newly paved areas will be directed toward a relocated catch basin on the south side of SE Elm Street, just north of the access drive for proposed Lot 2.

17.154.105 Water system.

The planner and public works director shall issue permits only where provisions for municipal water system extensions have been made, and:

- A. Any water system extension shall be designed in compliance with the comprehensive plan existing water system plans.*
- B. Extensions shall be made in such a manner as to provide for adequate flow and gridding of the system.*
- C. The public works director shall approve all water system construction materials. (Ord. 634 § 1 Exh. A, 1995)*

Response: See **Sheet C-3 – Frontage Improvement Plan**. It is proposed that water service be extended to the two new parcels (lot 1 and lot 2) by connecting to the existing public water main in SE Elm Street via new 1" laterals with $\frac{3}{4}$ " water meters. Lot 3100 will continue to use its existing well as a water source.

[...]

17.154.120 Utilities.

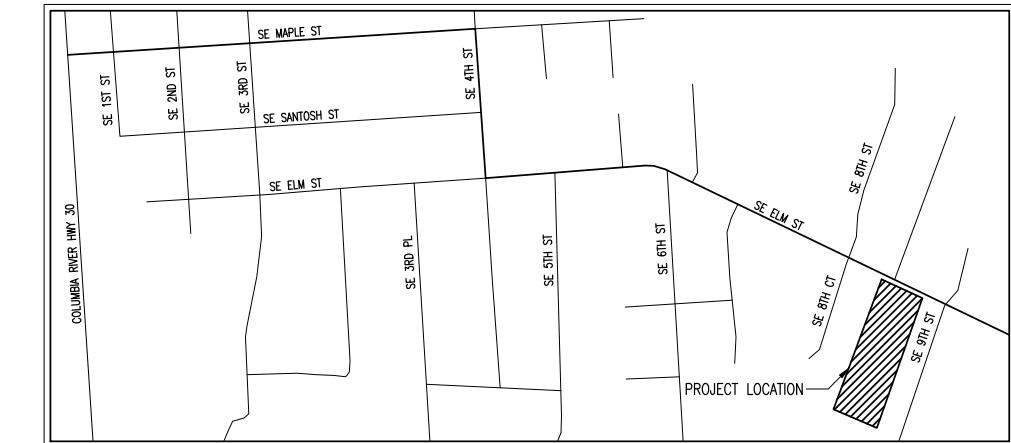
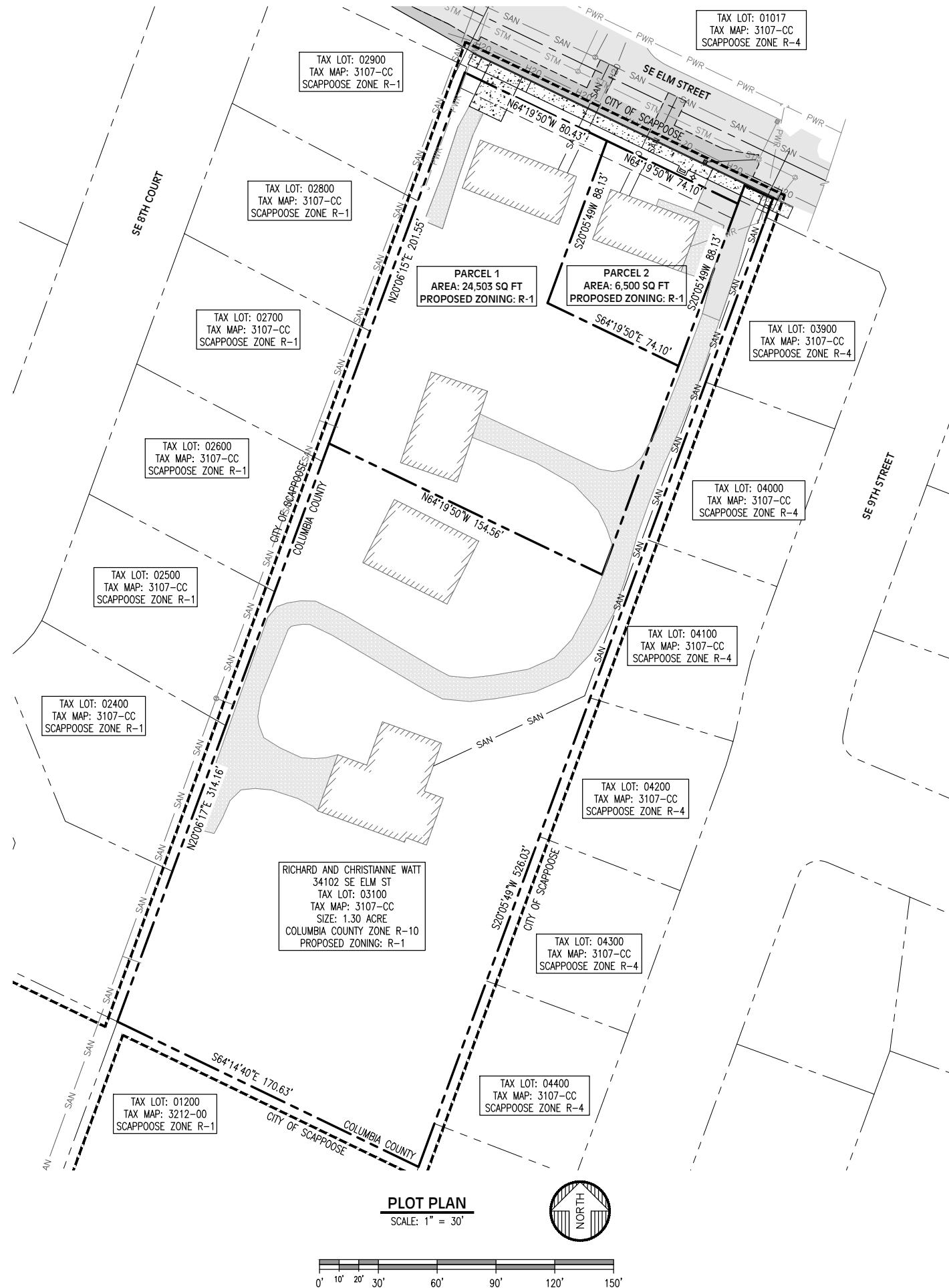
A. All utility lines including, but not limited to those required for electric, communication, lighting and cable television services and related facilities shall be placed underground, except for surface mounted transformers, surface mounted connection boxes and meter cabinets which may be placed above ground, temporary utility service facilities during construction, high capacity electric lines operating at fifty thousand volts or above, and:

- 1. The applicant shall make all necessary arrangements with the serving utility to provide the underground services;*
- 2. The city reserves the right to approve location of all surface mounted facilities;*
- 3. All underground utilities, including sanitary sewers, water lines, and storm drains installed in streets by the applicant, shall be constructed prior to the surfacing of the streets; and*
- 4. Stubs for service connections shall be long enough to avoid disturbing the street improvements when service connections are made.*

Response: Both subject properties already have internet and electric service. New underground utility service connections to water and sewer will be made as part of this proposal, but no stubs are required.

[...]

ELM STREET
PARTITION AND ANNEXATION
34094 & 34102 SE ELM ST
SCAPPOOSE, OR 97056
BRAD HENDRICKSON



| MASTER LEGEND | |
|-----------------------------|----------------------------------|
| | SUBJECT PROPERTY LINE |
| | ADJACENT PROPERTY LINE |
| | CITY LIMITS (OFFSET FOR CLARITY) |
| (E) AC ROADWAY | |
| (E) GRAVEL | |
| (N) AC ROADWAY | |
| (N) CONCRETE | |
| (N) SANITARY LINE | |
| (N) WATER LINE | |
| (E) SANITARY LINE | |
| (E) WATER LINE | |
| (E) STORM LINE | |
| (E) POWER LINE | |
| (N) PUBLIC UTILITY EASEMENT | |
| (E) MAJOR CONTOUR | |
| (E) MINOR CONTOUR | |
| (E) STRUCTURE | |
| ZONING SETBACK LINE | |

| DRAWING INDEX | |
|---------------|---------------------------|
| SHEET | DESCRIPTION |
| G-1 | GENERAL |
| | COVER SHEET |
| C-1 | CIVIL |
| C-2 | EXISTING CONDITIONS PLAN |
| C-3 | PROPOSED CONDITIONS PLAN |
| | FRONTAGE IMPROVEMENT PLAN |
| D-1 | DETAILS |
| | STANDARD DETAILS |

Exhibit 4A

DATE: 09/11/2025
REVISED PRINT
VOID ALL PREVIOUS

DATE: 02/25/2025
PRELIMINARY
NOT FOR CONSTRUCTION

G-1

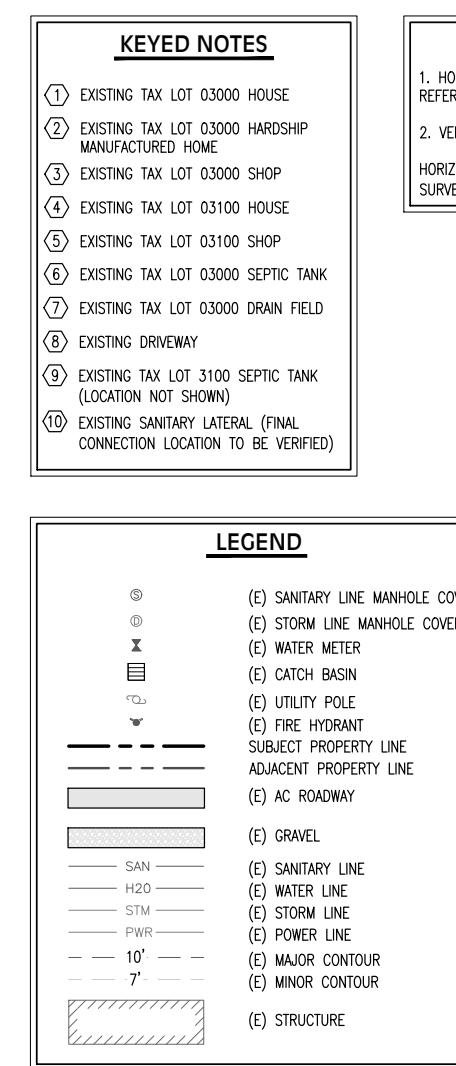
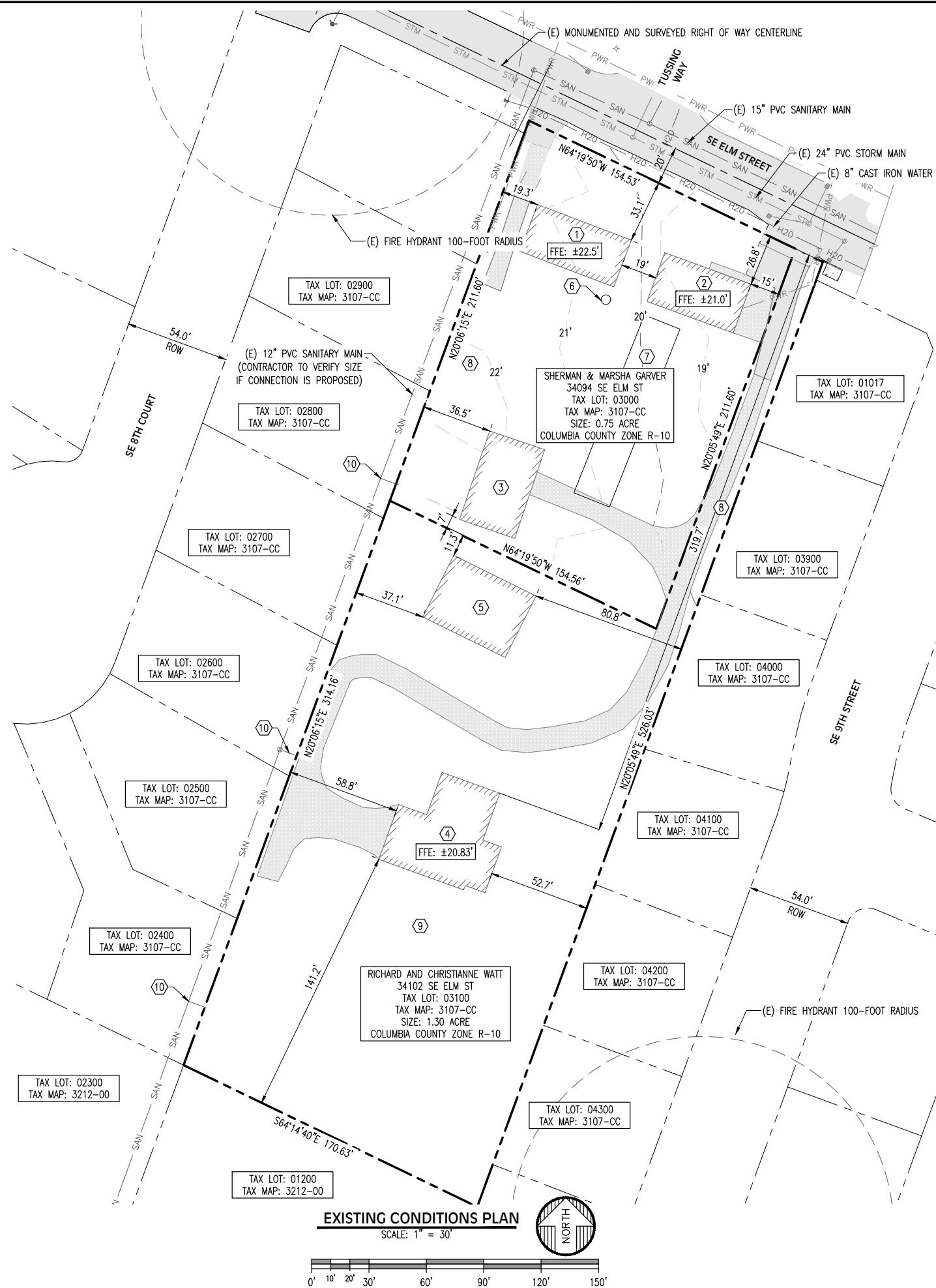


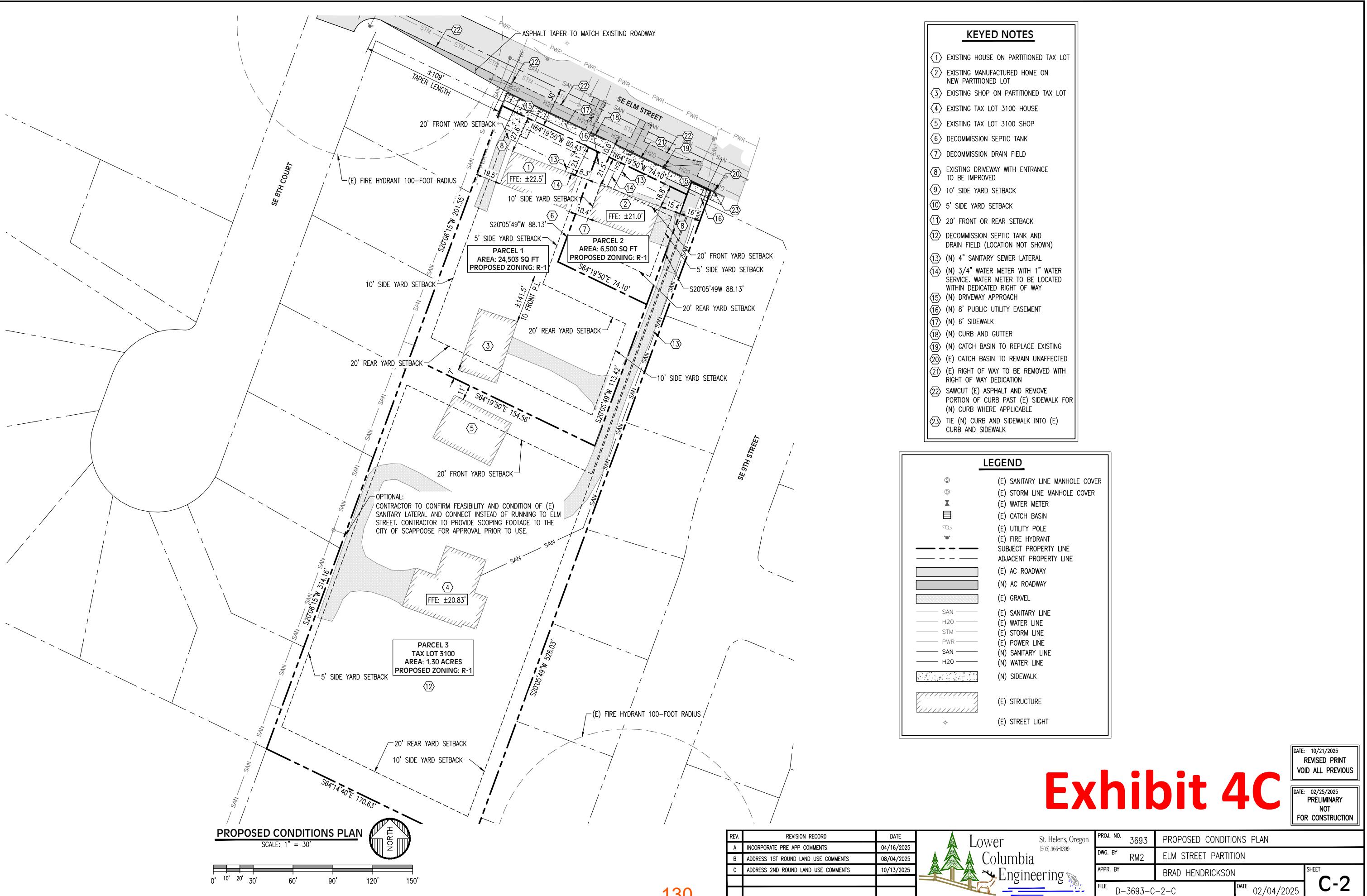
Exhibit 4B

DATE: 10/21/2025
REVISED PRINT
VOID ALL PREVIOUS

DATE: 02/25/2025
PRELIMINARY
NOT FOR CONSTRUCTION

| REV. | REVISION RECORD | DATE | PROJ. NO. | EXISTING CONDITIONS PLAN |
|------|-------------------------------------|------------|-----------|--------------------------|
| A | INCORPORATE PRE APP COMMENTS | 04/16/2025 | 3693 | |
| B | ADDRESS 1ST ROUND LAND USE COMMENTS | 08/04/2025 | DWG. BY | ELM STREET PARTITION |
| C | ADDRESS 2ND ROUND LAND USE COMMENTS | 10/13/2025 | APPR. BY | BRAD HENDRICKSON |
| | | | FILE | D-3693-C-1-C |
| | | | DATE | 02/04/2025 |





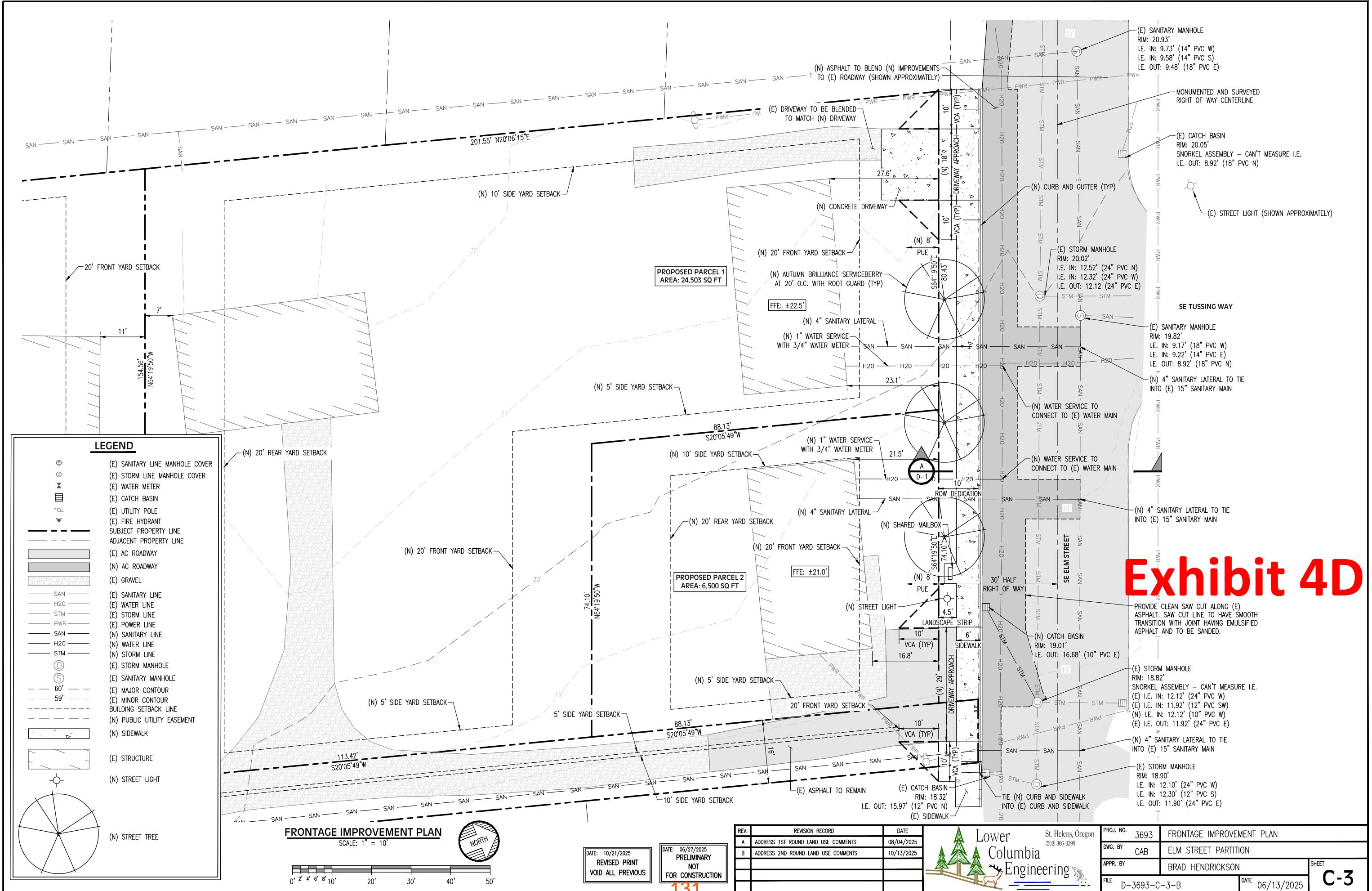
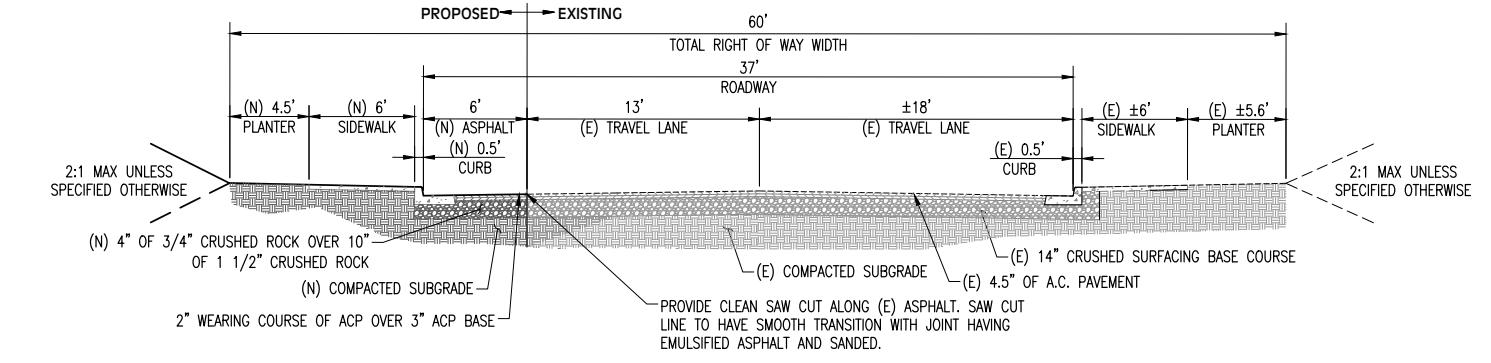


Exhibit 4D



COMPACTON STANDARDS:

- ASPHALTIC CONCRETE (AC): 91%/92% OF RICE DENSITY AASHTO T-209
- CRUSHED AGGREGATE BASE 3/4": (LEVELING COURSE): 95% OF MODIFIED PROCTOR AASHTO T-180
- CRUSHED AGGREGATE BASE 1-1/2": 95% OF MODIFIED PROCTOR AASHTO T-180
- SUBGRADE: 95% OF STANDARD PROCTOR AASHTO T-99 OR EQUIVALENT

A ELM STREET ROADWAY SECTION
D-1 SCALE: NTS

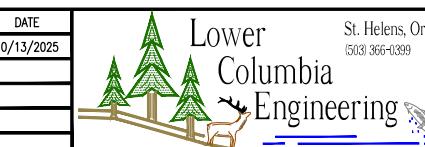
Exhibit 4E

DATE: 10/21/2025
REVISED PRINT
VOID ALL PREVIOUS

DATE: 06/27/2025
PRELIMINARY
NOT
FOR CONSTRUCTION

132

| REV. | REVISION RECORD | DATE | PROJ. NO. | STANDARD DETAILS |
|------|-------------------------------------|------------|----------------|----------------------------------|
| A | ADDRESS 2ND ROUND LAND USE COMMENTS | 10/13/2025 | (503) 366-0399 | DWG. BY CAB ELM STREET PARTITION |
| | | | APPR. BY | BRAD HENDRICKSON |
| | | | FILE | D-3693-D-1-A |
| | | | DATE | 06/13/2025 |



D-1

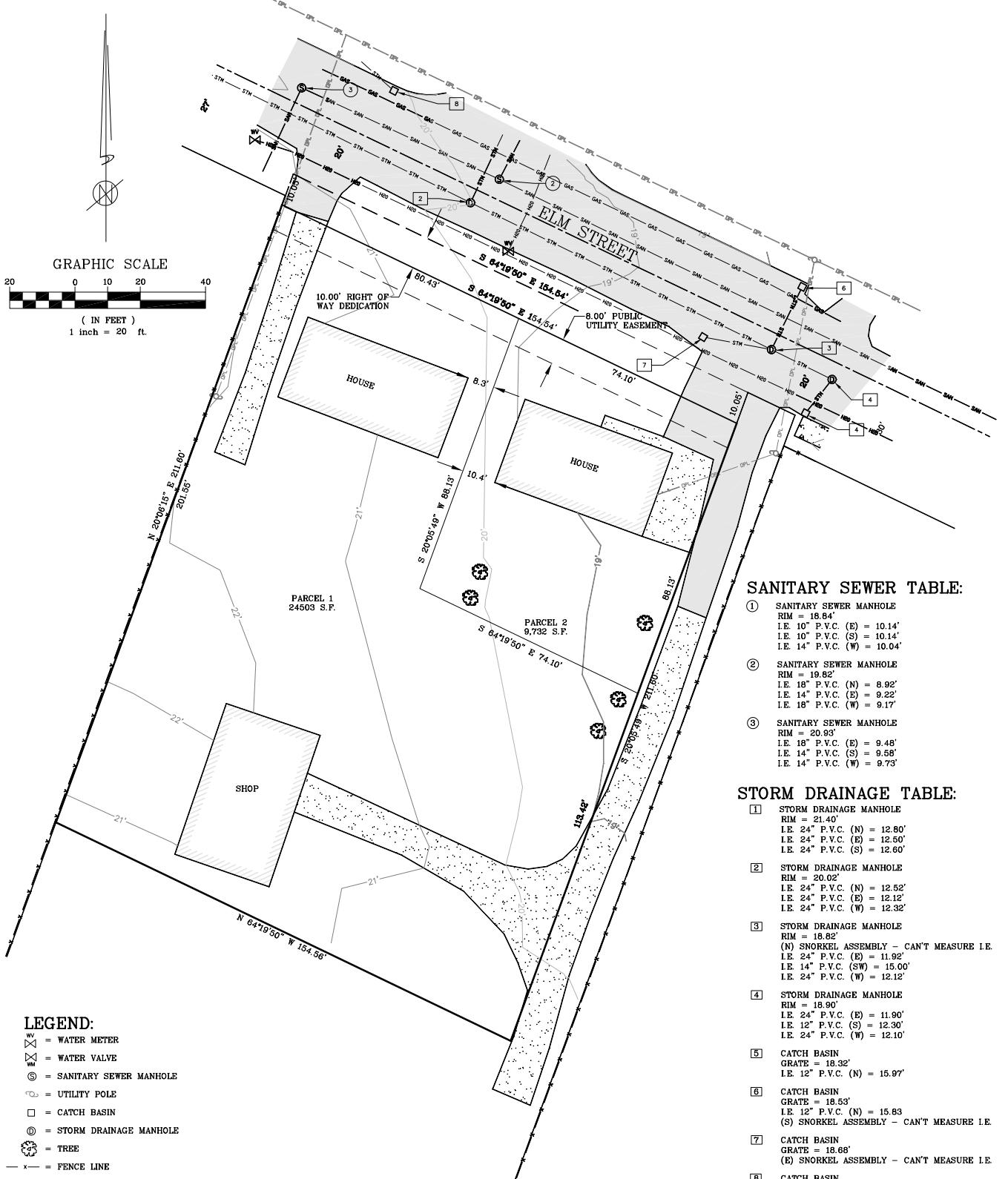


Exhibit 4F

NOTES:

1. HORIZONTAL DATUM = OREGON COORDINATE REFERENCE SYSTEM, COLUMBIA RIVER WEST ZONE
2. VERTICAL DATUM - NAVD88

REGISTERED
PROFESSIONAL
LAND SURVEYOR

JANUARY 19, 1993
DONALD D WALLACE JR
2601
RENEWS 6/30/26

K
S
L
N
K.L.S. SURVEYING INC.
1224 ALDER STREET
VERNONIA, OREGON 97064
(503) 429-6115

TOPOGRAPHIC SURVEY FOR
BRAD HENDRICKSON
IN THE SW 1/4 OF SECTION 7,
T3N, R1W, W.M.
COLUMBIA COUNTY, OREGON
SCALE 1" = 20' JUNE 7, 2025

DRAWN BY: SWM

FIELD: SWM/DDR

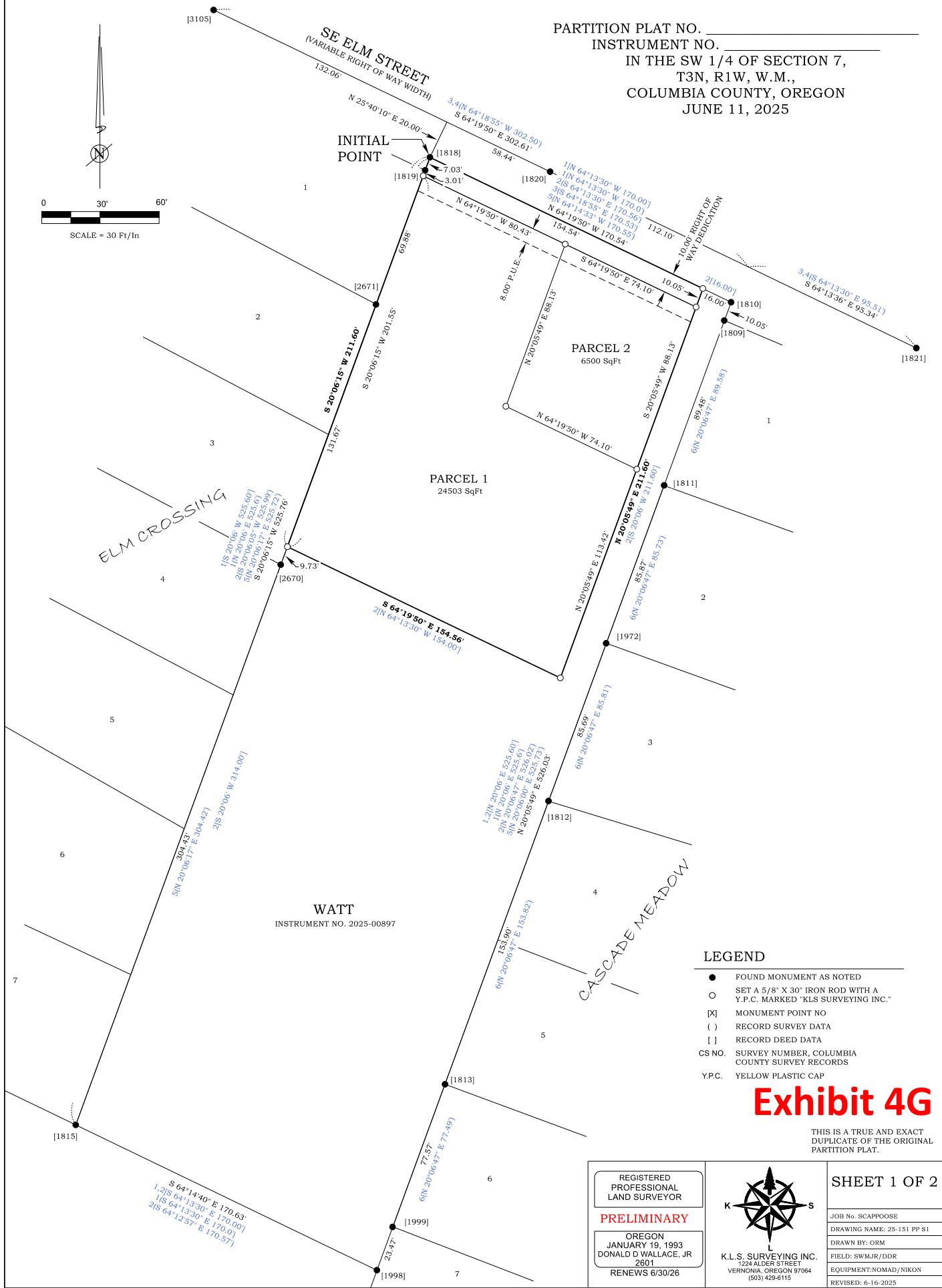
EQUIPMENT: FOCUS36

JOB No. 23-135

PROJECT No. 23-135

REVISED: 6/15/2025

PARTITION PLAT NO. _____
 INSTRUMENT NO. _____
 IN THE SW 1/4 OF SECTION 7,
 T3N, R1W, W.M.,
 COLUMBIA COUNTY, OREGON
 JUNE 11, 2025



NARRATIVE:

- THIS SURVEY WAS COMPLETED AT THE REQUEST OF BRAD HENDRICKSON TO MONUMENT A PARTITION OF THAT TRACT OF LAND DESCRIBED IN CLERKS INSTRUMENT NO. 1993-5273 COLUMBIA COUNTY DEED RECORDS.

- THE BASIS OF BEARINGS IS OREGON COORDINATE REFERENCE SYSTEM COLUMBIA RIVER WEST ZONE, NAD83 (2011) EPOCH 2010.00.

- FOR CONTROL I HELD THE MONUMENTS AS NOTED IN THE MONUMENT NOTES.

SE ELM STREET:

- I HELD MONUMENTS 3105, 1820, & 1821 FOR THE CENTERLINE, RIGHT OF WAY WIDTH VARIES.

INSTRUMENT NO. 1993-5273:

- I HELD MONUMENTS 1810 & 1998 FOR THE NE AND SE CORNERS. I HELD MONUMENTS 1998 & 1815 FOR THE SOUTH LINE. I HELD MONUMENTS 1815 & 1818 FOR THE WEST LINE. I HELD MONUMENTS 1818 & 1810 FOR THE NORTH LINE.

INSTRUMENT NO. 2025-00897:

- I ESTABLISHED THE MOST NORTHERLY NORTHWEST CORNER AT THE DEEDED DISTANCE [16.00'] NORTHWESTERLY FROM MONUMENT NO. 1810. I ESTABLISHED THE MOST SOUTHERLY NORTHWEST CORNER PARALLEL WITH THE WEST LINE OF SE ELM STREET WATT TRACT AT THE DEEDED DISTANCE [211.60']. I ESTABLISHED THE MOST WESTERLY NORTHWEST CORNER PARALLEL WITH THE CENTERLINE OF SE ELM STREET AT THE DEEDED DISTANCE [154.00'] WHICH I EXTENDED TO BE ON THE EAST LINE OF ELM CROSSING.

MONUMENT NOTES:

[1809] FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "RAJ LS 2725" PER CASCADE MEADOW, (HELD)

[1810] FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "RAJ LS 2725" PER CS NO. 5175, (HELD)

[1811] FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "RAJ LS 2725" PER CASCADE MEADOW, (HELD)

[1812] FOUND A 5/8" IRON ROD WITH AN ILLEGIBLE Y.P.C. UP 0.20' PER CASCADE MEADOW, (HELD)

[1813] FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "RAJ LS 2725" PER CASCADE MEADOW, (HELD)

[1815] FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "BRADY 1869" PER A 1997 UNRECORDED SURVEY AS NOTED ON ELM CROSSING, (HELD)

[1818] FOUND A 1/2" IRON PIPE PER CS NO. 377, (HELD)

[1819] FOUND A 5/8" IRON ROD WITH AN ILLEGIBLE Y.P.C. IN CONCRETE AT A FENCE CORNER PER ELM CROSSING, (HELD)

[1820] FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "RAJ LS 2725" INSIDE A MONUMENT BOX PER TUSSING, (HELD)

[1821] FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "RAJ LS 2725" INSIDE A MONUMENT BOX PER CASCADE MEADOW, (HELD)

[1972] FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "RAJ LS 2725" PER CASCADE MEADOW, (HELD)

[1998] FOUND A 1/2" IRON PIPE PER CS NO. 377, (HELD)

[1999] FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "RAJ LS 2725" PER CASCADE MEADOW, (HELD)

[2670] FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "KEENON LAND SERVICES INC." PER ELM CROSSING, (HELD)

[2671] FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "KEENON LAND SERVICES INC." PER ELM CROSSING, (HELD)

[3105] FOUND A 5/8" IRON ROD WITH A 2' ALUMINUM CAP MARKED "OTAK INC" PER CHINOOK CROSSING WEST, (HELD)

SURVEYOR'S CERTIFICATE:

I, DONALD D. WALLACE, JR., DO HEREBY CERTIFY THAT I HAVE CONDUCTED A SURVEY AND MARKED WITH PROTRACTED MONUMENTS, IN ACCORDANCE WITH O.R.S. CHAPTER 92, THE SOUTHWEST QUARTER OF SECTION 7, TOWNSHIP 3 NORTH, RANGE 1 WEST OF THE WILLAMETTE MERIDIAN, COLUMBIA COUNTY, OREGON, THE BOUNDARY BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE INITIAL POINT BEING A 1/2" IRON PIPE AT THE NORTHWEST CORNER OF THAT TRACT OF LAND CONVEYED TO SHERMAN R. & MARSHA J. GARVER IN DEED RECORDED AS INSTRUMENT NO. 1993-5273, COLUMBIA COUNTY, OREGON, THE SOUTHWEST QUARTER ALONG THE WEST LINE OF SAID GARVER TRACT 211.60 FEET TO THE SOUTHWEST CORNER THEREOF, THENCE SOUTH 64°19'50" EAST ALONG THE SOUTHWEST LINE OF SAID GARVER TRACT 154.56 FEET TO THE SOUTHEAST CORNER THEREOF, THENCE NORTH 23°40'40" EAST ALONG THE EAST LINE OF SAID GARVER TRACT 211.60 FEET TO THE SOUTH RIGHT OF WAY LINE OF SE ELM STREET, THENCE NORTH 64°19'50" WEST ALONG SAID SOUTH LINE 154.54 FEET TO THE INITIAL POINT.

NOTES:

I, THIS PLAT IS SUBJECT TO THE TERMS & CONDITIONS PER THE COLUMBIA COUNTY LAND USE & PLANNING DEPARTMENT CASEFILE NO. _____

DECLARATION:

KNOW ALL PEOPLE BY THESE PRESENTS THAT SHERMAN R. & MARSHA J. GARVER ARE THE OWNERS OF THE LAND REPRESENTED ON THE ANNEXED PLAT, AND MOST PARTICULARLY DESCRIBED IN THE ACCOMPANYING SURVEYOR'S CERTIFICATE, AND THAT THE PARTIES TO THE SURETY AGREEMENT PARTITIONED INTO PARCELS 1 & 2 AS SHOWN ON THE ANNEXED PARTITION PLAT IN ACCORDANCE WITH O.R.S. CHAPTER 92 AND DOES HEREBY GRANT ALL EASEMENTS AS SHOWN OR NOTED ON SAID PLAT AND HEREBY DEDICATE THE ADDITIONAL RIGHT OF WAY TO THE PUBLIC

SHERMAN R. GARVER MARSHA J. GARVER

REFERENCES:

SURVEYS:
1 = CS NO. 377
2 = CS NO. 5175
3 = CS NO. 5435
4 = CHINOOK
5 = ELM CROSSING
6 = CASCADE MEADOW
7 = CHINOOK CROSSING WEST

DEEDS
1 = INSTRUMENT NO. 1993-5273
2 = INSTRUMENT NO. 2025-00897

THIS IS A TRUE AND EXACT DUPLICATE OF THE ORIGINAL PARTITION PLAT.

PARTITION PLAT NO. _____
INSTRUMENT NO. _____
IN THE SW 1/4 OF SECTION 7,
T3N, R1W, W.M.,
COLUMBIA COUNTY, OREGON
JUNE 11, 2025

COLUMBIA COUNTY APPROVALS:

APPROVED THIS ____ DAY OF _____, 20_____
COLUMBIA COUNTY PLANNING DEPARTMENT

BY _____

APPROVED THIS ____ DAY OF _____, 20_____
COLUMBIA COUNTY SURVEYOR

BY _____

ALL TAXES, FEES, ASSESSMENTS OR OTHER CHARGES AS PROVIDED BY
ORS 92.095 HAVE BEEN PAID AS OF THIS ____ DAY OF _____

COLUMBIA COUNTY TAX COLLECTOR

BY _____

STATE OF OREGON) SS
COUNTY OF COLUMBIA)

I DO HEREBY CERTIFY THAT THIS PARTITION PLAT WAS RECEIVED
FOR RECORD ON THIS ____ DAY OF _____, AT
O'CLOCK, AND RECORDED AS INSTRUMENT NO. _____
AND AS PARTITION PLAT NUMBER _____, COLUMBIA COUNTY RECORDS

BY _____
COLUMBIA COUNTY CLERK

ACKNOWLEDGEMENT:

STATE OF OREGON) SS
COUNTY OF COLUMBIA)

THIS INSTRUMENT WAS ACKNOWLEDGED BEFORE ME ON
_____, 20_____, BY SHERMAN R. GARVER & MARSHA J. GARVER

NOTARY SIGNATURE _____

NOTARY PUBLIC - OREGON _____

COMMISSION NO. _____

MY COMMISSION EXPIRES _____

| |
|---|
| REGISTERED PROFESSIONAL LAND SURVEYOR |
| PRELIMINARY |
| OREGON JANUARY 19, 1993 DONALD D. WALLACE, JR. RENEWED 6/30/26 |



SHEET 2 OF 2
JOB NO. SCAPPPOSE
DRAWING NAME: 25-151 PP S2
DRAWN BY: ORM
FIELD: SWMLJR/DDR
EQUIPMENT: NOMAD/NIKON
REVISED: 6-16-2025



KLS Surveying Inc.

1224 Alder Street
Vernonia, OR 97064

Phone: (503) 429-6115
Fax: (866) 297-1402
Email: don@klssurveying.com

Exhibit A Annexation description

A tract of land lying in the Southwest quarter of Section 7, Township 3 North, Range 1 West of the Willamette Meridian, Washington County, Oregon being more particularly described as follows:

- Beginning at a point North 211.12 feet and East 206.73 feet from the Southwest corner of said Section 7;
- thence South $64^{\circ}14'40''$ East 170.63 feet to the West line of Cascade Meadow, Columbia County Survey Records;
- thence North $20^{\circ}05'49''$ East along said West line 526.03 feet to the South line of SE Elm Street;
- thence North $64^{\circ}19'50''$ West along said South line 170.54 feet;
- thence South $20^{\circ}06'15''$ West 525.76 feet to the point of beginning.

Containing 2.05 acres more or less.

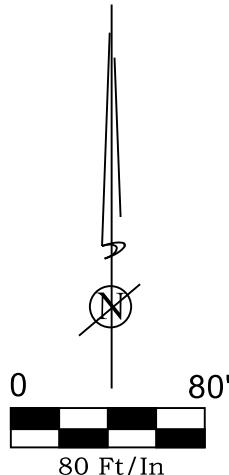
The basis of bearings for this description is Oregon Coordinate Reference System Columbia River West Zone, NAD83 (2011) EPOCH 2010.00.

REGISTERED
PROFESSIONAL
LAND SURVEYOR

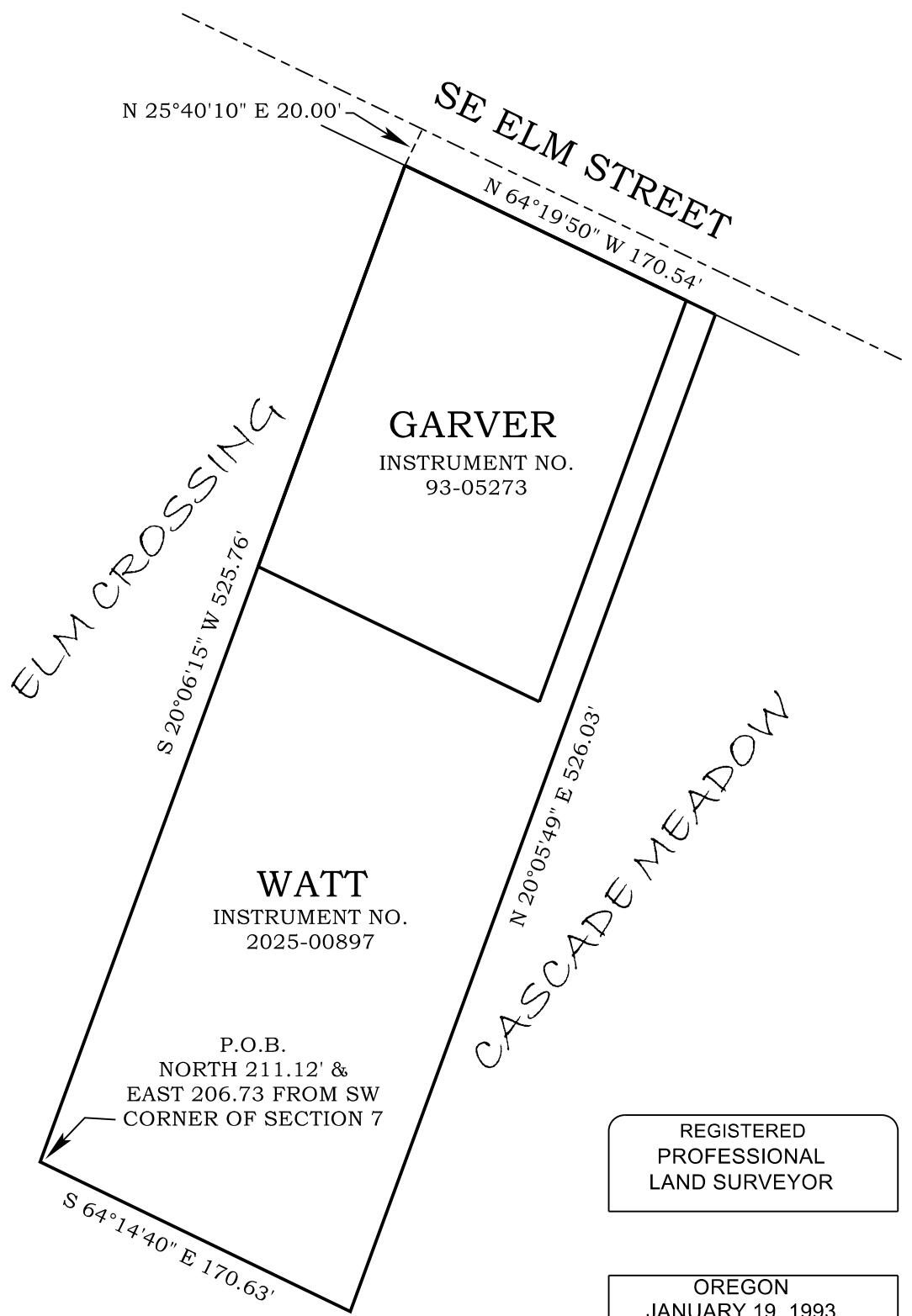
OREGON
JANUARY 19, 1993
DONALD D WALLACE, JR
2601

RENEWS 6/30/26

EXHIBIT B
ANNEXATION AREA
IN THE SW 1/4 OF SECTION 7,
T3N, R1W, W.M.,
COLUMBIA COUNTY, OREGON



DWG NO. 25-151 EXHIBIT
JOB NO. SCAPPOOSE
10-15-2025



K.L.S. SURVEYING INC.
1224 ALDER STREET
VERNONIA, OREGON 97064
(503) 429-6115

REGISTERED
PROFESSIONAL
LAND SURVEYOR

OREGON
JANUARY 19, 1993
DONALD D WALLACE, JR
2601
RENEWS 6/30/26

TICOR TITLE™ 3604 25000187

Grantor

Shirley M Beisley Revocable
Living Trust dated April 11, 2012

COLUMBIA COUNTY, OREGON **2025-00892**
DEED-AGN
Cnt=1 Pgs=2 GOLSONK 02/21/2025 02:47:01 PM
\$10.00 \$11.00 \$10.00 \$60.00 \$5.00 **\$96.00**

I, Debbie Klug, County Clerk for Columbia County, Oregon, certify that the instrument identified herein was recorded in the Clerk records.

Debbie Klug - County Clerk

Grantee

Sherman R. Garver
Marsha J. Garver

After recording Return to

34094 SE Elm St
Scappoose, OR 97056

SHARED DRIVEWAY AGREEMENT

Kathleen Blackburn, Trustee of the Shirley M Beisley Revocable Living Trust dated April 11, 2012, Grantor and existing owner of 34102 SE Elm, Scappoose, OR, further described as:

Beginning at a point North 211.12 feet and East 206.73 feet from the Southwest corner of Section 7, Township 3 North, Range 1 West of the Willamette Meridian, Columbia County, Oregon; and thence South 64°13 1/2' East 170.00 feet to an iron pipe; thence North 20°06' East 525.60 feet to an iron pipe; thence North 64°13 1/2' West 16.0 feet to a point; thence South 20°06' West a distance of 211.60 feet to a point; thence North 64°13 1/2' West a distance of 154 feet to a point; thence South 20°06' West a distance of 314 feet to the point of beginning.

Grants SHERMAN R GARVER and MARSHA J GARVER Grantee and existing owner of 34094 SE Elm St, Scappoose, OR and further described as:

Beginning at a point North 211.12 feet and East 206.73 feet from the Southwest corner of Section 7, Township 3 North, Range 1 West of the Willamette Meridian, Columbia County, Oregon; thence south 64 degrees 13 1/2' East 170.00 feet to an iron pipe; thence North 20 degrees 06' East 525.60 feet to an iron pipe; thence North 64 degrees 13 1/2' West 170 feet to an iron pipe; thence South 20 degrees 06' West 525.60 feet to point of beginning; being a part of William Watts Donation Land Claim.

The right to use existing driveway for ingress and egress access to the shop. In the event 34094 SE Elm St., Scappoose, OR changes ownership, the agreement will terminate.

Grantor

Shirley M Beisley Revocable Living Trust

Dated April 11, 2012

By Kathleen Blackburn Trustee

Kathleen Blackburn, Trustee

Grantee

Sherman R Garver

Sherman R. Garver

Marsha J Garver

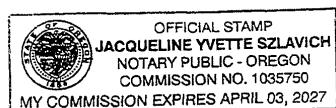
Marsha J. Garver

State of OREGON County of Columbia

This instrument was acknowledged before me on 2-21-25 by Kathleen Blackburn, Trustee of the Shirley M Beisley Revocable Living Trust dated April 11, 2012.



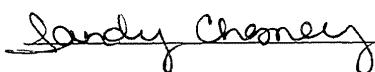
Notary Public



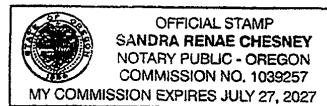
State of Oregon My Commission Expires: 4-3-27

State of OREGON County of Columbia

This instrument was acknowledged before me on 2-20-25 by Sherman R. Garver and Marsha J. Garver.


Notary Public

State of Oregon My Commission Expires: 7-27-27





SCAPPOOSE

Oregon

LAND USE ACTION REFERRAL (ANX 1-25, ZC 2-25, MiP 1-25)

November 4, 2025

RETURN TO: N.J. Johnson by November 18, 2025 via email at njohnson@scappoose.gov. If you have any questions, please call N.J. Johnson at (503) 543-7184, ext. 403.

REGARDING: Sherman & Marsha Garver and Richard & Christianne Watt have requested approval of a consolidated application for Annexation, Zone Change, and Minor Partition to allow for the annexation of the subject site to connect to municipal utilities and a 2-lot partition of Tax Lot 3000. The site is located across two tax lots addressed 34094 and 34102 SE Elm Street, south of the SE Elm Street and SE Tussing Way intersection, on properties described as Columbia County Assessor Map #3107-CC-03000 and #3107-CC-03100.

1. We have reviewed the enclosed application and have no objection to its approval as submitted.
2. Please see either our comments (below) or attached letter.
3. We are considering the proposal further and will have comments to you by _____.
4. Our board must meet to consider this; we will return their comments to you by _____.
5. Please contact our office so we may discuss this.
6. We recommend denial of the application. Please see either our comments (below) or attached letter:

COMMENTS: _____

Name: DAVE SULKAN

Title: Public Works Director

Signed: 

Date: 11/6/2025



SCAPPOOSE
Oregon

11/6/2025

To: N.J. Johnson, Assistant to the City Manager/City Planner

From: Dave Sukau, Public Works Director

Re: ANX 1-25, ZC 2-25, MiP 1-25

Dear N.J.,

I have reviewed the Land Use Action Referral packet and plans for the Annexation, Zone Change and Minor Partition.

The City of Scappoose Public Works has no objection to its approval, provided it meets all criteria set forth in the Scappoose Municipal Codes, Zoning Criteria and the SPWDS.

Sincerely,

Dave Sukau

City of Scappoose, Public Works Dept.


LAND USE ACTION REFERRAL (ANX 1-25, ZC 2-25, MiP 1-25)
November 4, 2025

RETURN TO: N.J. Johnson by November 18, 2025 via email at njohnson@scappoose.gov. If you have any questions, please call N.J. Johnson at (503) 543-7184, ext. 403.

REGARDING: Sherman & Marsha Garver and Richard & Christianne Watt have requested approval of a consolidated application for Annexation, Zone Change, and Minor Partition to allow for the annexation of the subject site to connect to municipal utilities and a 2-lot partition of Tax Lot 3000. The site is located across two tax lots addressed 34094 and 34102 SE Elm Street, south of the SE Elm Street and SE Tussing Way intersection, on properties described as Columbia County Assessor Map #3107-CC-03000 and #3107-CC-03100.

1. We have reviewed the enclosed application and have no objection to its approval as submitted.
2. Please see either our comments (below) or attached letter.
3. We are considering the proposal further and will have comments to you by _____.
4. Our board must meet to consider this; we will return their comments to you by _____.
5. Please contact our office so we may discuss this.
6. We recommend denial of the application. Please see either our comments (below) or attached letter:

COMMENTS: _____

Name: Don Vandomeier

Title: Building Official

Signed: D. Vandomeier

Date: 11-10-25



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COMMENTS: _____

Name: Branden Stachels

Title: Engineering manager

Signed: Branden Stachels

Date: 11/10/25

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COMMENTS: Columbia County Planning has no comment or concern regarding the proposal to annex.

Name: Kate J. McGuireTitle: Columbia County Planning DirectorSigned: K. McGuireDate: 11/19/2025

**RE: Referral Request: Garver & Watt Annexation & Partition**

From Grant DeJongh <Grant.DeJongh@columbiacountyor.gov>

Date Tue 12/2/2025 3:27 PM

To N.J. Johnson <njohnson@scappoose.gov>

Cc Chris Negelsoch <cnegelsoch@scappoose.gov>; Laurie Oliver Joseph <loliver@scappoose.gov>; Ryan Wallis <Ryan.Wallis@columbiacountyor.gov>

Good afternoon, N.J.,

Sorry for the delay – today is my first day back in the office, and I am playing catch-up.

Please see the comments for this referral, below:

1. The applicant will need to acquire an access permit for each parcel created through this planning action at the time of construction, per Columbia County Ordinance 2006-4. As all proposed parcels are within the City of Scappoose city limits or urban growth boundary, City of Scappoose approach standards will apply.
2. The applicant will need to improve the frontage along SE Elm Street to City of Scappoose standards, as the frontage is within City of Scappoose city limits or the urban growth boundary. Any work within the right-of-way of SE Elm Street will require a construction permit issued by Columbia County, per Columbia County Ordinance 91-6.

These comments are applicable if the portion of SE Elm Street fronting the subject site is currently a county road and would remain a county road following the annexation of the property. If this is not or would not be a county road following annexation of the property, the City of Scappoose will oversee all street improvements associated with the project and ongoing maintenance of the road. This could change the permitting agency for parcel access and frontage improvements from Columbia County to the City of Scappoose.

Please let me know if you have any questions. Thank you.

-Grant



Grant DeJongh | Assistant Director
Public Works Department | Columbia County
1054 Oregon St. | St Helens, OR 97051
503.397.5090

CITY OF SCAPPOOSE

| January 2026 | | Verify meetings, get virtual meeting details, and more meeting information can be found on our website at www.scappoose.gov . | | | | |
|--------------|-------------------------------------|--|-----------|---|-----------|-----------|
| Sunday | Monday | Tuesday | Wednesday | Thursday | Friday | Saturday |
| | | | | 1 City Offices Closed ~ Happy New Year! | 2 | 3 |
| 4 | 5 Council meeting, 7pm | 6 | 7 | 8 Planning Commission, 7pm | 9 | 10 |
| 11 | 12 | 13 | 14 | 15 EDC, Noon | 16 | 17 |
| 18 | 19 City Offices Closed | 20 Council meeting, 7pm | 21 | 22 Planning Commission, 7pm | 23 | 24 |
| 25 | 26 | 27 | 28 | 29 | 30 | 31 |