



SCAPPOOSE PLANNING COMMISSION

Council Chambers at City Hall
33568 East Columbia Avenue

Thursday, February 27th, 2020 at 7:00 p.m.

1.0 CALL TO ORDER

2.0 ROLL CALL

3.0 APPROVAL OF MINUTES

3.1 January 23, 2020 meeting minutes

4.0 CITIZEN INPUT

5.0 NEW BUSINESS

5.1 DOCKET # SDR5-19, MiV3-19

PHK Development, Inc. has requested approval of an application for Site Development Review to allow for a remodel of the existing Varsity Grill into three new tenant spaces, to include: a drive thru coffee shop, and two retail/office spaces. No additional square footage is proposed. Exterior alterations proposed include storefront replacement, additional doors/windows, drive-thru window, ATM alcove, canopy fascia, and parking lot upgrades. The requested minor variance is required to make alterations to the existing building, which is a non-conforming use due to its existing setbacks. The site is located north west of the SW Walnut Street and Columbia River Hwy intersection, on property described as Columbia County Assessor Map # 3213-BA-00300 and 3213-BA-00100.

Format: Consolidated Limited Land Use Decision and Quasi-Judicial Public Hearing;

- *Quasi-Judicial Decisions allow for both verbal and written testimony, which applies to the Minor Variance application.*
- *Limited Land Use Decisions only allow for written comments to be submitted by 5:00 p.m., Wednesday, February 26, 2020, which applies to the Site Development Review application.*

6.0 COMMUNICATIONS

6.1 Calendar Check

6.2 Commissioner Comments

6.3 Staff Comments

7.0 ADJOURNMENT

This is an open meeting and the public is welcome. The City of Scappoose does not discriminate on the basis of handicap status in its programs and activities. If special accommodations are required, please contact Susan M. Reeves, MMC, City Recorder, in advance, at 543-7146, ext. 224. TTY 1-503-378-5938

Meeting Packet items listed above can be viewed on City's website via the calendar links. www.ci.scappoose.or.us

SCAPPOOSE PLANNING COMMISSION

Council Chambers at City Hall
33568 East Columbia Avenue

Thursday, January 23rd, 2020 at 7:00 p.m.

1.0 CALL TO ORDER

Chair Freimuth called the meeting to order at 7:00 p.m.

2.0 ROLL CALL

Kevin Freimuth	Chair	Laurie Oliver	City Planner
Scott Jensen	Vice Chair	Chris Negelspace	City Engineer
Bill Blank	Commissioner	Susan M. Reeves	City Recorder
Bruce Shoemaker	Commissioner		
Tim Connell	Commissioner		
Rita Bernhard	Commissioner		
Reed Kelly	Commissioner		

EXCUSED: Elizabeth Happala CDC Office Administrator

3.0 APPROVAL OF MINUTES

3.1 September 26th, 2019 meeting

Commissioner Blank moved, and Commissioner Connell seconded the motion to approve the Planning Commission minutes from September 26, 2019, as presented.

Motion Passed 7-0. AYES: Chair Freimuth, Vice Chair Jensen, Commissioner Blank, Commissioner Shoemaker, Commissioner Connell, Commissioner Bernhard and Commissioner Kelly.

4.0 CITIZEN INPUT

There were no public comments.

5.0 NEW BUSINESS

5.1 PCC UPDATE WITH ANDREW LATTANNER

Andrew Lattanner, Director of the new PCC Training Center, stated he wanted to thank the Planning Commission for the opportunity to be here tonight. He has been in this role for about six months. He stated he is excited to say that they have broken ground and next week they will be having their official groundbreaking and he invited the Planning Commissioners to attend. He explained tonight he wanted to give the Planning Commission a little presentation on both the programming and the building and where they are and where they are going. He went over a power point.

PCC OMIC Training Center



Portland Community College

OREGON MANUFACTURING INNOVATION CENTER

To provide solutions to metals and other manufacturing challenges while preparing the industry's workforce through on-the-job learning and serving as a catalyst for local and regional economic prosperity.



Portland Community College

What Does OMIC Training Collaboration Look Like?

- Pathways to Careers in Advanced Manufacturing
- Industry-Aligned Apprenticeship, Pre-Apprenticeship & Workforce Development
- Customized Training & Master Classes
- Exposure to World-Class R&D, Advanced Technologies, and Researchers

Portland Community College

What does the skills gap in manufacturing look like?

- According to OED, there were **4,187 Oregon-based difficult-to-fill vacancies** in the manufacturing sector in 2018.
- 83% of difficult-to-fill vacancies **required some work experience**.
- Manufacturers cited **lack of applicants** and **lack of experience** as primary hiring challenges for difficult-to-fill vacancies.

Source: "Oregon's Workforce Gaps," Oregon Employment Department, May 2019.

Portland Community College

OMIC Pathways



Portland Community College

Pre-OMIC Pathways



OMIC Pre-Apprenticeship/OMIC-Aligned High School Curriculum

Portland Community College

OMIC Pathways



Portland Community College

OMIC Pathways



Portland Community College

OMIC Apprenticeship

- Combines on-the-job training with quality related instruction through PCC
- Students are sponsored and employed by an employer
- Industry-driven through a Joint Apprentice Training Committee
- State-approved (state-issued, nationally recognized journey card)
- National, third-party certifications



OMIC Apprenticeship: Benefits for Students

- Guaranteed Employment
- Rewards for Skills Gained (earn while you learn)
- Industry-Aligned Education Through PCC
- On-the-Job Training Through Employer
- National Credentials



OMIC Apprenticeship: Benefits for Employers

- Industry-Driven Curriculum
- Knowledge Retention
- Cost Savings
- Greater Employee Loyalty
- Certifications
- Pool of Skilled Talent



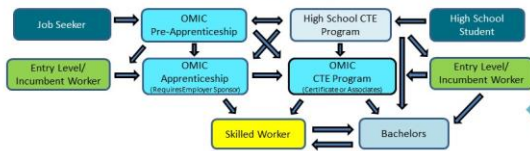
OMIC Registered Apprenticeships

The Oregon Industrial Development Joint Apprenticeship Training Committee is approved by BOLI to register Employers and Apprentices in four new programs:

- Machinist
- CNC Mill Operator
- CNC Lathe Operator
- Industrial Welder Fabricator
- Mechatronics Technician – **Currently in development**



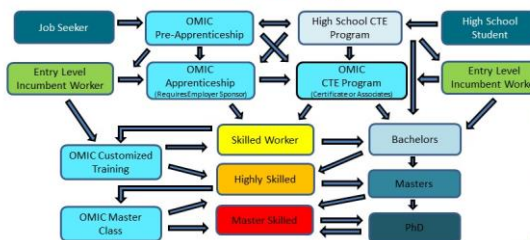
OMIC Pathways



OMIC Pathways



OMIC Pathways



PCC OMIC Training Center



Groundbreaking
Wednesday, January 29, 2020
9:00 AM – 11:00 AM
OMIC R&D



PCC OMIC Training Center

- 17.5 acres of land
- Building a 30,000 sq. ft. training facility
- Close proximity to OMIC R&D
- Easy access to Highway 30



PCC OMIC TRAINING CENTER RENDERINGS



Portland Community College

Portland Community College



Portland Community College

THANK YOU

Portland Community College

Commissioner Reed asked Andrew Lattanner what is the schedule?

Andrew Lattanner replied provided they stay on track they will have a grand open next spring.

The Planning Commission thanked Andrew.

5.2 STAFF UPDATE ON CURRENT PROJECTS

City Planner Laurie Oliver gave an overview on the inquiry meeting process and gave updates on some current projects:

Dealers Market is looking to do some site upgrades, they are getting rid of the old house and they are wanting to build an enlarged, nicer showroom.

Today they had an inquiry meeting for a proposed 8-lot subdivision just north of 6th Street and Elm Street. There are some challenges there, but they got staffs feedback and they will start working on potentially laying that out to see how it pencils out.

At the end of this month there is an inquiry meeting for an 8-unit apartment complex just north of Maple Street.

The preliminary plat for the Victoria Estate's subdivision that was approved several years ago has expired. Staff will be holding an inquiring meeting because the owner is looking to do a lot line adjustment and partition and create a couple of lots.

The next application City Planner Oliver is expecting is for a tri plex and a quad plex on adjoining lots on the east side of NE 3rd Street at Crown Zellerbach.

The next Planning Commission meeting will be held February 27 and that is for the Varsity Grill remodel.

City Engineer Chris Negelspach went over construction projects:

East Airport ~ there is a new road, new well, new sewer pump station, and part of this project is the sewer line on E Columbia Avenue. The sewer line has been tabled due to the weather.

The Crown Zellerbach extension road is mostly done. They have some landscaping, streetlights, street signs, franchise utility work, and a little more paving to do. All the heavy-duty utility work is done. The well is going to take another year or so. He has plans but they are still working on them. The sewer pump station and sewer lines are about 50% complete. He explained what they are doing now is only Phase I and II.

PCC has been doing all the onsite work. One good thing with this project is NW Natural just provided gas from Columbia Avenue to West Lane Road to Charles T Parker Way and then to Wagner Court. That portion never had gas. PCC Will have all their permits tomorrow and they are just working on site and it will take a couple of weeks to get ready to start the buildings.

OSG will have their permits this week to start all their work. They have a surcharge on there, so they won't be able to actually to start building until they move the dirt off.

The Devinaire Hangar project is just waiting for a clearance from FAA. They are doing all their

site work while they are waiting on the building. He explained the company is relocating all of their staff from Hillsboro Airport to the airport in Scappoose. The Port of Columbia County and their consultant team worked very hard to get this hybrid fire suppression approved.

Caswell Subdivision, it was previously named Urie, is actively in construction. Staff worked behind the scenes for a very long time to get the property owners on the corner to dedicate right-of-way and so the City could do frontage improvements. On the frontage that will all be new paving. The City has a plan to partner with the County on Elm Street. On 6th Street from Elm Street south they will probably do an overlay. As a part of the agreement the property owner on the corner wanted all the trees gone and new street trees will be planted.

On NW 1st Street there will be a 36-unit apartment complex. Staff has the plans and they are in review. This spring the contractor will be starting on that. You will see some half street paving with that as well.

Commissioner Bernhard asked what about the additional traffic that will be created in that area?

City Planner Laurie Oliver replied they did a traffic study and it shows that it met the v/c (volume/capacity) ratios for the intersections according to the City's requirements. Traffic is going to continue to increase around the City.

Commissioner Shoemaker stated it's the cars parked on the streets that is creating a bottleneck, in addition to no center line.

Commissioner Blank talked about a section being no parking.

DISCUSSION ON 2020 TRAINING TOPICS

City Planner Laurie Oliver explained we try to do training once a year and we are thinking of bringing in John Morgan, who is a well know planner around the State. She is wondering if there are specific trainings topics that the Planning Commission would like to have covered. She explained if you think of something later please email her.

City Engineer Chris Negelsbach stated other technical topics are on the table also, like reviewing plans or traffic or storm water.

Commissioner Bernhard replied traffic does seems to be a big issue.

Commissioner Blank talked about making major changes in housing and how we zone things, but more and more people are still looking at those issues.

City Planner Laurie Oliver talked about the Housing Implementation Plan, which was adopted by Council last July. She gave an overview of what has occurred so far. She is excited to see what comes of that.

Commissioner Bernhard asked what is the situation with the homeless here? City Planner Oliver replied she would have to defer to Chief Miller.

Commissioner Blank explained Habitat for Humanity is putting together a plan to discuss the issues on affordable housing and homelessness.

Vice Chair Jensen talked about getting training for Planning Commission on how to engage with the community.

City Planner Oliver talked about the City having a brochure on how to engage, to help the citizens understand how to testify at a public meeting.

Vice Chair Commissioner Jensen thinks it is good idea to know how to engage with the public.

Commissioner Bernhard talked the Town Meeting.

Commissioner Blank talked about an offer the City put in on some property.

City Planner Laurie Oliver talked about the City putting in an offer on the Grabhorn property, which is located off of NW EJ Smith Road, which is north of Veterans Park. She explained in conjunction with that the City would be able to create a creekside trail from the Buxton property to EJ Smith Road.

6.0 COMMUNICATIONS

6.1 Calendar Check

City Planner Oliver went over the calendar in the packet. She talked about the Boards & Commission event on January 30, 2020. She explained the February 3rd Council meeting has been canceled. She hopes the Commissioners can attend the Annual Town Meeting on February 8, 2020, at the Scappoose Middle School. The next Planning Commission meeting will be on February 27, 2020.

6.2 Commission Comments

Chair Freimuth talked about the need for workers in the trades and it is something they don't have to go to college for and he thinks it is exciting that Scappoose gets to be at the forefront of this.

6.3 Staff Comments

City Planner Oliver explained we are looking at a joint City Council/Planning Commission work session, either February 18 or sometime in March. She explained she had a planning intern working in her office for a little bit and she did some research on how other cities handle street renaming and they pulled together some examples to look at. She explained Huell, who is an intern, will do the work session. This is not a development code amendment; it would be a municipal code amendment.

City Planner Laurie Oliver explained she printed out a copy of the Community Development Centers updates for the Planning Commission to review. She explained this is something they do for Council.

Commissioner Bernhard replied that might be helpful.

Commissioner Blank explained the 12th of September will be the date of the next Sauerkraut Festival.

City Engineer Negelspach talked about the sewer treatment upgrades that will be starting soon.

7.0 ADJOURNMENT

Chair Freimuth adjourned the meeting at 8:24 p.m.

Chair Freimuth

Attest:

City Recorder Susan M. Reeves, MMC

CITY OF SCAPPOOSE STAFF REPORT

- Request:** Approval of an application for Site Development Review to allow for a remodel of the existing Varsity Grill into three new tenant spaces, to include: a drive thru coffee shop, and two retail/office spaces. The requested minor variance is required to make alterations to the existing building, which is a non-conforming use due to its existing setbacks.
- Location:** The site is located at 52001 Columbia River Hwy, north west of the SW Walnut Street and Columbia River Hwy intersection, on property described as Columbia County Assessor Map # 3213-BA-00300 and 3213-BA-00100. See attached Vicinity Map (**Exhibit 1**).
- Applicant(s):** PHK Development, Inc.
- Owner(s):** CRG Properties, LLC

EXHIBITS

1. Vicinity map [pg. 42](#)
2. Applicant's application and narrative [pg. 43](#)
3. Development Plans [pg. 55](#)
 - a) Demolition Plan, Sheet C1.0
 - b) Site and Paving Plan, Sheet C2.0
 - c) Grading and Drainage Plan, Sheet C3.0
 - d) Utility Plan, Sheet C4.0
 - e) Erosion and Sediment Control, Sheet C5.0
 - f) Landscape Plan, Sheet L1.00
 - g) Site Plan, Sheet A1.00
 - h) Exterior Elevations, Sheet A3.01
 - i) Site Lighting, Sheet E0.03
 - j) Site Photometric, Sheet E0.04
 - k) ALTA Survey
 - l) ALTA Site Plan
 - m) ALTA Easements and Dedications
 - n) ALTA Utilities
4. Transportation Impact Analysis, by Global Transportation Engineering, dated February 18, 2020 (appendix available upon request) [pg. 69](#)
5. Preliminary Stormwater Drainage Report, dated November 2019 (appendix available upon request) [pg. 85](#)
6. ODOT Region 2 comment, dated February 19, 2020 [Pg. 99](#)
7. ODOT Region 2 emails, dated February 18 and 19, 2020 [pg. 100](#)
8. Comment from Scappoose Fire District dated February 5, 2020 [pg. 102](#)

9. Comment from Public Works Director, dated January 31, 2020 [pg. 103](#)
10. Comment from Columbia River PUD, dated February 19, 2020 [pg. 104](#)
11. Public emailed comment submitted by John Rankin on behalf of Cliff Bauer, dated February 20, 2020. [pg. 105](#)
12. Applicant's emailed response to the submitted comment, dated February 20, 2020. [pg. 107](#)

SUBJECT SITE

The subject site consists of two separate legal lots. Tax Lot 100 (Columbia County Assessor Map # 3213-BA-00100) is an approximately .28 acre parcel of land that is currently occupied by Varsity Grill & Bar, a parking lot and landscaping and is situated on the corner of SW Walnut Street and Columbia River Hwy. Tax Lot 300 (Columbia County Assessor Map # 3213-BA-00300) is an approximately .27 acre parcel of land that is currently developed as a parking lot to serve Varsity Grill & Bar and is located just west of Tax Lot 100.

The subject site is bordered to the north by an existing non-conforming single-family residence (north of Tax Lot 300) and Roadrunner Gas & Grocery (north of Tax Lot 100); to the east by Columbia River Highway, and the High School beyond that; to the south by SW Walnut Street, and Subway and a small office building beyond that; and to the west by a private tract that contains Fir Lane, and mostly vacant land with some residential beyond that. The subject site is designated as Commercial (C) on the Comprehensive Plan map and is zoned Expanded Commercial (EC). Adjacent zoning is EC in all directions.

The existing building on site was originally constructed in 1967 while the property was still in Columbia County. Tax Lot 100 (which contains the existing building) was annexed into City limits the following year, in 1968. Tax Lot 300 (which contains the parking lot west of the building), as well as much of the property south west of Tax Lot 300 was annexed into City limits in 1971. The City's building department was not formed until the mid-90's and the City does not have a file for the original Site Development Review of the existing building.

OBSERVATIONS

REQUESTED APPROVAL

- The project proposes to redevelop the existing Varsity Grill & Bar into three new tenant spaces by demolishing the existing interior and building demising walls, and individual suite entrances. The existing parking lot to the west of the building is proposed to be redeveloped to include a drive-thru lane, reconfigured parking, lighting, and landscaping. Proposed exterior alterations include: demolishing the existing wall and storefront on the east elevation, and replacing with a new storefront, canopy fascia and ATM alcove. Additionally, the applicant proposes to add service doors, a drive-thru service window and fabric awnings to the west elevation, add a window to north elevation, and add a storefront door to the south elevation.
- The Scappoose Development Code (SDC) specifies that an applicant may request approval of a modification to existing development by providing a copy of the proposed

modified Site Development plan and providing a written rationale for the changes. If certain triggers are hit however, the applicant must submit a new Site Development Review application and the Planning Commission must approve the modification. In this case, the triggers which were hit were:

1. A change in the use as defined by the Uniform Building Code (17.120.070, B,4)
2. A change in land use (17.120.070, B,11)
3. A change in the type and location of access ways and parking areas where off-site traffic would be affected
4. An increase in vehicular traffic to and from the site and the increase can be expected to exceed fifty vehicles per day

TRANSPORTATION ANALYSIS

- The applicant submitted a Transportation Impact Analysis (**Exhibit 4**) prepared by Global Transportation Engineering to document the expected number of peak hour and daily trips the proposed uses would generate and to identify impacts to the transportation network.
- The current use at the subject site located at 52001 S Columbia River Highway is a drinking place¹ (ITE land use code 925).
- The proposed development is for the following land uses:
 - 2 Office/Retail spaces, together totaling 2,030 sq. ft. A Small Office Building use was assumed in the Transportation Impact Analysis.
 - Coffee/Donut Shop with Drive-Through Window totaling 2,030 sq. ft. (ITE land use code 937)
- Trip rates presented in the Institute of Transportation Engineer's (ITE) Trip Generation Manual, 10th Edition, were utilized to estimate the number of vehicle trips based on square footage of each land use. The site's trip generation is based on ITE trip generation rates for weekdays during the peak hour of adjacent street traffic. The analysis includes all trips that enter and exit the site, which includes the pass-by trips. The table below summarizes the estimated trip generation for the site.
- As summarized in the table below (Table 4 from the Transportation Impact Analysis, **Exhibit 4**), the proposed uses are expected to generate more morning peak hour trips and less afternoon peak hour trips than the current use. With pass-by trips removed from the coffee/donut shop land use, the proposed land uses have the potential to generate an additional 96 trips during the AM peak hour and one trip during the PM peak hour. The daily trip generation calculation was omitted from these net increase calculations due to small sample size for the ITE 937 land use code and lack of data for the ITE 925 land use code.

¹ A drinking place contains a bar, where alcoholic beverages and food are sold, and possibly some type of entertainment, such as music, television screens, video games, or pool tables. Varsity Grill & Bar is currently located at the project site.

Land Use	Size (sf)	Weekday						
		Daily	AMPeakHour			PMPeakHour		
			Total	Enter	Exit	Total	Enter	Exit
Existing Land Use								
Drinking Place (ITE 925)	4,170	--	--	--	--	11.36	66%	34%
Site Trips		--	--	--	--	48	32	16
Proposed Land Use								
Small Office Building (ITE 712) ¹	2,030 ³	16.19	1.92	83%	18%	2.45	32%	68%
Site Trips		33	4	3	1	5	2	3
Coffee/Donut Shop with Drive-Through Window (ITE 937)	2,030	--	88.99	51%	49%	43.38	50%	50%
Site Trips		--	181	92	89	88	44	44
Pass-By Trips (49% AM, 50% PM) ²		--	-89	-45	-44	-44	-22	-22
Net New Site Trips								
Proposed Minus Existing Land Use	--	--	185	95	90	45	14	31
Proposed Minus Existing (without Pass-By Trips)	--	--	96	50	46	1	-8	9
Notes: ¹ A Shopping Center (ITE Code 820) was also considered for potential retail options, but a Small Office Building has a higher trip generation rate during the morning peak hours. No Pass-by trip reductions were applied to represent a more conservative approach. ² Pass-by trips: Traffic already on the way from an origin to a primary trip destination that will make an intermediate stop at the site being studied without a route diversion. Pass-by trips will not add vehicles to the adjacent street network. The pass-by trips were based on the ITE Trip Generation Handbook, 10 th Edition code recommended by ODOT Region 2, "Fast Food Restaurant with Drive-Through" (ITE Code 934), which are identified as 49% (AM peak hour) and 50% (PM peak hour). ³ Includes 1,171 sq. ft and 848 sq. Ft								

- Since access from Tax Lot 100 would be directly onto an ODOT facility (Columbia River Hwy), ODOT has jurisdiction to review and comment on the Transportation Impact Analysis and intersection performance.
- As noted in the response from ODOT included as **Exhibit 6**, the US 30²/SW Walnut Street intersection will exceed the current Oregon Highway Plan (OHP) volume to capacity ratio (v/c) mobility target of 0.85 in 2021 under the AM background scenario (0.86), and after the development, operations are expected to decrease to 0.88. However, the Oregon Transportation Commission (OTC) currently has an action item on its March 2020 agenda to review and make a decision on adopting an alternative mobility target of 1.0 for signalized intersections in Scappoose. If the OTC adopts the proposed mobility target, mitigation may be determined to not be appropriate. No mitigation for the US 30/SW Walnut Street intersection is being proposed by the applicant since the intersection fails to meet the mobility targets with or without the proposed project. Additionally, ODOT has not identified any required mitigation for this project at this time.
- The 2016 Transportation System Plan proposed alternative mobility targets along Hwy 30 in Scappoose since none of the intersection operations were expected to meet the OHP v/c mobility targets during the 20-year planning horizon. Some intersections, including the Hwy

² US 30 and Columbia River Hwy are used interchangeably in this staff report.

30/High School Way intersection³, did not meet the adopted OHP v/c ratio of 0.85 even in 2012 when the traffic counts City wide were collected as part of the information used to develop the TSP.

- As noted in the email from ODOT dated February 19, 2020 (**Exhibit 7**), ODOT feels that closing the driveway entrance closest to the US 30/SW Walnut Street intersection is a good mitigation for the increased AM peak hour trips. ODOT has not suggested any mitigation is required at this time.
- Additionally, ODOT concluded that the remodel will require the applicant to submit a Change of Use permit application due to the fact that the proposal involves adding a drive-thru coffee restaurant which will increase the trips at the subject site, is close to a signal, and is in proximity to a top 5% SPIS⁴ site (Safety Priority Index System), see **Exhibit 7**. This requirement has been included in the recommended conditions of approval.

LANDSCAPING, SCREENING, AND FENCING

- New development and major modification to existing development is required to comply with SDC 17.100 – Landscaping, Screening and Fencing. Since the subject site abuts a non-conforming residential use to the north of Tax Lot 300 and west of Tax Lot 100, buffering and screening is required. The existing condition is no buffer with some landscaping and a partial fence at the north lot line of Tax Lot 300 and a partial fence at the west lot line of Tax Lot 100. Providing a full 10' buffer is not possible as it would render the parking lot unusable for the proposed uses along the north lot line and be within the building footprint along the west lot line.
- The applicant has proposed to improve the buffer area by providing a 6-foot tall continuous sight obscuring fence along the entire north lot line of Tax Lot 300 and west lot line of Tax Lot 100 where adjacent to the existing residential use. Additionally, at the north lot line of Tax Lot 300 and west lot line of Tax Lot 100, a 5' wide buffer and landscape plantings per SDC 17.100.090, D are proposed in the 5' buffer.
- Additional plantings are proposed within the parking lot islands and adjacent to SW Walnut Street, as seen on the Landscaping Plan, **Exhibit 3f**.

VEHICLE ACCESS AND OFF-STREET PARKING

- Tax Lot 100: Currently, there is a driveway entrance on the Columbia River Hwy frontage that measures ~ 50-feet in width, which exceeds the commercial driveway standards in the Public Works Design Standards (PWDS) of 36-feet. The applicant proposes to reconstruct the driveway entrance to ADA standards and to reduce the width to 36-feet. Additionally, the applicant proposes to close the driveway at the south west corner of Tax Lot 100, nearest to the intersection of Columbia River Hwy and SW Walnut Street, since the City Engineer stated that it was a safety concern. The applicant will remove the driveway entrance and replace it with a public sidewalk.

³ The TSP does not refer to the US 30/SW Walnut intersection, instead it is only referred to as the Hwy 30/High School Way intersection.

⁴ The SPIS is a systemic scoring method that identifies potential safety problems on state highways. The SPIS score is based on three years of crash data and considers crash frequency, crash rate, and crash severity. A highway segment becomes a SPIS site if a location has: 1) three or more crashes or, 2) one or more fatal crashes, over the three-year period.

- Tax Lot 300: The applicant proposes to remove the one existing driveway along SW Walnut Street and to replace it with two new driveway entrances; one 24-foot wide two way entrance to be located at the west end of the site and a 12-foot wide one way drive thru lane exit, to be located at the east end of the tax lot. See **Exhibit 3g**.
- The existing development provides 42 parking stalls, including 1 ADA accessible stall. The SDC requires 22 parking spaces to accommodate the proposed uses. The applicant has proposed to provide a total of 31 parking stalls, including 2 ADA stalls, 18 standard stalls and 13 compact stalls.

MINOR VARIANCE/NON-CONFORMING USE

- Section 17.132.030(A) of the SDC states that a proposal for the alteration of fifteen percent or less of the gross building volume of a non-conforming use or structure may be approved by the City Manager and City Planner as a minor variance⁵. Portions of the existing use/building are non-conforming for the following reasons:
 1. Width of the existing access aisle is less than 24-feet wide for two way traffic on Tax Lot 100.
 2. Width of the existing buffer adjacent to the residential use, north of Tax Lot 300 and west of Tax Lot 100.
 3. Screening of existing off-street parking on Tax Lot 100 (Adjacent to Columbia River Hwy).
 4. Setback of existing building to the south property boundary of Tax Lot 100.
 5. Lot coverage on both Tax Lots 100 and 300 exceed the maximum permittable lot coverage of 90% in the Expanded Commercial zone.
 6. Access spacing standard related to the distance of a commercial driveway to an intersection.
 7. Commercial driveway width at Columbia River Hwy frontage.
- The applicant is not expanding the non-conforming use, in fact, the existing gross building volume would decrease by 4.1% once a small portion at the south east corner of the building is removed as part of the remodel; however, the applicant is proposing an alteration to a non-conforming use, so the applicant is following the correct procedure by applying for a minor variance.
- The applicant seeks to improve the non-conformance of the use/structure where possible by: installing a landscape buffer and providing screening between the proposed uses and the abutting residential property; increasing landscaping on site to decrease the lot coverage; closing the non-conforming driveway entrance directly adjacent to the Columbia River Hwy and SW Walnut Street intersection (on SW Walnut Street frontage); and reconstructing the commercial driveway entrance on the Columbia River Hwy frontage to meet the standard width of 36-feet.

SIGNAGE

- The applicant will be required to apply for a sign permit for any new signs proposed on site.

⁵ Since the Site Development Review and Minor Variance applications are being processed through the consolidated permit process, the Planning Commission will be the approval body for the Minor Variance.

PUBLIC & PRIVATE AGENCIES AND PUBLIC NOTICE

- The City of Scappoose City Manager, Engineering, Building, Police and Public Works Departments; the Scappoose Fire District; Columbia River PUD; and ODOT have been provided an opportunity to review the proposal. Staff did not receive any objections from these agencies. The Fire Department's comment is attached as **Exhibit 8**, the Public Works Director comment is attached as **Exhibit 9**, Columbia River PUD's comment is attached as **Exhibit 10** and ODOT's comments are included as **Exhibits 6 & 7**. Those issues applicable to the Planning Commission have been included in the recommended Conditions of Approval.
- Notice of this request was mailed to property owners located within 300 feet of the subject site on February 6, 2020. Notice was also posted on the property on February 7, 2020 and published in the local newspaper on February 14, 2020.
- One emailed comment was received on February 20, 2020, which is attached as **Exhibit 11** and the applicant's response is attached as **Exhibit 12**.
 - The comment was sent by Attorney at Law, John Rankin, on behalf of his client, Cliff Bauer, Bauer Rentals LLC, and Bauer Square LLC – owner of the properties north of the subject site that contain Ichabod's, Roadrunner's Gas and Grocery, Scappoose Bagel, etc.
 - Mr. Rankin recommends approval of the application and requests that additional findings related to the ownership and use of Fir Lane be included in the staff report and that several conditions of approval be required. Findings related to Fir Lane can be found later in this report, under Section 17.154.030 – Streets.
 - The first requested condition of approval related to the requirement for a stop sign at the southbound leg of Fir Lane at SW Walnut Street was identified in the TIA as a mitigation for the sub-standard access spacing standard between Fir Lane and the new proposed driveway access at the south west portion of Tax Lot 300 and is already included as a condition of approval.
 - The second requested condition of approval will require further discussions with staff prior to a decision on whether or not to include it as a condition of approval, but would be decided prior to the Planning Commission hearing scheduled for February 27, 2020.
 - The applicant has provided an emailed response to the submitted comment, stating that "the applicant does not object to the requested signage, as approved by the City Engineer" (**Exhibit 12**).

FINDINGS OF FACT

- 1. The following sections of the Scappoose Municipal Code are applicable to this Site Development Review request:**

Chapter 15.04 Building Construction, Alteration and Repair

15.04.010 Compliance with uniform codes. *A. In addition to compliance with this chapter and other ordinances of the city, any permit or approval, building and other related activities shall comply with the current version of the following:*

- 1. American Public Works Association Standards;*
- 2. Current edition of Oregon Structural Specialty Code and Fire and Life Safety Regulations;*

3. Current edition of Oregon Mechanical Specialty Code and Mechanical Fire and Life Safety Regulations;
4. Current edition of One and Two-Family Dwelling Specialty Code;
5. Current Scappoose Fire District Ordinance, the Fire Prevention Code of the Scappoose Rural Fire District;
6. Current edition of Oregon State Plumbing Specialty Code; [...]

Finding: The applicant is required by the recommended conditions of approval to comply with the applicable uniform codes, as specified above. Section 15.04.010 is satisfied.

Chapter 17.68 EC, Expanded Commercial

17.68.030 Permitted and Conditional Uses.

[...]

Eating and drinking establishments - Permitted outright
General retail sales - Permitted outright
Professional and administrative offices – Permitted outright

Finding: The subject site is zoned Expanded Commercial and is currently in use as an eating and drinking establishment, Varsity Grill and Bar. The applicant proposes to remodel the Varsity Grill and Bar into a drive thru coffee shop and two office/retail spaces, all of which are allowed outright by the Scappoose Development Code (SDC). Section 17.68.030 is satisfied.

17.68.050 Dimensional requirements – Commercial and mixed-use buildings

<i>Dimensional requirements</i>	<i>Requirement</i>
<i>Minimum lot size</i>	<i>Ten thousand square feet</i>
<i>Minimum lot width</i>	<i>One hundred feet</i>
<i>Minimum setback</i>	
<i>Front yard</i>	<i>Shall be a minimum of ten feet and shall be landscaped per 17.100.090</i>
<i>Corner lots and through lots</i>	<i>The minimum setback for the side facing the street shall be twenty feet</i>
<i>Side or rear yard</i>	<i>None required except thirty feet shall be required where abutting a residential zoning district and the planning commission may reduce the required yard setback by fifty percent pursuant to Chapter 17.100, Landscaping, Screening and Fencing.</i>
<i>Maximum height</i>	<i>No commercial building shall exceed fifty feet in height. A mixed-use building shall not exceed sixty feet in height. Within fifty feet of a residential zone, no building shall exceed thirty-five feet in height.</i>
<i>Maximum lot coverage</i>	<i>The maximum lot coverage shall be ninety percent including all buildings and impervious surfaces</i>

Finding: Tax Lot 100 (containing the existing building) and Tax Lot 300 both exceed the required minimum of 10,000 square feet. Both tax lots also exceed the required minimum width of 100-feet, as depicted on the ALTA survey Site Plan, **Exhibit 31**.

The existing building was constructed under a previous set of standards in 1967, which was prior to the adoption of the SDC; therefore, the building does not meet the current rear or street side setbacks or lot coverage requirements, as specified above. This is considered a legal non-conforming use; however, the applicant seeks to alter the non-conforming use to allow for the drive thru coffee shop and retail/office spaces which requires that the applicant apply for a minor variance to permit the alteration. The applicant has applied for the required minor variance and findings related to the minor variance are found later in the report. Section 17.68.050 is satisfied.

Chapter 17.100 LANDSCAPING, SCREENING, AND FENCING

17.100.090 Buffering and screening requirements. A. Buffering and screening are required to reduce the impacts on adjacent uses which are of a different type. The owner of each proposed development is responsible for the installation and effective maintenance of buffering and screening. When different uses abut one another, buffering and screening are required. When different uses would be abutting one another except for separation by a right-of-way, buffering, but not screening, shall be required.

B. A buffer consists of an area within a required interior setback adjacent to a property line, having a width of ten feet, except where the planning commission requires additional width, and a length equal to the length of the property line of the abutting use or uses.

C. Occupancy of a buffer area shall be limited to utilities, screening, and landscaping. No buildings, access ways or parking areas shall be allowed in a buffer area.

D. The minimum improvements within a buffer area shall include:

1. One row of trees, or groupings of trees equivalent to one row of trees. At the time of planting, these trees shall not be less than ten feet high for deciduous trees and five feet high for evergreen trees measured from the ground to the top of the tree after planting. Spacing for trees shall be as follows:

[...] b. Medium sized trees between twenty-five feet to forty feet tall and with sixteen feet to thirty-five feet wide branching at maturity shall be spaced no greater than twenty-five feet apart;

[....]

2. In addition, at least one five-gallon shrub shall be planted for each one hundred square feet of required buffer area.

3. The remaining area shall be planted in groundcover or spread with bark mulch.

E. Where screening is required the following standards shall apply in addition to those required for buffering:

1. A hedge of narrow or broadleaf evergreen shrubs shall be planted which will form a four-foot continuous screen within two years of planting; or

2. An earthen berm planted with evergreen plant materials shall be provided which will form a continuous screen six feet in height within two years. The unplanted portion of the berm shall be planted in lawn, ground cover or bark mulched; or

3. A five-foot or taller fence or wall shall be constructed to provide a continuous sight obscuring screen. Fences and walls shall be constructed of any materials commonly used in the construction of fences and walls such as wood or brick, or otherwise acceptable by the planner. Corrugated metal is not considered to be acceptable fencing material. Chain link fences with slats may qualify as screening when combined with a planting of a continuous evergreen hedge;

4. An evergreen hedge or other dense evergreen landscaping may satisfy a requirement for a sight obscuring fence where required. Such hedge or other dense landscaping shall be properly maintained and shall be replaced with another hedge, other dense evergreen landscaping, or a fence or wall when it ceases to serve the purpose of obscuring view; and no hedge shall be grown or maintained at a height greater than that permitted by these regulations for a fence or wall when located within a vision clearance area as set forth in Chapter 12.10, Visual Clearance Areas

F. Buffering and screening provisions shall be superseded by the vision clearance requirements as set forth in Chapter 12.10, Visual Clearance Areas.

[...]

Finding: Since the subject site abuts a non-conforming residential use to the north of Tax Lot 300 and west of Tax Lot 100, buffering and screening is required. The existing building on Tax Lot 100 was constructed in 1967 and the non-conforming residential structure was built in 1966, so the house has been adjacent to this commercial use for over 53 years. The existing condition is non-conforming since no buffer exists (there is some landscaping and a partial fence, but it does not meet SDC standards) at the north lot line of Tax Lot 300 and at the west lot line of Tax Lot 100. Providing a full 10' buffer is not possible as it would render the parking lot unusable for the proposed uses along the north lot line of Tax Lot 300 and be within the building footprint along the west lot line of Tax Lot 100.

The applicant seeks to improve the non-conforming situation by providing a 6-foot tall continuous sight obscuring fence along the entire north lot line of Tax Lot 300 and west lot line of Tax Lot 100 where adjacent to the existing residential use. Additionally, at the north lot line of Tax Lot 300 and west lot line of Tax Lot 100, a 5' wide buffer and landscape plantings per SDC 17.100.090(D) are proposed. As seen on the Landscaping Plan (**Exhibit 3f**), the applicant proposes a mix of evergreen shrubs, Arnold Tulip Poplar trees and ground cover within the buffer area. As depicted on the Utility Plan (**Exhibit 3d**), there is a sanitary sewer easement located in the northwest corner of Tax Lot 100 which coincides with the required landscape buffer adjacent to Tax Lot 300. Within the sanitary sewer easement shrubs and small plantings are allowed; however, the applicant will need to ensure that no shrubs or trees normally expected to exceed 4-feet in height at maturity are planted, per 3.0024 – Sanitary Easements, of the PWDS. Section 17.100.090 is satisfied.

17.100.100 Screening--Special provisions.

A. If four or more off-street parking spaces are required under this title, off-street parking adjacent to a public street shall provide a minimum of four square feet of landscaping for each lineal foot of street frontage. Such landscaping shall consist of landscaped berms or shrubbery at least two feet in height, which shall be dispersed adjacent to the street as much as practical. Additionally, one tree which shall provide a canopy of at least three hundred square feet upon

maturity shall be provided for each fifty lineal feet of street frontage or fraction thereof. Landscaped parking areas may include special design features which effectively screen the parking lot areas from view. These design features may include the use of landscaped berms, decorative walls, and raised planters. Landscape planters may be used to define or screen the appearance of off-street parking areas from the public right-of-way. Materials to be installed shall achieve a balance between low lying and vertical shrubbery and trees,

B. Loading areas and outside storage shall be screened from public view from public streets and adjacent properties by means of sight obscuring landscaping, fences, walls or other means. The screen shall have a minimum height of six feet and the planning commission may require a taller screen depending on the location and height of the loading or storage area.

C. Except for one-family and two-family dwellings, any refuse container or disposal area and service facilities such as gas meters and air conditioners which would otherwise be visible from a public street, customer or resident parking area, any public facility or any residential area, shall be screened from view by placement of a solid wood fence, masonry wall or evergreen hedge between five and eight feet in height. All refuse materials shall be contained within the screened area.

Finding: Since four or more off-street parking spaces are required for the proposed uses, the applicant has provided a minimum of four square feet of landscaping for each lineal foot of street frontage along SW Walnut Street, as depicted on the Landscaping Plan (**Exhibit 3f**).

The refuse containers will be screened from view to the north and west by fencing along the property lines adjacent the trash enclosure, to the south by the building itself, and to the east by a metal gate that will give access to the trash enclosure. The applicant proposes to install a 6-foot high sight obscuring black vinyl coated fence with dark grey privacy slats around the refuse containers and a metal gate on the east elevation, see **Exhibits 3f and 3h**. Additionally, the applicant proposes to install evergreen plantings adjacent to the fencing with slats, in conformance with the development code requirements above.

The Public Works Director submitted a comment in regard to the trash enclosure being within the sanitary sewer easement located on Tax Lot 100, just north of the existing building (**Exhibit 9**). Previously, the applicant had proposed a block wall to enclose the garbage area which, while not a “structure” per se, raised concerns since the City would not want a block wall that would be difficult to remove for any future repairs that might be needed to the public sewer line in that location. The applicant subsequently, after receiving the Public Works Directors comment, modified the enclosure to be constructed of black powder coated chain link with dark grey privacy slats surrounded by evergreen plantings which has alleviated the Public Works Directors concerns since fencing and plantings not expected to exceed 4 feet in height at maturity are permitted within sanitary sewer easements, per the PWDS. Sections 17.100.100(B &C) are satisfied.

Chapter 17.104 STREET TREES

17.104.020 Applicability.

[...]

B. All development shall be required to plant street trees. Street trees shall be defined as trees located on land lying between the property lines on either side of all streets, avenues or public rights-of-way within the city or within easements defined on a recorded plat as street tree easements.

[...]

17.104.040 Standards for street trees.

A. Street trees shall be selected from the approved street tree list on file with the Planning Department.

B. At the time of planting, street trees shall not be less than ten feet high for deciduous trees and five feet high for evergreen trees.

C. Spacing and minimum planting areas for street trees shall be as follows:

- 1. Street trees under twenty-five feet tall and less than sixteen feet wide at maturity shall be spaced no further than fifteen feet apart in planting areas containing no less than sixteen square feet of porous surface and not less than four feet wide;*
- 2. Street trees under twenty-five feet tall and greater than sixteen feet wide at maturity shall be spaced no further than twenty feet apart in planting areas containing no less than sixteen square feet of porous surface and not less than four feet wide;*
- 3. Street trees between twenty-five feet to forty feet tall and less than twenty-five feet wide at maturity shall be spaced no greater than twenty-five feet apart in planting areas containing no less than twenty-four square feet of porous surface and not less than six feet wide;*
- 4. Street trees between twenty-five feet to forty feet tall and greater than twenty-five feet wide at maturity shall be spaced no greater than thirty feet apart in planting areas containing no less than twenty-four square feet of porous surface and not less than six feet wide;*
- 5. Street trees greater than forty feet tall at maturity shall be spaced no greater than forty feet apart in planting areas containing not less than thirty-six square feet of porous surface and not less than eight feet wide.*

D. Street trees located under or within ten feet of overhead utility lines shall be less than twenty-five feet tall at maturity.

E. Street trees shall be planted in accordance with the requirements of Scappoose Municipal Code Section 13.28.020(C).

Finding: The current 50-foot width of SW Walnut Street does not permit street trees to be planted within the right of way, where they would normally be planted. As depicted on the Landscaping Plan (**Exhibit 3f**), the applicant proposes to plant Trident Maple street trees 20-feet apart in a minimum 16-square foot porous surface on Tax Lot 300, adjacent to SW Walnut Street. Trident Maple trees are listed on the approved street tree list, which states that they are to be spaced a maximum of 20-feet apart. Since the western lot line of Tax Lot 300 borders a private tract that is used as Fir Lane, rather than public right of way, no street trees are required along that frontage. The recommended conditions of approval require the applicant to record a

Street Tree Easement over the planter strip containing the street trees on Tax Lot 300. Section 17.104.040 is satisfied.

Chapter 17.106 OFF-STREET PARKING AND LOADING REQUIREMENTS

17.106.020 General provisions.

A. The dimensions for parking spaces are subject to the requirements in Section 17.106.050, and as follows:

1. Nine feet wide and eighteen feet long for a standard space;
2. Eight and one-half feet wide and fifteen feet long for a compact space; and
3. In accordance with the applicable state and federal standards, at least twelve feet wide and eighteen feet long for designated handicapped parking spaces.

[...]

H. Location of Required Parking.

[...]

2. Off-street parking spaces for uses other than single-family or duplex residential shall be located not further than four hundred feet from the building or use they are required to serve, measured in a straight line.

I. Where several uses occupy a single structure or parcel of land or a combination of uses are included in one business, the total off-street parking spaces and loading area is the sum of the requirements of the several uses, computed separately.

[...]

N. All parking areas which contain over five required spaces shall be provided with one handicapped parking space. All parking provisions required by the ADA shall be met.

[...]

P. Bicycle Parking.

1. Standards. At a minimum, bicycle parking shall be provided based on the standards in Subsection 5 below. [...]
2. Design. Bicycle parking shall consist of staple-design steel racks or other City-approved racks, lockers, or storage lids providing a safe and secure means of storing a bicycle.
3. [...]
4. Prohibitions. Bicycle parking shall not impede or create a hazard to pedestrians or vehicles and shall be located so as to not conflict with the visual clearance areas as provided in Chapter 12.10. Bicycle parking areas shall not be located within parking aisles, landscape areas, or pedestrian ways.
5. Number of spaces. The bicycle parking standards below shall apply to the uses listed. Bicycle parking spaces shall be installed in conjunction with the installation of required new or additional vehicle parking. When two standards are provided, the standard that results in the greater number of bicycle parking spaces shall govern.

Commercial: 2 spaces per primary use or 1 per 5 vehicles spaces

Q. Any lights provided to illuminate any public or private parking area or vehicle sales area shall be so arranged as to reflect the light away from any abutting or adjacent residential district.

[...]

V. Fractional space requirements shall be counted as a whole space.

[...]

X. Off street parking of any vehicle or recreational vehicle, watercraft, or parts designed to be affixed thereto, which obstructs the visual clearance area or creates a potential safety hazard shall not be allowed in required yard.

Y. Parking and loading areas shall be designed to minimize disturbances of adjacent residents by erection between the uses of a sight-obscuring fence of not less than four feet in height, provided that the provisions for visual clearance areas are met. Parking spaces within a parking lot shall be designed and constructed so that no portion of a parked vehicle, including an opened door, will extend beyond the property line.

Finding: The existing development provides 42 parking stalls, including 1 ADA accessible stall. The SDC requires 22 parking spaces to accommodate the new proposed uses. The applicant has proposed to provide a total of 31 parking stalls, including 2 ADA stalls, 18 standard stalls and 13 compact stalls, meeting the requirements above.

The existing development does not provide any bicycle parking currently. The recommended conditions of approval require the applicant to provide 5 bicycle parking spaces since there are 22 required vehicle parking spaces and the code specifies that 1 bicycle parking space shall be required per 5 vehicle spaces (fractional space requirements are counted as a whole space).

The applicant proposes parking lot lighting and wall packs on the building, which will need to incorporate baffles to reflect the light away from abutting residential uses. The Photometric Plan included as **Exhibit 3j** shows that light may be reflected towards the abutting residential use. The recommended conditions of approval require the applicant to revise the lighting plan to ensure that light is reflected away from the abutting residential use.

The applicant proposes to maintain visual clearance areas clear of any vehicle, as required by the code. **Exhibit 3g** illustrates the visual clearance area; no parking is proposed in this area.

As described in the narrative (**Exhibit 2**), the applicant proposes a 6-foot high sight obscuring fence made of black powder coated chain link with dark grey slats and evergreen shrubs along the abutting residential property to the north of Tax Lot 300 and west of Tax Lot 100. Parking spaces have been designed so that no portion of a parked vehicle, including an open door, will extend beyond a property line. Section 17.106.020 is satisfied.

17.106.030 Minimum off-street parking requirements.

[...]

C. Commercial Uses.

1. Administrative and professional services: 1 space for each 400 square feet of gross floor area

[...]

11. Eating and drinking establishments: 1 space per 120 square feet of gross floor area

[...]

14. General retail sales: 1 space for each 400 square feet of gross floor area

Finding: The SDC requires 22 parking spaces to accommodate the new proposed uses since 2,030 square feet is proposed for an eating and drinking establishment and 2,030 square feet is proposed for either an office or retail use (which both have the same parking requirements). The applicant has proposed to provide a total of 31 parking stalls, including 2 ADA stalls, 18 standard stalls and 13 compact stalls, meeting the requirements above. Section 17.106.030 is satisfied.

17.106.050 Parking dimension standards.

B. Minimum standards for a standard parking stall's length and width, aisle width, and maneuvering space shall be determined from the following table. Dimensions for designated compact spaces are noted in parenthesis:

Overall				
Angle from curb	Stall Width "A"	Channel Depth "B"	Aisle Width* "C"	Curb Length per stall "D"
Parallel	9'0" (8'6")	9'0" (8'6")	12'0" (12'0")	23'0" (20'0")
30°	9'0" (8'6")	16'10" (14'10")	12'0" (12'0")	18'0" (17'0")
45°	9'0" (8'6")	19'1" (16'7")	14'0" (14'0")	12'9" (12'0")
60°	9'0" (8'6")	20'1" (17'3")	18'0" (18'0")	10'5" (10'3")
90°	9'0" (8'6")	18'0" (15'0")	24'0" (24'0")	9'0" (8'6")
* Aisles accommodating two direction traffic or allowing access from both ends shall be 24 feet in width.				

[...]

C. *Excluding single-family and duplex residences, groups of more than two parking spaces shall be served by a service drive so that no backing movements or other maneuvering within a street or other public right-of-way would be required. Service drives shall be designed and constructed to facilitate the flow of traffic, provide maximum safety of traffic access and egress, and maximum safety of pedestrians and vehicular traffic on the site.*

D. *Each parking or loading space shall be accessible from a street and the access shall be of a width and location as described in the public works design standards.*

E. *Except for single-family and two-family residences, any area intended to be used to meet the off-street parking requirements as contained in this chapter shall have all parking spaces clearly marked using a permanent paint. All interior drives and access aisles shall be clearly marked and signed to show direction of flow and maintain vehicular and pedestrian safety.*

[...]

G. *Access Drives.*

1. *Access drives from the street to off-street parking or loading areas shall be designed and constructed to facilitate the flow of traffic and provide maximum safety for pedestrian and vehicular traffic on the site.*

2. *The number and size of access drives shall be in accordance with the requirements of public works design standards. [...]*

H. *Parking spaces along the boundaries of a parking lot or adjacent to interior landscaped areas or sidewalks shall be provided with a wheel stop at least four inches high located three feet back from the front of the parking stall. The front three feet of the parking stall may be concrete, asphalt or low lying landscape material that does not exceed the height of the wheel stop. This area cannot be calculated to meet landscaping or sidewalk requirements.*

I. *Except for single-family and two-family residences, off-street parking and loading facilities shall be drained to avoid flow of water across public sidewalks in accordance with specifications approved by the public works director to ensure that ponding does not occur.*

J. *Artificial lighting on all off-street parking facilities shall be designed to deflect all light away from surrounding residences and so as not to create a hazard to the public use road or street and shall not exceed intensities for adjacent streets as included in public works design standards.*

[...]

Finding: The proposed parking lot design (**Exhibit 3g**) complies with the dimensional requirements for parking spaces as noted above. The standard parking spaces are 9-feet x 18-feet and the compact stalls are 8.5-feet x 15-feet.

The drive aisles on Tax Lot 300 meet the minimum 24-foot width for two-way traffic; however, the existing drive aisle on Tax Lot 100 (with the existing building) is non-conforming with a width that varies from 20'-9 1/4" at the narrowest to 23'-5 7/8" at the widest, according to the narrative and site plan (**Exhibits 2 & 3g**). The applicant has proposed compact parking stalls in the vicinity of the narrowest point of the drive aisle to alleviate some of the concern, which reduces the length of the parking stall from 18-feet down to 15-feet. The applicant proposes to mark all parking spaces with a 4-inch wide painted parking stripe. The site plan demonstrates that no backing movements would occur on public streets.

The applicant has illustrated wheel stops meeting the requirements above. As shown on the Utility Plan and discussed within the Preliminary Stormwater Drainage Report (**Exhibits 3d & 5**), stormwater will flow into two existing catch basins, then be routed into an existing drywell within the parking lot on Tax Lot 300.

Parking lot lighting is depicted on the Site Lighting Plan as well as on the Photometric Plan for this development (**Exhibits 3i & 3j**). The recommended conditions of approval require that the applicant ensure that lighting levels and shields will limit light spillover onto adjoining roadways and abutting residential properties. Section 17.106.050 is satisfied.

L. Pedestrian walkway

Where a walkway crosses a parking area or driveway, it shall be clearly marked with contrasting paving materials (e.g., pavers, light-color concrete inlay between asphalt, or similar contrast). The crossing may be part of a speed table to improve driver visibility of pedestrians. If crossings involve grade changes, the crossing shall include ADA accessible ramps. Painted striping, thermo-plastic striping, and similar types of non-permanent applications are discouraged, but may be approved for lower-volume crossings of 24 feet or less.

Finding: As shown on the Site Plan (**Exhibit 3g**), the applicant proposes to provide a raised speed table where the walkway crosses through the drive-thru lane to access the buildings west entrances to improve driver visibility of pedestrians. As stated in the narrative, the crosswalk itself and the sloped portions of the speed table are concrete, contrasting from the adjacent asphalt pavement. Section 17.106.050, L is satisfied.

Chapter 17.132 NON-CONFORMING USES

17.132.030 Alteration of non-conforming use or structure. *As used in this section, “alteration” of a nonconforming use or structure including a change in use of structure of no greater adverse impact to the neighborhood.*

A. Minor Alteration. A proposal for the alteration of fifteen percent or less of the gross building volume of a nonconforming use or structure may be approved by the city manager and the planner as a minor variance.

Finding: The applicant proposes an alteration to a non-conforming use/structure, of no greater adverse impact to the neighborhood. The proposed alteration will decrease the building volume by 4.1%, which does not exceed the threshold of 15%, and requires that the applicant apply for a minor variance. Since the applicant has requested a consolidated approval of the Site Development Review and the Minor Variance applications, the Planning Commission is the approval authority. Section 17.132.030 is satisfied.

Chapter 17.134 VARIANCE

17.134.030 Criteria for granting a variance. *The planning commission or planner shall approve, approve with conditions, or deny an application for a variance based on finding that the following criteria are satisfied. Minor variances shall satisfy criteria (A) through (C) and major variances shall satisfy criteria (A) through (E):*

A. The proposed variance will not be materially detrimental to the purposes of this title, be in conflict with the policies of the comprehensive plan, to any other applicable policies and standards, and to other properties in the same zoning district or vicinity;

B. The use proposed will be the same as permitted under this title and city standards will be maintained to the greatest extent that is reasonably possible while permitting some economic use of the land;

C. Existing physical and natural systems, such as but not limited to traffic, drainage, dramatic land forms, or parks will not be adversely affected any more than would occur if the development were located as specified in the title;

[...]

Finding: The proposed variance is to allow for the alteration of an existing non-conforming use, which will not be materially detrimental to the purposes of Title 17, be in conflict with the policies of the Comprehensive Plan, to any other applicable policies and standards, or to other properties in the same zoning district. The uses proposed are outright permitted within the Expanded Commercial zone.

The existing building/use was constructed in 1967 under a previous set of standards and the application seeks to improve the non-conforming situation by installing a landscape buffer and providing screening between the proposed uses and the abutting residential property; increasing landscaping on site to decrease the lot coverage; closing the non-conforming driveway entrance directly adjacent to the Columbia River Hwy and SW Walnut Street intersection (on SW Walnut Street frontage); and reconstructing the commercial driveway entrance on the Columbia River Hwy frontage to meet the standard width of 36-feet.

Existing physical and natural systems will not be adversely affected any more than would occur if the development were as specified in the development code. Section 17.134.030 is satisfied.

Chapter 17.120 SITE DEVELOPMENT REVIEW

17.120.180 Approval standards. *The planning commission shall make a finding with respect to each of the following criteria when approving, approving with conditions, or denying an application:*

A. Provisions of all applicable chapters;

B. Buildings shall be located to preserve topography, and natural drainage; located in areas not subject to ground slumping or sliding; located to provide adequate distance between adjoining buildings for adequate light, air circulation, and firefighting; and oriented with consideration for sun and wind; and

C. Existing trees having a six-inch caliper or greater shall be preserved or replaced by new plantings of equal character;

[...]

Finding: The applicable chapters of the Development Code are discussed elsewhere in this report. The building was originally constructed in 1967, and the applicant is not proposing to alter the footprint of the building, therefore, 17.120.080, B is not applicable. No existing trees are proposed to be removed. Sections 17.120.180(A-C) are satisfied.

H. Demarcation of public, semipublic, and private spaces; crime prevention:

a. The structures and site improvements shall be designed so that public areas such as streets or public gathering places, semipublic areas and private outdoor areas are clearly defined in order to establish persons having a right to be in the space, in order to provide for crime prevention and to establish maintenance responsibility; and

b. These areas may be defined by a deck, patio, low wall, hedge or draping vine, a trellis or arbor, a change in level or landscaping;

Finding: The site implements landscape strips where possible to demarcate private property or semi-public space from the right of way or public portions of the right of way. Private use areas such as the trash enclosure are proposed to be fenced from view or access. A continuous fence at the adjacent residential use clearly demarcates the lot line and establishes responsibility for the landscape and paving areas on the property side of the fence.

The patio proposed for the right of way is subject to City Council approval, as specified in Chapter 12.12.040 of the Scappoose Municipal Code. The applicant will be required to formally request approval of a right of way obstruction permit under a separate process. Sections 17.120.180(H, a and b) are satisfied.

I. Crime prevention and safety:

*1. Windows shall be located so that areas vulnerable to crime can be surveyed by the occupants,
[...]*

Finding: As stated in the narrative, the existing building's windows are proposed to remain. As depicted on the Exterior Elevation Plan (**Exhibit 3h**), additional windows are proposed along the east elevation in the two south bays, one additional window on the north elevation and one service window on the west elevation. These north windows will support the surveillance of the trash area. Extensive windows along the east and south will support the surveillance of the patio and east parking areas. The addition of the drive-thru service window on the west elevation will support the surveillance of the west parking area. Sections 17.120.180(I, 1) is satisfied.

4. The exterior lighting levels shall be selected and the angles shall be oriented towards areas vulnerable to crime, and

5. Light fixtures shall be provided in areas having heavy pedestrian or vehicular traffic and in potentially dangerous areas such as parking lots, stairs, ramps and abrupt grade changes. Fixtures shall be placed at a height so that light patterns overlap at a height of seven feet which is sufficient to illuminate a person;

Finding: The applicant has provided a Lighting Plan and Photometric Plan, included as **Exhibits 3i & 3j**, which show that the parking lots and trash enclosure area will be adequately lit to ensure safety. Sections 17.120.180(I, 4 and 5) are satisfied.

J. Access and circulation:

1. The number of allowed access points for a development shall be as provided in the public works design standards.

2. *All circulation patterns within a development shall be designed to accommodate emergency vehicles.*
3. *Provisions shall be made for pedestrian ways and bicycle ways consistent with 17.120.180(Q);*

Finding: The applicant proposes to close one of the access points along the SW Walnut Street frontage on the south east corner of Tax Lot 100 since it is not in conformance with the access spacing standards for the distance between an intersection and driveway, according to the provisions contained in the PWDS and TSP (Transportation System Plan).

On Tax Lot 300, the applicant proposes to close the existing single mid-block access point and install two new driveways, to include; a 24-foot wide two-way driveway access at the south west corner of the tax lot and a 12-foot wide one-way drive thru exit lane at the south east corner of the tax lot, both along the SW Walnut Street frontage.

The Fire Department has adequate space to respond to an emergency from the streets fronting the building and does not anticipate the need to accommodate emergency vehicles within the circulation patterns on site. The Fire Department has provided a comment regarding addressing of the building in order to meet the current Fire Ordinance regulations (see **Exhibit 8**). Findings related to 17.120.180(Q) are found later in this report. Section 17.120.080(J) is satisfied.

- L. All parking and loading areas shall be designed in accordance with the requirements set forth in Sections 17.106.050 and 17.106.080, Chapter 12.10, and the public works design standards;*
- M. All landscaping shall be designed in accordance with the requirements set forth in Chapter 17.100;*

Finding: Findings related to these criteria are found in the applicable sections of this report. Sections 17.120.080(L, M) are satisfied.

- O. All facilities for the handicapped shall be designed in accordance with the requirements set forth in the ADA requirements; and*

Finding: The existing parking area on Tax Lot 100 will be restriped to accommodate ADA and OSSC (Oregon Structural Specialty Code) requirements for accessible parking. Specifically, one space will be added to accommodate the full combined site's parking count and both spaces will be increased to 9-feet wide with a van accessible 9-foot wide access aisle provided. The applicant proposes to remove and regrade the existing paving at these spaces to meet ADA and OSSC slope and cross slope requirements and to provide the proper signage. Additionally, the applicant proposes to provide new storefront entrances along the east elevation, which would be required to meet ADA and OSSC accessibility requirements, and would be verified during construction document review. Section 17.120.080(O) is satisfied.

- P. All of the provisions and regulations of the underlying zone shall apply.*

Finding: Findings related to the provision and regulations of the Expanded Commercial zone were discussed previously in this report. Section 17.120.080(P) is satisfied.

Q. Pedestrian Access and Circulation Standards. Developments shall conform to all of the following standards for pedestrian access and circulation:

1. Continuous Walkway System. A pedestrian walkway system shall extend throughout the development site and connect to adjacent sidewalks, if any, and to all future phases of the development, as applicable.

2. Safe, Direct, and Convenient. Walkways within developments shall provide safe, reasonably direct, and convenient connections between primary building entrances and all adjacent parking areas, transit stops, recreational areas/playgrounds, and public rights-of-way based on all of the following criteria:

a. The walkway is reasonably direct. A walkway is reasonably direct when it follows a route that does not deviate unnecessarily from a straight line or it does not involve a significant amount of out-of-direction travel;

b. The walkway is designed primarily for pedestrian safety and convenience, meaning it is reasonably free from hazards and provides a reasonably smooth and consistent surface and direct route of travel between destinations. The city planning commission may require landscape buffering between walkways and adjacent parking lots or driveways to mitigate safety concerns.

c. The walkway network connects to all primary building entrances and, where required, Americans With Disabilities Act requirements.

3. Vehicle/Walkway Separation. Except as required for crosswalks, pursuant to Subsection 4, below, where a walkway abuts a driveway or street it shall be raised 6 inches and curbed along the edge of the driveway/street. Alternatively, the city planning commission may approve a walkway abutting a driveway at the same grade as the driveway if the walkway is physically separated from all vehicle-maneuvering areas. An example of such separation is a row of bollards (designed for use in parking areas) with adequate minimum spacing between them to prevent vehicles from entering the walkway. [...]

5. Walkway Width and Surface. Walkways, including access ways required for subdivisions pursuant with Chapter 17.150, shall be constructed of concrete, asphalt, brick/masonry pavers, or other durable surface, as approved by the city engineer, and not less than 5 feet wide. Multi-use paths (i.e., designed for shared use by bicyclists and pedestrians) shall be concrete or asphalt and shall conform to the public works design standards.

6. Walkway Construction. Walkway surfaces may be concrete, asphalt, brick/masonry pavers, or other city-approved durable surface meeting Americans with Disabilities Act requirements. Walkways shall be not less than 5 feet in width, except that concrete walkways a minimum of 6 feet in width are required in commercial developments and where access ways are required for subdivisions under Chapter 17.150 the planning commission may also require 6-foot-wide, or wider, concrete sidewalks in other developments where pedestrian traffic warrants walkways wider than 5 feet. [...]

Finding: As seen on the Site Plan (**Exhibit 3g**), each parking lot on the subject site is connected to the buildings nearest entries by a pedestrian walkway system. The walkway between the east parking lot (on Tax Lot 100) and the building is proposed to be extended through the project site to the public sidewalk along SW Walnut St. All new walkways are direct with no out-of-

direction travel and are proposed to be smoothly graded and ADA accessible with direct routes of travel from parking areas to building entries. Where possible, at the west parking area, portions of the walkways have landscape buffering.

Both primary walkways in the project connect their respective parking lots with the nearest building entrances. ADA parking is proposed to be regraded to meet current code requirements and connected to the building's primary ADA entries on the east elevation. The west walkways are also proposed to be accessible as are the entries along the west elevation; however, they are not connected directly to the ADA parking which is concentrated, appropriately, in the east parking lot.

The proposed walkways west of the building are all raised and curbed. The existing walkway east of the building between the building and east parking lot is generally flush with the adjacent asphalt. Wheel stops are proposed at each parking stall to separate the walkway from vehicles. Raising the walkway here is not feasible in order to maintain accessible primary building entries at each tenant space.

The only proposed crosswalk in the project, from the west parking lot to the building's west entrances, crosses the one-way drive thru lane. The crosswalk is part of a speed table to improve driver visibility. As noted on the Site Plan (**Exhibit 3g**), the crosswalk itself and the sloped portions of the speed table are concrete, contrasting from the adjacent asphalt pavement.

All walkways proposed in the project are concrete. The new walkway proposed between the west parking and west elevation building entries is 6 feet wide. The existing concrete walkway between the building's main entry and the east parking lot is approximately 5'-10" wide and the new portion of sidewalk proposed adjacent matches this width. The walkway between the building and the west property line of Tax Lot 100 ranges from 3'-8" to 4'-6" which is the maximum possible width given the buildings existing setback to the property line. Section 17.120.080(Q) is satisfied.

Chapter 17.154 STREET AND UTILITY IMPROVEMENT STANDARDS

17.154.030 Streets. *A. No development shall occur unless the development has frontage or approved access to a public street;*

1. Streets within a development and streets adjacent to a development shall be improved in accordance with this title and the public works design standards and specifications.

2. Any new street or additional street width planned as a portion of an approved street plan shall be dedicated and improved in accordance with this title and the public works design standards and specifications.

[...]

Finding: The subject site has frontage on Columbia River Hwy and SW Walnut Street. Additionally, the western boundary of Tax Lot 300 abuts "Fir Lane" (the portion that runs north-south), which is neither a public or private road, but is a privately owned tract of land which has been paved and connects to the portion of Fir Lane which runs east-west, which has been dedicated as right of way. The north-south portion of "Fir Lane" has been in public use for many

years by the existing residences and businesses to the west and north of the subject site to access SW Walnut Street and Columbia River Hwy. Staff have reviewed deed documents related to this section of “Fir Lane” and have not found evidence of any recorded maintenance agreements or recorded legal rights of access. There has been no judicial proceeding to determine the rights of property owners, or the general public, regarding use of the north-south portion of “Fir Lane”. The proposed development is not expected to generate trips that would utilize this section of “Fir Lane” since the subject site has convenient access to Columbia River Hwy and SW Walnut Street.

No dedication of right of way is proposed in conjunction with this application. While Tax Lot 300 is adjacent to the privately owned tract that contains “Fir Lane”, the applicant has not been required to dedicate right of way since “Fir Lane” is not a public roadway and the TSP does not show a public road connecting north of SW Walnut Street in this location.

The applicant proposes to close the existing driveway located at the southeast corner of Tax Lot 100, which is adjacent to the Columbia River Hwy and SW Walnut Street intersection, and does not meet the TSP access spacing standards. The applicant also proposes to reduce the width of the driveway on the Columbia River Hwy frontage to 36-feet, meeting the PWDS. The sidewalk section in this location is proposed to be regraded to meet ADA standards.

For the west parking lot (Tax Lot 300), the project proposes closing the existing two-way drive aisle at mid-block and adding a new 24-foot wide two-way drive aisle at the southwest portion of the site and adding a 12-foot one-way exit drive aisle from the drive-thru lane at the south east portion of the site. Lastly, the applicant proposes adding a concrete patio in the right-of-way south of the building between the building and the sidewalk. The applicant must formally request a permit to occupy the right of way, per SMC 12.12.040 (Scappoose Municipal Code). City Council is the approval body for this request. Section 17.154.030, A, 1-2 is satisfied.

H. Concrete vertical curbs, curb cuts, wheelchair, bicycle ramps and driveway approaches shall be constructed in accordance with standards specified in this chapter and the city’s public works design standards. Concrete curbs and driveway approaches are required and shall be built to the city’s configuration standards.

[...]

Finding: As depicted on the Site Plan (**Exhibit 3g**), a 36-foot wide concrete driveway approach is proposed along Columbia River Hwy. Additionally, 12-foot wide drive-thru exit lane is proposed on Tax Lot 300 at the south east portion of the site and a 24-foot wide two-way driveway approach is proposed at the south west end of the site. Review during the permitting phase of the project will ensure that these items are constructed to City standards. Section 17.154.030, H is satisfied.

[...]

S. A Transportation Impact Study (TIS) must be submitted with a land use application if the conditions in (1) or (2) apply in order to determine whether conditions are needed to protect and minimize impacts to transportation facilities, consistent with Section 660-012-0045(2)(b) and (e) of the State Transportation Planning Rule.

1. *Applicability – TIS letter.* A TIS letter shall be required to be submitted with a land use application to document the expected vehicle trip generation of the proposal. The expected number of trips shall be documented in both total peak hour trips and total daily trips. Trip generation shall be estimated for the proposed project using the latest edition of the Institute of Engineers Trip Generation Manual or, when verified with the City prior to use, trip generation surveys conducted at similar facilities.

2. *Applicability – TIS report.* A TIS report shall be required to be submitted with a land use application if the proposal is expected to involve one or more of the following:

a. *The proposed development would generate more than 10 peak hour trips or more than 100 daily trips.*

b. *The proposal is immediately adjacent to an intersection that is functioning at a poor level of service, as determined by the city engineer.*

c. *A new direct approach to US 30 is proposed.*

d. *A proposed development or land use action that the road authority states may contribute to operational or safety concerns on its facility(ies).*

e. *An amendment to the Scappoose Comprehensive Plan or Zoning Map is proposed.*

3. *Consistent with the city's Traffic Impact Study (TIS) Guidelines, the city engineer will determine the project study area, intersections for analysis, scenarios to be evaluated and any other pertinent information concerning the study and what must be addressed in either a TIS letter or a TIS report.*

Finding: The proposed uses are expected to generate more morning peak hour trips and less afternoon peak hour trips than the current use. With pass-by trips removed from the coffee/donut shop land use, the proposed land uses have the potential to generate an additional 96 trips during the AM peak hour and one trip during the PM peak hour. As required, the applicant has submitted a full TIA, Transportation Impact Analysis (**Exhibit 4**), prepared by Global Transportation Engineering. Section 17.154.030, S. 1 -2 is satisfied.

4. *Approval Criteria.* When a TIS Letter or Report is required, a proposal is subject to the following criteria:

a. *The TIS addresses the applicable elements identified by the city engineer, consistent with the Traffic Impact Study Guidelines;*

b. *The TIS demonstrates that adequate transportation facilities exist to serve the proposed development or, in the case of a TIS report, identifies mitigation measures that resolve identified traffic safety problems in a manner that is satisfactory to the city engineer and, when state highway facilities are affected, to ODOT;*

c. *For affected non-highway facilities, the TIS report establishes that mobility standards adopted by the city have been met; and*

d. *Proposed public improvements are designed and will be constructed consistent with Public Works Design Standards and access standards in the Transportation System Plan.*

[...]

Finding: **Exhibit 4** includes a discussion of trip generation levels for the proposed development. With pass-by trips removed from the coffee/donut shop land use, the proposed land uses have the

potential to generate an additional 96 trips during the AM peak hour and 1 trip during the PM peak hour, which triggers the need for a full TIA (Transportation Impact Analysis), which the applicant has submitted and is attached as **Exhibit 4**.

The TIA addresses the applicable elements as specified by the City Engineer. Table 5 (shown below) within the study (**Exhibit 4**) demonstrates that the intersection of SW Walnut Street and Columbia River Hwy is operating at a Level of Service (LOS) E for the AM peak hour and LOS F for the PM peak hour, under both the 2021 background and 2021 buildout scenarios, which does not meet the City's mobility target standards (City standards state that the intersection must operate at a Level of Service D or better).

Table 5 from Transportation Impact Analysis, **Exhibit 4**

Description	Mobility Targets	2021 Background		2021 Buildout	
		AM	PM	AM	PM
Walnut Street / SE High School Way at US 30					
Level of Service	LOS D (City)	E	F	E	F
Control Delay (Sec) ¹	v/c=0.90 (City)	62.4	89.2	70.3	88.1
Volume to Capacity	v/c=0.85 (ODOT) ²	0.86	1.34	0.88	1.33
Notes: ¹ Delay is reported as the average total delay of the intersection					
² Based on the 1999 Oregon Highway Plan, Policy 1F, Table 6.					
Red = does not meet mobility target					

Since this is an ODOT facility; however, the transportation analysis must consider ODOT's mobility targets, which state that the volume to capacity ratio (v/c) should not exceed 0.85. As depicted in Table 5 above, during the AM and PM peak hours under both the 2021 background and 2021 buildout scenarios, the v/c ratio exceeds 0.85. The 2016 Transportation System Plan proposed alternative mobility targets along Hwy 30 in Scappoose since none of the intersection operations were expected to meet the OHP v/c mobility targets during the 20-year planning horizon. Some intersections, including the Hwy 30/High School Way intersection⁶, didn't meet the adopted OHP v/c ration of 0.85 even in 2012 when the traffic counts city wide were collected as part of the data used to develop the TSP.

ODOT submitted an official comment in regard to the TIA, which is attached as **Exhibit 6**. ODOT has stated that the Oregon Transportation Commission (OTC) currently has an action item on its March 2020 agenda to review and make a decision on adopting an alternative mobility target of 1.0 for signalized intersections in Scappoose. No mitigation for the US 30/SW Walnut Street intersection is being proposed by the applicant⁷ since the intersection fails to meet the mobility targets with or without the proposed project. ODOT has not required intersection mitigations for this project at this time.

ODOT additionally commented in a separate email attached as **Exhibit 7** that the closing of the driveway entrance adjacent to the Columbia River Hwy/SW Walnut Street intersection is a good

⁶ The TSP does not refer to the US 30/SW Walnut intersection, instead it is only referred to as the Hwy 30/High School Way intersection.

⁷ The applicant voluntarily opted to close the driveway entrance adjacent to the US 30/SW Walnut Street intersection to mitigate for the pass-through trips that were occurring to by-pass the intersection; however, they did not consider this a "mitigation" for intersection operations. ODOT's emailed comment does refer to the driveway closure as a "mitigation" though.

mitigation to address the increased trips associated with the remodel. ODOT will require that the applicant apply for a Change of Use Permit application due to the fact that the proposal involves adding a drive-thru coffee restaurant which will increase the trips at the subject site, is close to a signal, and is in proximity to a top 5% SPIS site (Safety Priority Index System), see **Exhibit 7**. This requirement has been included in the recommended conditions of approval.

The Public Works Director provided a comment included as **Exhibit 9** stating that the proposed new two-way driveway entrance near the Fir Lane and SW Walnut Street intersection (on Tax Lot 300) does not meet the access spacing standards in the PWDS for the required spacing between a commercial driveway and intersection. Since SW Walnut Street is considered a Collector, the required distance between the intersection and driveway should be 100-feet. As depicted on the Site Plan (**Exhibit 3g**), the distance between Fir Lane and the centerline of the new driveway is 32' - 2 ¾". Fir Lane is a privately owned tract that is not dedicated as right of way; however, for purposes of safety, staff requested the applicant have their traffic engineer review operations at the Fir Lane/SW Walnut Street intersection to see if mitigation was recommended. The TIA (**Exhibit 4**) does discuss the review of this intersection and recommends that a stop sign be installed for southbound traffic on Fir Lane at SW Walnut Street.

The PWDS does allow for a deviation if adhering to the standard would prohibit access to the site and if the City Engineer approves of the deviation. Since the proposed use is a drive thru coffee shop, one two-way driveway entrance and one exit only driveway is required for proper circulation on site. Meeting the standard distance of 100-feet would prohibit proper access for the drive thru use. The City Engineer has reviewed the proposed mitigation of having the developer install a stop sign on the southbound leg of Fir Lane at SW Walnut Street, which is acceptable to the City Engineer, and has been included in the recommended conditions of approval.

In conclusion, while the Columbia River Hwy/SW Walnut Street intersection does not meet the adopted LOS or v/c ratio for either the City of Scappoose or OHP (ODOT) standards, this is a known deficiency throughout the City at most signalized intersections in Scappoose on Columbia River Hwy, which will be addressed once the OTC adopts the alternative mobility standards changing the v/c target to 1.0 instead of 0.85. The applicant has proposed to close the existing driveway adjacent to the Columbia River Hwy/SW Walnut Street intersection to mitigate a known safety issue of drivers using it as a cut-through, by moving into the eastbound through lane and skipping the southbound left queues at the US 30/Walnut Street/SE High School Way intersection. Section 17.154.030, S, 4 is satisfied.

17.154.050 Easements.

A. Easements for sewers, drainage, water mains, electric lines or other public utilities shall be either dedicated or provided for in the deed restrictions, and where a subdivision is traversed by a watercourse, drainageway, channel or stream, there shall be provided a stormwater easement or drainage right-of-way conforming substantially with the lines of such watercourse and such further width as will be adequate for conveyance and maintenance.

B. A property owner proposing a development shall make arrangements with the city, the applicable district and each utility franchise for the provision and dedication of utility easements necessary to provide full services to the development.

Finding: The recommended conditions of approval require the applicant to provide a 5-foot Public Utility and Street Tree easement along the southern boundary of Tax Lot 300 adjacent to the SW Walnut Street frontage, as well as a 5-foot Public Utility Easement along the eastern boundary of Tax Lot 100 (adjacent to Columbia River Hwy) and the western boundary of Tax Lot 300 (adjacent to Fir Lane), per the written comment submitted by the Columbia River PUD (**Exhibit 10**).

The recommended conditions of approval also require that the applicant record a stormwater easement and stormwater access and maintenance covenant for City records for the drywells on site. Section 17.154.050, A - B is satisfied.

17.154.070 Sidewalks

A. Sidewalks are required and shall be constructed, replaced or repaired in accordance with the city's public works design standards.

[...]

Finding: As previously discussed, the applicant proposes to reconstruct the sidewalk where the existing driveway access is located near the intersection of SW Walnut and Columbia River Hwy (this driveway access is proposed to be closed permanently as part of this application). The applicant also proposes to reconstruct portions of the sidewalk on the Columbia River Hwy frontage when the commercial driveway is reduced in width to 36-feet, in conformance with the PWDS and ODOT requirements. The recommended conditions of approval require that the applicant inspect and replace any existing sidewalk panels along the SW Walnut and Columbia River Hwy frontages that are cracked or lifted or otherwise a danger to pedestrians. Section 17.154.070, A is satisfied.

17.154.090 Sanitary sewers. A. Sanitary sewers shall be installed to serve each new development and to connect developments to existing mains in accordance with the provisions set forth by the city's public works design standards and the adopted policies of the comprehensive plan.

B. The public works director shall approve all sanitary sewer plans and proposed systems prior to issuance of development permits involving sewer service.

C. Proposed sewer systems shall include consideration of additional development within the area as projected by the comprehensive plan and the wastewater treatment facility plan and potential flow upstream in the sewer sub-basin.

Finding: This application is for a remodel to an existing building. The existing building is already connected to sanitary sewer, as illustrated on the Utility Plan (**Exhibit 3d**). Sections 17.154.090, A – C are satisfied.

D. Applications shall be denied by the approval authority where a deficiency exists in the existing sewer system or portion thereof which cannot be rectified within the development and which if not rectified will result in a threat to public health or safety, surcharging of existing mains, or violations of state or federal standards pertaining to operation of the sewage treatment system.

Finding: This application is for a remodel to an existing building. There are no known deficiencies within the existing system that relate to this application. Section 17.154.090, D is satisfied.

17.154.100 Storm drainage. A. The planner and public works director shall issue permits only where adequate provisions for stormwater and floodwater runoff have been made, and:

- 1. The stormwater drainage system shall be separate and independent of any sanitary sewerage system.*
 - 2. Where possible, inlets shall be provided so surface water is not carried across any intersection or allowed to flood any street.*
 - 3. Surface water drainage patterns shall be shown on every development proposal plan.*
 - 4. All stormwater analysis and calculations shall be submitted with proposed plans for public works directors review and approval.*
 - 5. All stormwater construction materials shall be subject to approval of the public works director.*
- [...]*

Finding: As shown on the Grading and Utility Plans (**Exhibits 3c & 3d**), stormwater is proposed to flow into catch basins in the west parking area and then discharge to an underground drywell on site. The applicant proposes to replace the existing catch basin in the west parking area (Tax Lot 300) with new storm filter cartridge catch basins which will provide water quality treatment to the storm system. The existing downspouts from the building are proposed to be rerouted to the existing drywell. The applicant will be required to register all existing drywells onsite with the DEQ and retrofit the existing drainage system for the east parking lot as needed to comply with DEQ requirements.

As stated in the Preliminary Stormwater Drainage Report (**Exhibit 5**), the proposed stormwater management systems are designed in accordance with the PWDS and City of Portland Stormwater Management Manual (SWMM) for treatment and conveyance. The recommended conditions of approval require that the applicant submit a final stormwater report prepared by a professional engineer demonstrating that the proposed development's stormwater management is consistent with the PWDS. The recommended conditions of approval also require that prior to final occupancy, the applicant shall record a stormwater easement and sign and record a stormwater access and maintenance covenant for the City. Section 17.154.100 is satisfied.

17.154.105 Water system. The planner and public works director shall issue permits only where provisions for municipal water system extensions have been made, and:

- A. Any water system extension shall be designed in compliance with the comprehensive plan existing water system plans.*
- B. Extensions shall be made in such a manner as to provide for adequate flow and gridding of the system.*
- C. The public works director shall approve all water system construction materials.*

Finding: This application is for a remodel to an existing building. The existing building is already connected to the water main within the Columbia River Hwy frontage via a 3/4" meter, as

illustrated on the Utility Plan (**Exhibit 3d**). The applicant proposes to install sub water meters to each tenant space. Section 17.154.105 is satisfied.

17.154.107 Erosion Controls

- A. Any time the natural soils are disturbed and the potential for erosion exists, measures shall be taken to prevent the movement of any soils off site. The public works director shall determine if the potential for erosion exists and appropriate control measures.*
- B. The city shall use the city's public works design standards as the guidelines for erosion control.*

Finding: The applicant will be required to comply with the City's erosion control guidelines within the Public Works Design Standards. Section 17.154.107 is satisfied.

17.154.120 Utilities

- A. All utility lines including, but not limited to those required for electric, communication, lighting and cable television services and related facilities shall be placed underground, except for surface mounted transformers, surface mounted connection boxes and meter cabinets which may be placed above ground, temporary utility service facilities during construction, high capacity electric lines operating at fifty thousand volts or above, and:*
- 1. The applicant shall make all necessary arrangements with the serving utility to provide the underground services;*
 - 2. The city reserves the right to approve location of all surface mounted facilities;*
 - 3. All underground utilities, including sanitary sewers, water lines, and storm drains installed in streets by the applicant, shall be constructed prior to the surfacing of the streets; and*
 - 4. Stubs for service connections shall be long enough to avoid disturbing the street improvements when service connections are made.*

Finding: This application is for a remodel to an existing building. The existing building is already served by franchise utilities. The applicant would be required to make arrangements with the serving utility company to relocate the communication pedestal and utility vault adjacent to the new proposed drive-thru exit lane. Section 17.154.120 is satisfied.

17.162.090 Approval authority responsibilities. [...]

C. The planning commission shall conduct a public hearing in the manner prescribed by this chapter and shall have the authority to approve, approve with conditions, approve with modifications or deny the following development applications:

[...]

- 5. Minor Variance pursuant to Chapter 17.134*

Finding: The applicant has submitted a Minor Variance application on forms provided by the City of Scappoose, has paid the applicable land use fees, and the Planning Commission is by the very nature of the public hearing deliberation, following the correct procedures. Based on the submitted materials, the findings within this staff report, including the conditions of approval, the application complies with the City's Comprehensive Plan, and with the requirements of Title 17 of the Scappoose Municipal Code. Section 17.162.090(C) is satisfied.

17.162.140 Decision process.

A. The decision shall be based on:

1. Proof by the applicant that the application fully complies with:

- a. Applicable policies of the city comprehensive plan; and
- b. The relevant approval standards found in the applicable chapter(s) of this title, the public works design standards, and other applicable implementing ordinances

B. Consideration may also be given to:

1. Proof of a substantial change in circumstances or a mistake in the comprehensive plan or zoning map as it relates to the property which is the subject of the development application; and
2. Factual oral testimony or written statements from the parties, other persons and other governmental agencies relevant to the existing conditions, other applicable standards and criteria, possible negative or positive attributes of the proposal or factors in subsections (A) or (B) (1) of this section.

Finding: Based on the submitted materials, the findings within this staff report, including the conditions of approval, the application complies with the City's Comprehensive Plan, and with the requirements of Title 17 of the Scappoose Municipal Code. Section 17.162.140, A & B is satisfied.

Chapter 17.164 PROCEDURES FOR DECISION MAKING—LIMITED LAND USE DECISIONS

[...]

17.164.110 Approval authority responsibilities.

B. The planning commission shall have the authority to approve, deny or approve with conditions the following applications:

[...]

3. Site development review pursuant to Chapter 17.120.

Finding: The request is for Site Development Review approval for a remodel to existing development, which is a limited land use decision. The applicant has submitted the required application on forms provided by the City of Scappoose, has paid the applicable land use fees, and the Planning Commission is following the correct procedures by the public meeting deliberation. Section 17.164.110(B) is satisfied.

17.164.150 Decision process.

A. The decision shall be based on proof by the applicant that the application fully complies with:

1. The city comprehensive plan; and
2. The relevant approval standards found in the applicable chapter(s) of this title and other applicable implementing ordinances;

Finding: The application is in conformance with the Comprehensive Plan and the relevant approval standards found in the applicable chapters of the Scappoose Municipal Code, as discussed previously within this report. Section 17.164.150 is satisfied.

RECOMMENDATION

Based on the Findings of Fact and the materials submitted by the applicant, staff recommends that the Planning Commission **APPROVE** **SDR5-19, MiV3-19** subject to the following conditions:

GENERAL

1. This approval (SDR5-19 and MiV3-19) authorizes the remodel of Varsity Grill & Bar into three new tenant spaces, to include a coffee shop drive thru restaurant and two office/retail spaces, and associated site amenities. This approval is effective for one year.
2. The applicant shall meet all applicable uniform codes such as structural, plumbing, mechanical, electrical and fire codes, as stated in Section 15.04.010 of the Scappoose Municipal Code, prior to occupancy and the issuance of a business license for the drive-thru coffee shop and office/retail uses.

SITE PROVISIONS

3. The applicant shall be required to modify the Lighting Plan to ensure that light is deflected away from the residential use to the west of the building. Parking lot and site lighting shall be required and shall be designed to deflect light away from streets and neighboring properties. Fixture height, light type and lighting levels shall function so as to assure compatibility with neighboring land uses. Shields shall be incorporated as necessary to minimize glare and to focus lighting to its intended area.
4. The applicant shall provide signing for the disabled parking spaces and label all parking spaces using permanent paint. Regular parking spaces shall have a width of nine feet and a length of eighteen feet and compact spaces shall have a width of eight and a half feet and a length of 15 feet. The applicant shall install wheel stops or curbs along the boundaries of the parking lot and adjacent to interior landscaped areas. All interior drives and access aisles shall be clearly marked and signed to show direction of flow and maintain vehicular and pedestrian safety.
5. The applicant shall screen any outside storage areas or any refuse containers, meeting the requirements of Sections 17.100.100(B & C) of the Scappoose Municipal Code.
6. The applicant shall provide 5 bicycle parking spaces meeting the requirements of Section 17.106.020(P) of the Scappoose Municipal Code prior to final occupancy of any tenant spaces.
7. The applicant shall install the landscaping substantially in conformance with the Landscaping Plan (**Exhibit 3f**) prior to final occupancy of any tenant spaces.

UTILITIES AND STREET IMPROVEMENTS

8. The applicant shall obtain a right-of-way (ROW) permit for all public improvements (ODOT facilities require that the applicant apply for a ROW permit through ODOT)

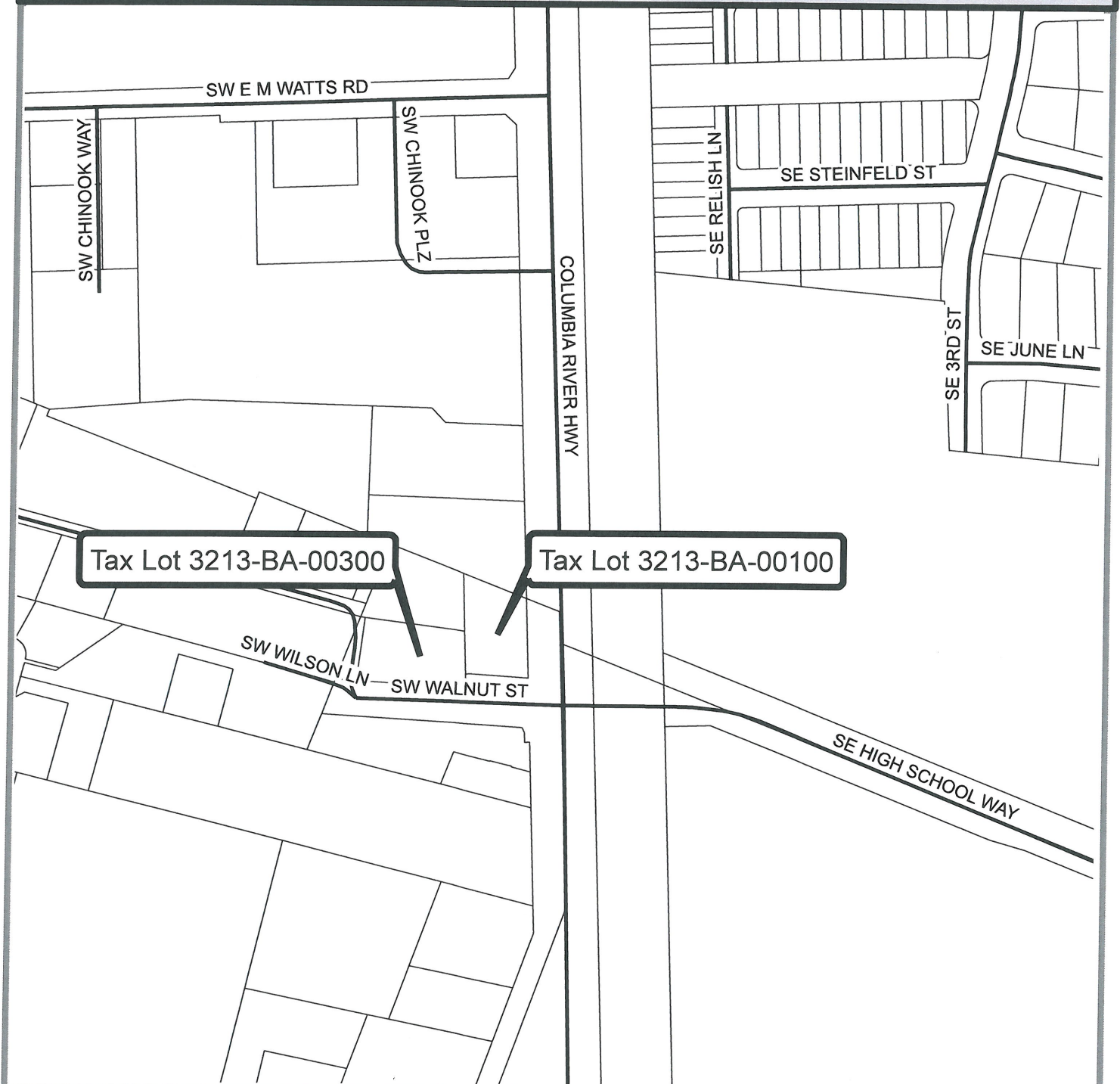
along with an engineer's estimate including administration fees, sign an Improvement Agreement and provide a bond for all proposed public improvements. Improvements within the ROW shall meet the Scappoose Public Works Design Standards (PWDS).

9. The applicant shall apply for a Change of Use Permit from ODOT and shall comply with the provisions of the permit prior to final occupancy of any tenant spaces.
10. The applicant shall provide a final stormwater report prepared by a professional engineer demonstrating that the proposed development's stormwater management is consistent with the Public Works Design Standards. Prior to final occupancy, the applicant shall record a stormwater easement and sign and record a stormwater access and maintenance covenant for City records.
11. The applicant shall register all of the on-site drywells with DEQ prior to final occupancy of any tenant spaces.
12. The applicant shall inspect and replace any existing sidewalk panels along the Columbia River Hwy and SW Walnut Street frontages that are cracked, lifted, or otherwise a danger to pedestrians prior to final occupancy of any tenant spaces.
13. The applicant shall obtain a fill and grading permit from the City for lot fill and grading, including the installation of any necessary erosion control measures, per the standards set forth in the Scappoose Public Works Design Standards and Specifications. Erosion control measures shall be reviewed and approved by the City Engineer and the Oregon Department of Environmental Quality as part of an Erosion Control Plan. The applicant shall submit an acceptable Erosion Control Plan meeting DEQ requirements and City of Scappoose Public Works Design Standards, Section 2.0051.
14. The applicant shall provide a 5-foot Public Utility and Street Tree easement along the southern boundary of Tax Lot 300 adjacent to the SW Walnut Street frontage. The applicant shall also provide a 5-foot Public Utility Easement along the eastern boundary of Tax Lot 100 (adjacent to Columbia River Hwy) and the western boundary of Tax Lot 300 (adjacent to Fir Lane).
15. The applicant shall install a total of three Trident Maple (or similarly sized trees from the approved street tree list) street trees along the frontage of SW Walnut Street, in accordance with the requirements of Scappoose Municipal Code 13.28.020, including the provision of root guard adjacent to the sidewalk.
16. Within the sanitary sewer easement on Tax Lot 100, the applicant shall ensure that no shrubs or trees normally expected to exceed 4-feet in height at maturity are planted, per 3.0024 – Sanitary Easements, of the PWDS.
17. The applicant shall install a stop sign on the southbound leg of Fir Lane at SW Walnut Street at a location to be determined and approved by the City.

Exhibit 1

Vicinity Map - Varsity Grill Remodel
Site Development Review - SDR5-19 and Minor Variance - MiV3-19

Columbia County Assessor Map # 3213-BA-00300 and 3213-BA-00100



Scappoose GIS

Legend

-  Streets
-  Taxlots Boundary





Scappoose Planning Department
 33568 E. Columbia Ave. Scappoose, OR 97056
 Phone: 503-543-7184 Fax: 503-543-7182
www.ci.scappoose.or.us

SITE DEVELOPMENT REVIEW APPLICATION

NOTICE TO APPLICANT: On original application form, please print legibly using black/dark blue ink or type. Applicants are advised to review the list of submittal requirements and recommendations indicated on each land use application form and in the applicable code section prior to submitting an application. Applicants are advised to schedule a pre-application meeting with the staff prior to submitting final application. **INCOMPLETE APPLICATIONS WILL NOT BE PROCESSED UNTIL THE PLANNING DEPARTMENT RECEIVES ALL REQUIRED SUBMITTAL MATERIALS.**

TRACKING INFORMATION (For Office Use Only)

Application Submittal Includes:

☐ 7 Hard Copies Required

☐ Electronic Submittal

☐ Fee

File # _____ Hearing Date _____

SITE LOCATION & DESCRIPTION

Tax Map #(s) 30213BA

Account #: 4588; Map Tax Lot #: 3N2W13BA 300

Tax Lot #(s) Account #: 4586; Map Tax Lot #: 3N2W13BA 100

Frontage Street or Address 52001 S COLUMBIA RIVER HWY

Nearest Cross Street WALNUT ST

Plan Designation _____ Zoning EC Site Size 23,492 (11,802 + 11,690) ☐ acres ☒ sq. ft.

Dimensions IRREGULAR - SEE DRAWINGS

SUMMARY OF REQUEST

Proposed Project Name VARSITY GRILL REMODEL Estimated Valuation \$ \$350,000

Project Type/Narrative Summary: (Provide a brief summary and specify project type: Single-Family Residential (SFR), Multi-Family Residential (MFR), Accessory Dwelling Unit (ADU), Commercial, Industrial, Mixed Use) COMMERCIAL

The project proposes to redevelop the existing bar/restaurant into three tenant spaces. Exterior alterations to the existing building include: storefront replacement, additional doors/windows, drive-thru window, ATM alcove, and canopy fascia. The parking lot to the west will be redeveloped to include a drive-thru, code compliant parking, lighting, and landscape.

Is a variance requested? ☒ Yes ☐ No (If yes, identify what type of request) ☒ Minor Variance ☐ Major Variance

NOTE: Procedures and applicable criteria for variances may be found in SDC Chapter 17.134

Subject to previous land use approval? ☐ Yes ☒ No File No. _____ (attach copy of Notice of Decision)

of Phases Proposed 1 # of lots 2 Landscaping (sq. ft.) 2,703 (2,193 + 510)

SITE DEVELOPMENT REVIEW APPLICATION

(CONTINUED)

Paving (sq. ft.) 16,322 # of Parking Spaces 31 # of Accessible Parking Spaces 2
Maximum Lot Size 11,802 (Sq. Ft.) Minimum Lot Size 11,690 (Sq. Ft.) Average Lot Size 11,746 (Sq. ft.)

NOTE: *If a residential project is proposed, a Residential Density Calculation Worksheet must be submitted.*

If Mixed Use, please specify types of uses and approximate percentages of overall site area in each use:

Commercial _____ % Industrial _____ % Residential _____ %

If Commercial or Industrial: # of non-residential buildings 1 Total Square Footage 4,060

DETAILED SITE INFORMATION

Are any of the following present on site? *If so, please specify the number of acres and/or percentage of site affected.*

Floodplain _____ Wetlands _____ Significant Natural Resources _____

Cultural Resources _____ Airport Noise Contours _____ Slopes greater than 20% _____

Water Provider: ☒ City of Scappoose ☐ Well

Does the site have access to City street(s)? ☒ Yes ☐ No (Please explain): _____

Does the site have access to County road(s)? ☒ Yes ☐ No (Please explain): _____

Are there existing structures on the site? ☒ Yes ☐ No (If Yes, briefly explain future status of structures.) The existing structure will remain with minor alterations to renovate the existing bar/restaurant into three new tenant spaces.

OWNERSHIP AND APPLICANT INFORMATION (Property owner signature must be a wet-ink signature. If the property is under-going a change of ownership, proof of purchase or purchase contract must be provided if property owner of record is not the signing party.)

Property Owner(s): Name(s) CRG PROPERTIES, LLC

Business Name CRG PROPERTIES, LLC

Mailing Address 52001 S COLUMBIA RIVER HWY City SCAPPOOSE State OR Zip 97056

Phone # 503-543-7444 Fax # _____ Email Address _____

503-317-0902
Does the owner of this site also own any adjacent property? ☐ Yes ☒ No (If Yes, please list tax map and tax lots) _____

Property Owner(s) Signature(s)  Date: 11.18.19
(If more than one property owner, please attach additional sheet with names and signatures.)

Applicant: Name PATRICK KESSI

Business Name PHK DEVELOPMENT, INC

Mailing Address 3330 NW YEON AVE, SUITE 210 City PORTLAND State OR Zip 97210

Phone # (503) 248-9370 Fax # _____ Email Address PKESSI@PHKINC.COM

Applicant's Signature  , PRESIDENT Date: 11-18-19

Applicant's interest in property DEVELOPER, OWNER'S AUTHORIZED REPRESENTATIVE

Additional Project Team Members

Applicant's Representative: Contact Name MICHAEL PERSO

Business Name WILLIAM KAVEN ARCHITECTURE

Mailing Address 4075 N WILLIAMS AVE, SUITE 440 City PORTLAND State OR Zip 97227

Phone # (503) 841-5239 Fax # _____ Email Address MIKE@WILLIAMKAVEN.COM

Civil Engineer: Contact Name RYAN MILKOWSKI

Business Name KPFF CONSULTING ENGINEERS

Mailing Address 111 SW FIFTH AVE, SUITE 2600 City PORTLAND State OR Zip 97204

Phone # (503) 542-3860 Fax # _____ Email Address RYAN.MILKOWSKI@KPFF.COM

Architect: Contact Name MICHAEL PERSO

Business Name WILLIAM KAVEN ARCHITECTURE

Mailing Address 4075 N WILLIAMS AVE, SUITE 440 City PORTLAND State OR Zip 97227

Phone # (503) 841-5239 Fax # _____ Email Address MIKE@WILLIAMKAVEN.COM

Landscape Architect: Contact Name NATE OTANI

Business Name SHAPIRO DIDWAY

Mailing Address 1204 SE WATER AVE, SUITE 21 City PORTLAND State OR Zip 97214

Phone # (503) 841-5239 Fax # _____ Email Address NATE@SHAPIRODIDWAY.COM

Additional Personnel:

Role TRAFFIC ENGINEER Contact Name MONICA LEAL

Business Name GLOBAL TRANSPORTATION ENGINEERING

Mailing Address 227 SW PINE ST, SUITE 220 City PORTLAND State OR Zip 97204

Phone # (503) 719-7997 Fax # _____ Email Address MONICA.LEAL@GLOBALTRANSENG.COM

WILLIAM / KAVEN

SITE ACTIVITY STATEMENT

DATE: November 12, 2019
PROJECT NAME: Varsity Grill Remodel
PROJECT ADDRESS: 52001 Columbia River Hwy

Project Description

The project proposes to redevelop the existing bar/restaurant into three new tenant spaces by demolishing the existing interior and building demising walls and individual suite entrances. The existing parking lot to the west of the building will be redeveloped to include a drive-thru, code compliant parking, lighting, and landscape.

Proposed exterior alterations include: demolish existing wall and storefront on east elevation and replace with storefront, canopy fascia and an ATM alcove, add service doors, drive-thru service window and fabric awning to west elevation, add window to north elevation, add storefront door to south elevation.

Proposed site improvements include: close both existing curb cuts on Walnut, add new curb cuts for two-way parking access and one-way drive-thru exit on Walnut, repave/restripe east parking area for two code compliant ADA stalls, demolish existing trash area, construct new trash enclosure partially in existing easement, add drive-thru lane, planting island, landscaping, pole mounted lighting and pedestrian circulation system to existing west parking lot.

Proposed Use

The proposed uses for the site are:	Gross SF	Hours
Eating/Drinking Establishments:	2,030 GSF	Early Morning-Evening/Night
General Retail/Office:	2,030 GSF	Unknown

Planned Number of Shifts

The exact number of shifts and employees per shift are unknown at this time, but approximate hours are indicated above.

Treatment and Disposal of Industrial Wastes

No industrial waste is planned to be generated by the proposed uses for the site. A grease interceptor will be provided for the plumbing systems for the Restaurant use.

Mitigation Plans

The proposed redeveloped parking lot will be screened from the adjacent residential use by a 6' high continuous sight obscuring fence, and a landscaped buffer. The curb cut at Lot 100 along Walnut St will be closed to promote safety at the Walnut St and Hwy 30 intersection. Parking lot lighting will be added to the Lot 300 parking area to promote safety and directed away from adjacent uses to limit glare and light pollution. A 10-11 car reservoir is provided on site for the drive-thru to mitigate traffic impacts on Walnut St. Landscaping is proposed to screen the Lot 300 parking area from the adjacent ROW.

WILLIAM / KAVEN

SITE DEVELOPMENT REVIEW NARRATIVE AND RESPONSE TO APPROVAL CRITERIA

DATE: December 23, 2019, REV 1/21/20
PROJECT NAME: Varsity Grill Remodel
PROJECT ADDRESS: 52001 Columbia River Hwy

Project Description

The project proposes to redevelop the existing bar/restaurant into three new tenant spaces by demolishing the existing interior and building demising walls and individual suite entrances. The existing parking lot to the west of the building will be redeveloped to include a drive-thru, code compliant parking, lighting, and landscape.

Proposed exterior alterations include: demolish existing wall and storefront on east elevation and replace with storefront, canopy fascia and an ATM alcove, add service doors, drive-thru service window and fabric awning to west elevation, add window to north elevation, add storefront door to south elevation.

Proposed site improvements include: close both existing curb cuts on Walnut, add new curb cuts for two-way parking access and one-way drive-thru exit on Walnut, repave/restripe east parking area for two code compliant ADA stalls, demolish existing trash area, construct new trash enclosure partially in existing easement, add drive-thru lane, planting island, landscaping, pole mounted lighting and pedestrian circulation system to existing west parking lot.

Response to 17.120.180 Approval Standards

A. Provisions of all applicable chapters

Response:

17.104 Street Trees: The current ROW sidewalk section does not provide sufficient width for street trees. However, the project proposes to provide trees on private property adjacent to the ROW to act as street trees. The trees will also meet the requirements for off-street parking landscaping. The three trees proposed between the two drive aisles on Walnut Street are proposed as street trees and meet the requirements of 17.104.040. They are proposed to be Trident Maples, selected from the approved tree list under Small Trees with a maximum required spacing of 20' and a four foot wide planting area. The proposed trees will be spaced 20' apart in a landscape strip that is 4'-2" wide. These trees will also meet the 300 sf canopy requirement for parking screening/landscaping.

17.132 Nonconforming Uses: Portions of the existing conditions are legally nonconforming as the building was constructed prior to the establishment of Title 17. The proposed development seeks to improve these nonconforming situations, but cannot bring the project into conformance in the following instances which are described in greater detail in the subsequent narrative sections:

1. Width of existing parking aisle at Lot 100 - no proposed change
2. Width of existing and proposed buffer at adjacent residential use, north lot line of Lot 300, partial west lot line of Lot 100 - propose adding required fence screen, required landscape plantings, increase average width of buffer
3. Screening of existing off-street parking at Lot 100 - no proposed change

4. Setback of existing building at south lot line of Lot 100 - reduce non conforming length of wall by 6'-9" or 13.4%
5. Minimum existing and proposed lot coverage of Lot 100 - limited change to Lot 100, proposed increased impermeable surface at Lot 300 to bring the combined lot coverage to 88.5% from an existing combined lot coverage of 93.7%

The existing gross building area is being reduced 4.1%.

17.134 Variance: A minor variance may be granted if the proposal meets criteria A through C in section 17.134.030. The proposed development is not materially detrimental to the purposes of Title 17 or other applicable policies nor to other properties in the vicinity. The site has been used successfully for many years and the proposed development seeks to mitigate existing nonconforming situations by: eliminating the existing Lot 100 curb cut to Walnut St, decreasing the lot coverage, and providing required screening and landscaping and increasing the buffer width at the adjacent residential use. The use proposed at the site is conforming and permitted outright by Title 17. Existing physical and natural systems are not adversely affected by the proposed development as the primary adverse physical feature of the Lot 100 Walnut St curb cut will be eliminated.

17.154 Street and Utility Standards:

Existing Conditions: The proposed development has frontage on both Walnut St and Hwy 30. No change to the existing ROW profile is proposed. The existing conditions are as follows: at Walnut St the dimension from the face of curb to back of sidewalk is a minimum of 5'-10 ½" with an approximately 1'-2" strip between the back of the sidewalk and the lot line. This zone is proposed as part of the parking lot landscape planter. At Hwy 30 the dimension from the face of the curb to the back of sidewalk varies between approximately 6'-0" to 7'-3" with a strip between the back of the sidewalk and the lot line varying from 1'-3" to 2'-7 ½". This strip consists of existing asphalt pavement to remain. The centerline of Walnut St to the south lot line of Lot 300 is 27' and to the south lot line of Lot 100 it is 42'.

Proposed Street Improvements: The project proposes to close the curb cut between Walnut Street and the east parking lot which is currently used by drivers to short circuit the intersection, creating an unsafe condition. The project also proposes to reduce the width of the curb cut between the east parking lot and Hwy 30 to 36' bringing this drive aisle into conformance with City standards. The sidewalk section here will also be repaved to ensure proper ADA grading. For the west parking lot, the project proposes closing the existing two-way drive aisle and adding a new 24' two-way drive aisle to the west and adding a 12' one-way exit drive aisle from the drive-thru lane. Lastly, the project proposes adding a concrete patio in the right-of-way south of the building between the building and the sidewalk. The project will seek City Manager approval for this use.

Existing Easements: Easements are shown on the provided ALTA survey and site plan. The project does propose a portion of the trash enclosure wall and pad to extend over the road/sanitary/water easement that extends south 20' from the Lot 100 north lot line. 78 SF of the enclosure would extend into the easement, 3'-4" in the NW corner and 9'-0" in NE corner. Required plumbing for drain and hose bib would not extend into the easement.

- B. *Buildings shall be located to preserve topography, and natural drainage; located in areas not subject to ground slumping or sliding; located to provide adequate distance between adjoining buildings for adequate light, air circulation, and firefighting; and oriented with consideration for sun and wind; and*

Response: N/A, the building is existing.

- C. *Existing trees having a six-inch caliper or greater shall be preserved or replaced by new plantings of equal character;*

Response: There are no existing trees currently on the site.

D. *Privacy and noise:*

- a. *The buildings shall be oriented in a manner which protects private spaces on adjoining properties from view and noise,*

Response: N/A the building is existing

- b. *Residential buildings shall be located on the portion of the site having the lowest noise levels, and*

Response: N/A

- c. *On-site uses which create noise, lights, or glare shall be buffered from adjoining residential uses;*

Response: The proposed trash area will be screened from view by a 7'-4" ground face CMU wall and buffered from the adjacent residential use by landscaping and a 6' high fence. Lights will be directed down to adjacent walking surfaces and minimized near the adjacent residential use.

E. *Private outdoor area: residential use:*

Response: N/A

F. *Shared outdoor area: residential use:*

Response: N/A

- G. *Where landfill and /or development is allowed within and adjacent to the one hundred-year floodplain, the city may require the dedication of sufficient open land area for greenway adjoining and within the floodplain. This area shall include portions at a suitable elevation for the construction of a pedestrian/bicycle pathway within the floodplain;*

Response: N/A

H. *Demarcation of public, semipublic, and private spaces; crime prevention:*

- a. *The structures and site improvements shall be designed so that public areas such as streets or public gathering places, semipublic areas and private outdoor areas are clearly defined in order to establish persons having a right to be in the space, in order to provide for crime prevention and to establish maintenance responsibility; and*

Response: The site implements landscape strips where possible to demarcate private property or semi-public space from the ROW or public portions of the ROW. Private use areas such as the trash enclosure are secured from view or access. A continuous fence at the adjacent residential use clearly demarcates the lot line and establishes responsibility for the landscape and paving areas on the property side of the fence.

- b. *These areas may be defined by a deck, patio, low wall, hedge or draping vine, a trellis or arbor, a change in level or landscaping;*

Response: The patio proposed in the ROW and subject to City Manager approval is proposed to be framed with landscaping separating it from the public sidewalk and concrete paved and raised from the adjacent parking area

I. *Crime prevention and safety:*

- a. *Windows shall be located so that areas vulnerable to crime can be surveyed by the occupants,*

Response: The existing building's windows are proposed to remain. Additional windows are proposed along the east elevation in the two south bays, one additional window on the north elevation and one service window on the west elevation. These north windows

will support the surveillance of the trash area. Extensive windows along the east and south will support the surveillance of the patio and east parking areas. The addition of the drive-thru service window on the west elevation will support the surveillance of the west parking area.

- b. Interior laundry and service areas shall be located in a way that they can be observed by others,*

Response: N/A

- c. Mail boxes shall be located in lighted areas having vehicular or pedestrian traffic,*

Response: N/A, mail will be delivered directly to each tenant space.

- d. The exterior lighting levels shall be selected and the angles shall be oriented towards areas vulnerable to crime, and*

Response: Additional pole mounted lighting is proposed in the west parking area while additional wall pack lighting is proposed around the perimeter of the building and directed at the walking surfaces. See provided photometric site plan.

- e. Light fixtures shall be provided in areas having heavy pedestrian or vehicular traffic and in potentially dangerous areas such as parking lots, stairs, ramps, and abrupt grade changes. Fixtures shall be placed at a height so that light patterns overlap at a height of seven feet which is sufficient to illuminate a person;*

Response: Light fixtures have been provided as indicated above. See provided photometric site plan.

J. Access and circulation:

- a. The number of allowed access points for a development shall be as provided in the public works design standards.*

Response: The existing access point to Lot 100 along Walnut St which is too close to the Hwy 30 intersection is proposed to be closed to bring the development closer to conformance with access requirements. One access point to Lot 100 has been provided off Hwy 30 in the north portion of the site. This two-way aisle is proposed to be reduced to 36' wide per public works design standards. One access point to Lot 300 is proposed along Walnut St in the southwest portion of the site. This two-way aisle is proposed to be 24' wide. Additionally, one exit only egress point is proposed along Walnut St in the center of the development, southeast portion of Lot 300. This one-way aisle is proposed to be 12' wide.

- b. All circulation patterns within a development shall be designed to accommodate emergency vehicles.*

Response: 24' wide two-access aisles are provided at Lot 300 and a thru, one way 12' access aisles is provided at the drive-thru. Curb cuts meet public works standards.

- c. Provisions shall be made for pedestrian ways and bicycle ways if such facilities are shown on an adopted plan;*

Response: N/A

K. Public transit:

Response: N/A, no public transit related facilities or connections have been identified in conjunction with this proposal.

- L. *All parking and loading areas shall be designed in accordance with the requirements set forth in Sections 17.106.050 and 17.106.080, Chapter 12.10, and the public works design standards;*

Response:

Minimum Off-street Parking Requirements:

Eating/Drinking Establishments (1/120 GSF):	2,030 GSF	16.92
General Retail (1/400 GSF):	2,030 GSF	5.08
<u>Total Required:</u>		<u>22.00</u>
Total Provided:		31 (18 standard, 13 compact)

Additionally, a 10 space reservoir is required for each drive-in restaurant service window. The 10 spaces have been shown on the site plan while maintaining access to the parking area. Specifically, there is a 4 car reservoir at the order station, a 5 car reservoir at the service window and a 1 car reservoir at the curb cut. An additional car could be accommodated on site by temporarily blocking access to the parking area.

Parking Dimension Standards: All 18 proposed standard parking spaces are proposed to be 9'-0"x18'-0". All 13 proposed compact parking spaces are proposed to be 8'-6"x15'-0" and are identified on the site plan. On Lot 300, all two-way drive aisles are proposed to be 24' wide and the one-way drive-thru aisles is proposed to be 12' wide. At Lot 100, the existing parking is proposed to be restriped as noted above. No changes are proposed to the existing drive aisle which varies from 20'-9 1/4" at the narrowest to 23'-5 7/8" at the widest. See proposed site plan for widths at various points. A minor variance will be required for this existing non-conforming condition. Visual clearance areas are noted on the plans and proposed development is outside of these areas. Wheel stops are located at all parking spaces, three feet back from the front of the parking stall. Drainage has been designed to be directed to the drywell in the center of the lot. Artificial lighting has been designed to illuminate the parking areas and walkways but be directed away from adjacent development, see the proposed photometric plan. The walkway that crosses the Lot 300 drive-thru lane is raised and concrete, contrasting from the asphalt drive-aisle.

Off-street Loading: N/A

Bicycle Parking: For commercial uses, the greater of two bicycle spaces or two per five vehicle spaces are required. At 31 vehicle spaces, seven bicycle spaces are required. The project proposes four floor mounted hoop style powder coated steel racks providing seven 2'x6' clear bicycle spaces. Three are located in the northeast corner of Lot 300 near the west elevation tenant entries. Four are located just south of the compact parking space in the east parking lot.

- M. *All landscaping shall be designed in accordance with the requirements set forth in Chapter 17.100;*

Response:

Buffering and screening requirements: The north lot line of Lot 300 and the north portion of the west lot line of Lot 100 are adjacent to an existing non-conforming residential use in the EC zone. Buffering and screening are required per 17.100.090. The existing condition is no buffer with some landscape and a partial fence at the north lot line and a partial fence at the west lot line with portions of the use coming directly up to the existing building. Providing a full 10' buffer is not possible as it would render the parking lot unusable along the north lot line and be within the building footprint along the west lot line. The proposed plan is to provide a 6' tall continuous sight obscuring fence along the entire north lot line of Lot 300 and west lot line of Lot 100 where adjacent to the existing residential use. Additionally, at the north lot line of Lot 300, a 5' wide buffer and required landscape plantings per 17.100.090.D are proposed in the 5' buffer:

1. Trees: At the Lot 300 north property line, tulip poplars, small or narrow stature trees, will be placed 15 feet apart for approximately two-thirds of the property line until the planting strip tapers to be too narrow to support tree growth. Along the Lot 100 west property line, trees are not possible because of the very narrow distance from the lot line to the building itself.
2. Shrubs: Along the north lot line of Lot 300 and north of the building along the west property line of Lot 100, five-gallon greenspire shrubs are proposed, spaced approximately 18" apart. Required quantity is one per 100 sf or, given the required 10' buffer, one per 10 feet. The more dense spacing is intended to mitigate the required plantings that cannot be provided because of practical size constraints, and to support an evergreen shrub as described below.
3. Groundcover/mulch: the remaining buffer is proposed to be covered in sedge groundcover.

A screen is also required per 17.100.090.E. The project proposes to use option 3, a continuous sight obscuring fence. For durability, the project proposes a black vinyl coated steel chain link fence with dark grey privacy slats. The greenspire shrubs will combine to create a dense evergreen hedge except along the portion of the site adjacent to the building itself where there is not sufficient width for any planting.

Overall, while not strictly compliant, the project proposes as much buffer width and planting as practical to meet the intent of the standard and significantly improve the existing non-confirming condition. A minor variance will be required for this.

Screening-Special Provisions: Off-street parking screening is required. Since limited restriping and repaving are proposed at the existing Lot 100 parking area, no screening or trees are proposed as adding any planting area adjacent to the Hwy 30 ROW would reduce the already non-confirming parking aisle width in this condition; a minor variance is required for this existing condition. At Lot 300, five trees for the combined Lot 300 and Lot 100 Walnut St frontage (152 LF + 92 LF = 244 LF) are proposed, four along Walnut St and one along SW Fir LN in the NW corner of the site. Additionally, 1,166 SF of landscaping per 17.100.100.A is proposed along the Walnut frontage at Lot 300 where 976 SF would be required for the combined Lot 300 and Lot 100 Walnut frontage (608 SF + 368 SF). The trash and recycling area located to the north of the existing building will be screened per 17.100.100.C with a 7'-4" high ground face CMU wall on three sides and a square profile box rib metal gate on the fourth.

- N. *All drainage plans shall be submitted to the public works director for review and approval;*

Response: Drainage plans, drywell test reports, and storm water analysis have been provided for review. The existing drywell on site has been tested and has sufficient capacity to manage the existing Lot 300 surface drainage and the roof drainage from the existing building on Lot 100 which currently daylight to the surface planter on the west side of the lot.

- O. *All facilities for the handicapped shall be designed in accordance with the requirements set forth in the ADA requirements; and*

Response: The existing parking area on Lot 100 will be restriped to accommodate ADA and OSSC requirements for accessible parking. Specifically, one space will be added to accommodate the full combined site's parking count and both spaces will be increased to 9' wide with a van accessible 9' wide access aisle provided. The existing paving at these spaces will be removed and regraded to meet ADA and OSSC slope and cross slope requirements and signage will be provided. New storefront entrances along the east elevation will meet ADA and OSSC accessibility requirements.

- P. *All of the provisions and regulations of the underlying zone shall apply*

Response:

The underlying zone is 17.68, EC Expanded Commercial.

Use: The uses proposed for the site: an Eating/Drinking Establishment and one or two General Retail Sales or Professional/Administrative Office uses are permitted outright. While a patio with tables for the Eating/Drinking Establishment is proposed to the south of the existing building, this area is in the ROW and we understand that its use is subject to approval by the City Manager and does not require a conditional use as it is not on private property.

Lot Size: The lots conform to specified dimensional requirements.

Setbacks: The existing building conforms to all required setbacks, except along Walnut St, where Lot 100 has been dedicated. At this condition the existing building is set back 4'-0 5/8" at the SE corner and extends over the property line by 0'-2 1/8" at the SW corner pilaster. The building face is set back 0'-6" at the SW corner. We understand a variance is required for this condition.

Height: The existing building is approximately 12' tall and 15' tall at the canopy; allowable: 50'.

Maximum Lot Coverage: The proposed lot coverage for Lot 300 is 81.4% and for Lot 100 is 95.7%. A minor variance is required for Lot 100, however Lot 100 already does not comply with the lot coverage requirements. The combined lot coverage, for both lots, is proposed to be 88.5%. The existing combined lot coverage is 93.7%.

Q. Pedestrian Access and Circulation Standards. Developments shall conform to all of the following standards for pedestrian access and circulation:

- a. Continuous Walkway System. A pedestrian walkway system shall extend throughout the development site and connect to adjacent sidewalks, if any, and to all future phases of the development, as applicable.*

Response: Each parking lot in the project is connected to the buildings nearest entries by a pedestrian walkway system. The walkway between the east parking lot and the building is proposed to be extended through the ROW to the public sidewalk along Walnut St enhancing the site's connectivity with a direct to the adjacent sidewalk

- b. Safe, Direct, and Convenient. Walkways within developments shall provide safe, reasonably direct, and convenient connections between primary building entrances and all adjacent parking areas, transit stops, recreational areas/playgrounds, and public rights-of-way based on all of the following criteria:*

- i. The walkway is reasonably direct. A walkway is reasonably direct when it follows a route that does not deviate unnecessarily from a straight line or it does not involve a significant amount of out-of-direction travel;*

Response: All walkways, proposed and existing, provide direct access with minimal to no deviation from destinations and no out-of-direction travel.

- ii. The walkway is designed primarily for pedestrian safety and convenience, meaning it is reasonably free from hazards and provides a reasonably smooth and consistent surface and direct route of travel between destinations. The city planning commission may require landscape buffering between walkways and adjacent parking lots or driveways to mitigate safety concerns.*

Response: All new walkways are proposed to be smoothly graded and ADA accessible with direct routes of travel from parking areas to building entries. Where possible, at the west parking area, portions of the walkways have landscape buffering.

- iii. The walkway network connects to all primary building entrances and, where required, Americans With Disabilities Act requirements.*

Response: Both primary walkways in the project connect their respective parking lots with the nearest building entrances. ADA parking is proposed to be regraded to meet current code requirements and connected to the building's

primary ADA entries on the east elevation. The west walkways are also proposed to be accessible as are the entries along the west elevation however they are not connected directly to the ADA parking which is concentrated, appropriately, in the east parking lot.

- c. *Vehicle/Walkway Separation. Except as required for crosswalks, pursuant to Subsection 4, below, where a walkway abuts a driveway or street it shall be raised 6 inches and curbed along the edge of the driveway/street. Alternatively, the city planning commission may approve a walkway abutting a driveway at the same grade as the driveway if the walkway is physically separated from all vehicle-maneuvering areas. An example of such separation is a row of bollards (designed for use in parking areas) with adequate minimum spacing between them to prevent vehicles from entering the walkway.*

Response: The proposed walkways west of the building are all raised and curbed. The existing walkway east of the building between the building and east parking lot is generally flush with the adjacent asphalt. Wheel stops are proposed at each parking stall to separate the walkway from vehicles. Raising the walkway here is not feasible in order to maintain accessible primary building entries at each tenant space.

- d. *Crosswalks. Where a walkway crosses a parking area or driveway ("crosswalk"), it shall be clearly marked with contrasting paving materials (e.g., pavers, light-color concrete inlay between asphalt, or similar contrast). The crosswalk may be part of a speed table to improve driver-visibility of pedestrians. Painted or thermo-plastic striping and similar types of non-permanent applications are discouraged, but may be approved for lesser used crosswalks not exceeding 20 feet in length.*

Response: The only proposed crosswalk in the project, from the west parking lot to the building's west entrances crosses the one-way drive thru lane. The crosswalk is part of a speed table to improve driver visibility. The crosswalk itself and the sloped portions of the speed table are concrete, contrasting from the adjacent asphalt pavement.

- e. *Walkway Width and Surface. Walkways, including access ways required for subdivisions pursuant with Chapter 17.150, shall be constructed of concrete, asphalt, brick/masonry pavers, or other durable surface, as approved by the city engineer, and not less than 5 feet wide. Multi-use paths (i.e., designed for shared use by bicyclists and pedestrians) shall be concrete or asphalt and shall conform to the public works design standards.*

Response: All proposed walkways are concrete. See below for further explanation of width.

- f. *Walkway Construction. Walkway surfaces may be concrete, asphalt, brick/masonry pavers, or other city-approved durable surface meeting Americans with Disabilities Act requirements. Walkways shall be not less than 5 feet in width, except that concrete walkways a minimum of 6 feet in width are required in commercial developments and where access ways are required for subdivisions under Chapter 17.150 the planning commission may also require 6 foot wide, or wider, concrete sidewalks in other developments where pedestrian traffic warrants walkways wider than 5 feet.*

Response: All walkways proposed in the project are concrete. The new walkway proposed between the west parking and west elevation building entries is 6 feet wide. The existing concrete walkway between the buildings main entry and the east parking lot is approximately 5'-10" wide and the new portion of sidewalk proposed adjacent matches this width. The walkway between the building and the west property line of Lot 100 ranges from 3'-8" to 4'-6" which is the maximum possible width given the buildings existing relationship to the property line.

PROJECT

VARSITY GRILL
REMODEL

52001 Columbia River Hwy
Scappoose, OR 97056

DRAWING TITLE
DEMOLITION
PLAN

REVISIONS

NOT FOR
CONSTRUCTION

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DATE January 17, 2020

JOB NO. 19.04

DRAWING NO.

C1.0

Page 55 of 109
SITE DEV REVIEW

SHEET NOTES

1. CONTRACTOR MAY STAGE WITHIN LIMITS OF DEMOLITION.
2. REMOVE ALL SITE COMPONENTS AND RECYCLE COMPONENTS AS REQUIRED IN THE SPECIFICATIONS.
3. GENERAL DEMOLITION PERMIT SHALL BE SECURED BY THE CONTRACTOR.
4. ALL TRADE LICENSES AND PERMITS NECESSARY FOR THE PROCUREMENT AND COMPLETION OF THE WORK SHALL BE SECURED BY THE CONTRACTOR PRIOR TO COMMENCING DEMOLITION.
5. THE CONTRACTOR SHALL PRESERVE AND PROTECT FROM DAMAGE ALL EXISTING RIGHT-OF-WAY SURVEY MONUMENTATION DURING DEMOLITION. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING AND PAYING FOR THE REPLACEMENT BY A LICENSED SURVEYOR OF ANY DAMAGED OR REMOVED MONUMENTS.
6. PROTECT ALL ITEMS ON ADJACENT PROPERTIES AND IN THE RIGHT OF WAY INCLUDING BUT NOT LIMITED TO SIGNAL EQUIPMENT, PARKING METERS, SIDEWALKS, STREET TREES, STREET LIGHTS, CURBS, PAVEMENT AND SIGNS. CONTRACTOR SHALL BE RESPONSIBLE FOR RESTORING ANY DAMAGED ITEMS TO ORIGINAL CONDITION.
7. PROTECT STRUCTURES, UTILITIES, SIDEWALKS, AND OTHER FACILITIES IMMEDIATELY ADJACENT TO EXCAVATIONS FROM DAMAGES CAUSED BY SETTLEMENT, LATERAL MOVEMENT, UNDERMINING, WASHOUT AND OTHER HAZARDS.
8. SAWCUT STRAIGHT LINES IN SIDEWALK, AS NECESSARY.
9. CONTRACTOR IS RESPONSIBLE TO CONTROL DUST AND MUD DURING THE DEMOLITION PERIOD, AND DURING TRANSPORTATION OF DEMOLITION DEBRIS, ALL STREET SURFACES OUTSIDE THE CONSTRUCTION ZONE MUST BE KEPT CLEAN.

DEMOLITION KEY NOTES

- 20 SAWCUT LINE
- 21a REMOVE CONCRETE CURB.
- 21b REMOVE CONCRETE CURB AND GUTTER.
- 22 REMOVE CONCRETE SIDEWALK
- 23 REMOVE ASPHALT PAVEMENT AND CRUSHED ROCK SUBGRADE.
- 24 REMOVE WHEEL STOP
- 25 REMOVE CONCRETE DRIVEWAY.
- 26 REMOVE EXISTING FENCE
- 27 REMOVE EXISTING STORM ROOF DRAIN
- 28 REMOVE CATCH BASIN
- 29 REMOVE EXISTING STORM PIPE
- 30 REMOVE EXISTING WATER METER AND LINE

PROTECTION KEY NOTES

- 40 PROTECT CURB AND SIDEWALK.
- 41 PROTECT ELECTRICAL VAULT.
- 42 PROTECT EXISTING UTILITIES.
- 43 PROTECT EXISTING UTILITY STRUCTURE.
- 44 PROTECT MONUMENTATION.
- 45 PROTECT EXISTING CATCH BASIN

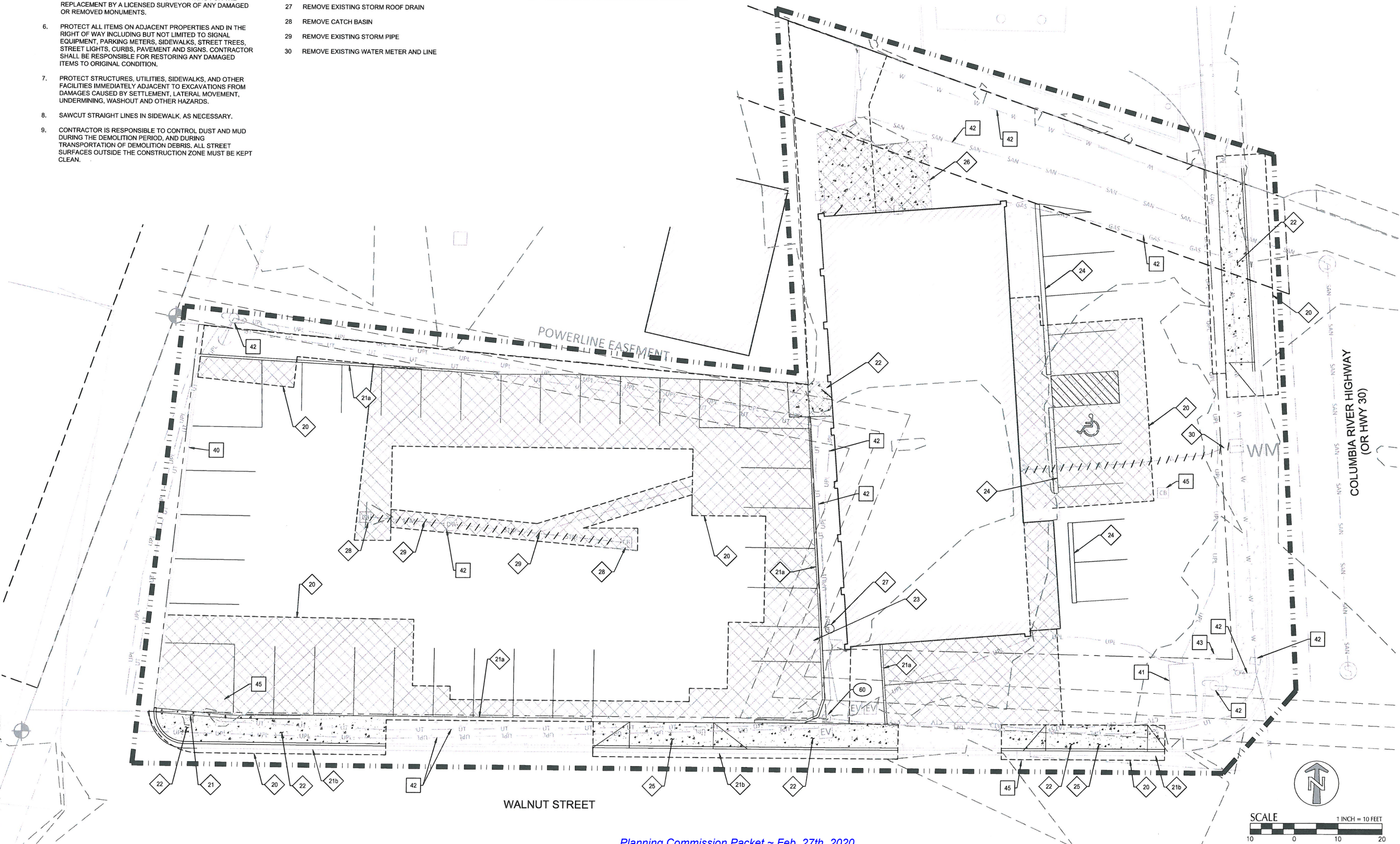
SALVAGE KEY NOTES

- 60 UTILITY VAULT TO BE RELOCATED BY OTHERS

SHEET LEGEND

- PROPERTY LINE
- DEMOLITION/WORK LIMITS (SHOWN OFFSET FOR CLARITY)
- SAWCUT LINE
- REMOVE OR ABANDON UTILITY LINE IN PLACE
- EXISTING GRADE CONTOUR
- REMOVE CONCRETE OR AC PAVEMENT

Files: \\civildocs\Civil-Projects\c\p\2019\900318-Varsity-Grill-Redevelopment\CAD\LOT\19318-10-DEMO.dwg TAB:C1.0
Plotted: 1/16/20 at 11:02am By: Sium



File: \\civildocs\Civil-Projects\c\p\2019\1900318-Varsity-Grill-Redevelopment\CAD\LOT\19318-20-SITE&PAVING.dwg TAB: C2.0
Plotted: 1/16/20 at 1:26pm By: S.Lam

SHEET NOTES

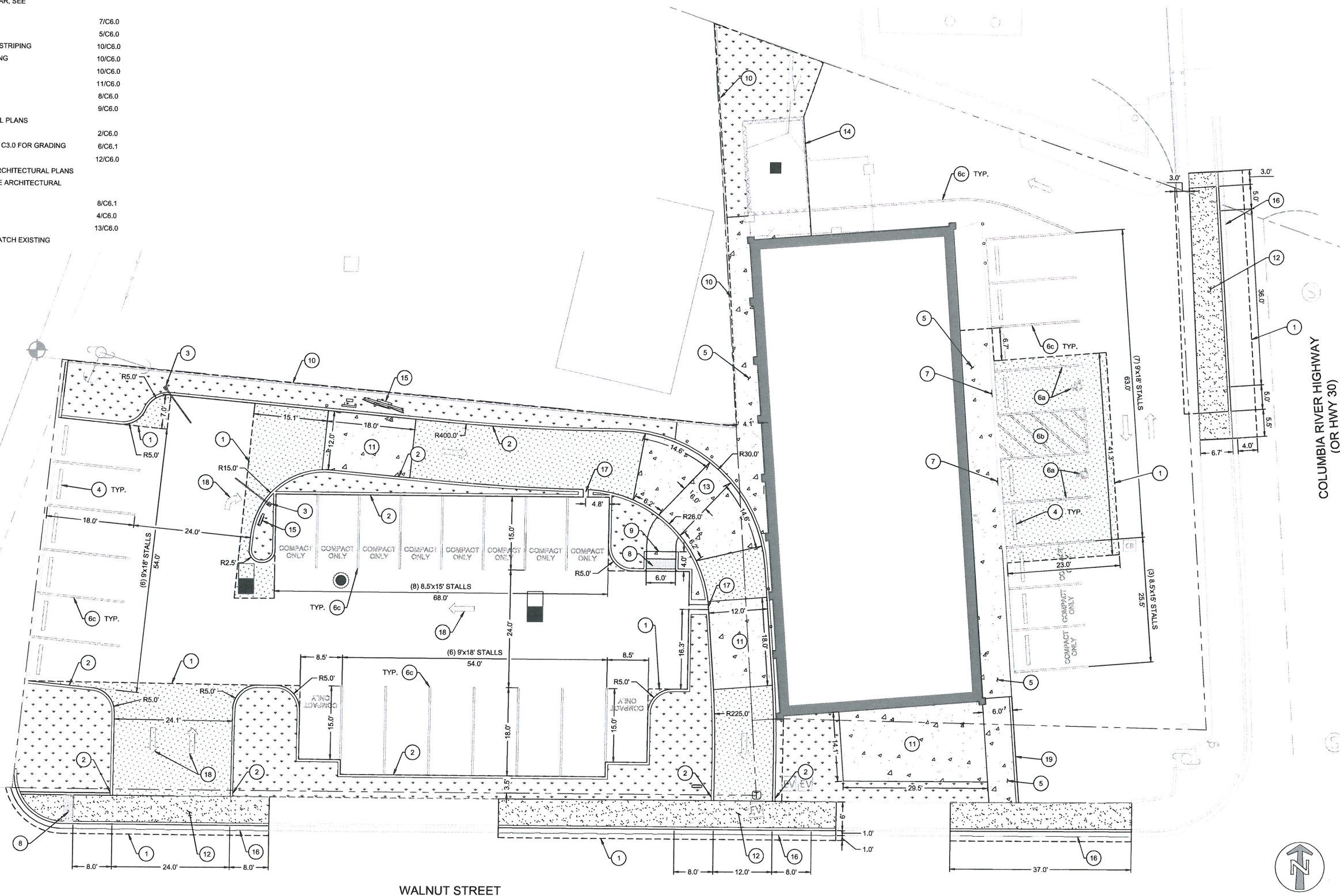
1. ALL DIMENSIONS ARE TO FACE OF CURB OR FACE OF WALL.
2. ALL SIDEWALK PAVEMENT JOINTS SHALL BE CONSTRUCTED PER DETAIL 6/C6.0.
3. PROPOSED FRONTAGE IMPROVEMENTS IN RIGHT-OF-WAY SHALL BE CONSTRUCTED PER CITY STANDARDS.

KEY NOTES

#	DESCRIPTION	DETAIL REF.
1	SAWCUT LINE	
2	STANDARD CURB	3/C6.0
3	DRIVE THRU CLEARANCE BAR, SEE ARCHITECTURAL PLANS	
4	WHEEL STOP	7/C6.0
5	CONCRETE SIDEWALK	5/C6.0
6a	ADA PARKING STALLS AND STRIPING	10/C6.0
6b	'NO PARKING' ZONE STRIPING	10/C6.0
6c	4" WIDE WHITE STRIPE	10/C6.0
7	ADA PARKING SIGN	11/C6.0
8	DETECTABLE WARNING	8/C6.0
9	CURB RAMP	9/C6.0
10	FENCE PER ARCHITECTURAL PLANS	
11	CONCRETE PAD	2/C6.0
12	CONCRETE DRIVEWAY, SEE C3.0 FOR GRADING	6/C6.1
13	TABLE TOP CROSSWALK	12/C6.0
14	TRASH ENCLOSURE, SEE ARCHITECTURAL PLANS	
15	DRIVE THROUGH SIGNS, SEE ARCHITECTURAL PLANS	
16	CURB AND GUTTER	8/C6.1
17	CURB CUT	4/C6.0
18	STRIPING	13/C6.0
19	INSTALL FLUSH CURB TO MATCH EXISTING	

SHEET LEGEND

---	PROPERTY LINE	
[Pattern]	OFF-SITE CONCRETE SIDEWALK	7/C6.1
[Pattern]	ON-SITE CONCRETE SIDEWALK	5/C6.0
[Pattern]	STANDARD ASPHALT PAVEMENT	1/C6.0
[Pattern]	LANDSCAPING, SEE LANDSCAPE PLANS	



111 SW Fifth Ave., Suite 2600
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WILLIAM KAVEN

Ex. 36

WILLIAM KAVEN ARCHITECTURE
4080 N. WILLIAMS AVE. STUDIO 100
PORTLAND, OR 97227
www.williamkaven.com

PROJECT

VARSITY GRILL REMODEL

52001 Columbia River Hwy
Scappoose, OR 97056

DRAWING TITLE SITE AND PAVING PLAN

REVISIONS

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Page 56 of 109
SITE DEV REVIEW



SHEET NOTES

SLOPES PROVIDED ON SLOPE ARROW ARE FOR REFERENCE ONLY.

LANDINGS ON ACCESSIBLE ROUTES SHALL NOT EXCEED 2% IN ANY DIRECTION.

ALL ACCESSIBLE ROUTES SHALL COMPLY WITH CURRENT ADA ACCESSIBILITY GUIDELINES FOR BUILDING AND FACILITIES (ADAAG).

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Ex. 3c

PROJECT

VARSITY GRILL REMODEL

DRAWING TITLE

**GRADING AND
DRAINAGE PLAN**

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C3.0
Page 57 of 109
SITE DEV REVIEW



SCALE 1 INCH = 10 FEET

1. ON-SITE PIPE BEDDING AND BACKFILL FOR ALL UTILITIES SHALL BE DONE PER DETAIL 1/C6.1.
2. STRUCTURES LOCATIONS ARE BASED ON CENTER OF STRUCTURE.
3. CONTRACTOR TO INCLUDE ALL NECESSARY FITTINGS AS REQUIRED.

STRUCTURE LABEL

UTILITY TYPE (SD=STORM DRAINAGE, S=SANITARY SEWER, W=WATER, FP=FIRE PROTECTION)

STRUCTURE TYPE CALLOUT

ID NUMBER (WHERE APPLICABLE)

LOCATION (WHERE APPLICABLE)

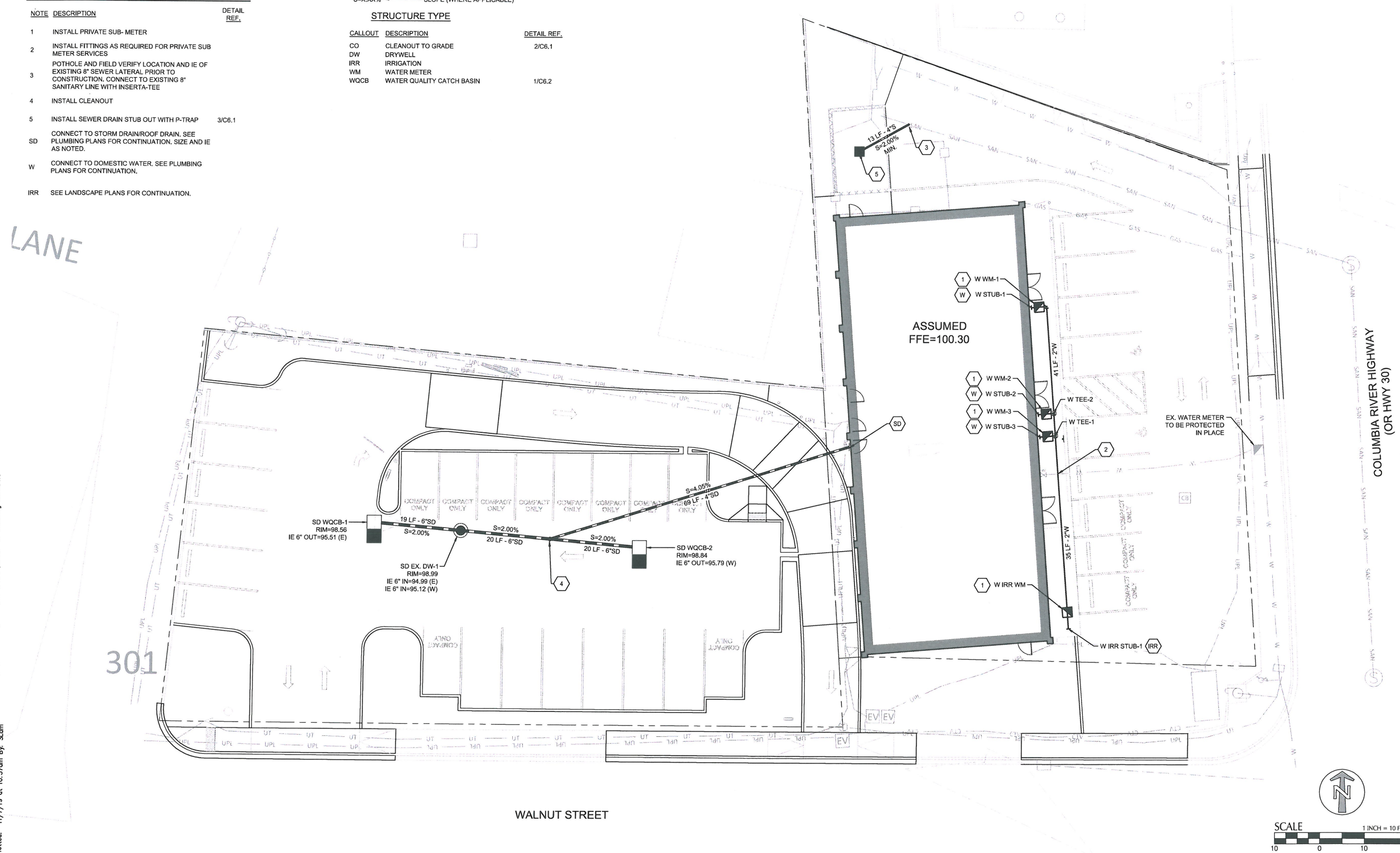
STRUCTURE INFO (WHERE APPLICABLE)

EXAMPLE:
XX-XXX-XX
ID=XXXXXX
LOC=XXXXXX
IM=
IN = XX.X
OUT = XX.X

UTILITY LENGTH
UTILITY SIZE
XXLF - XX" XX ← UTILITY TYPE
S=X.XX% ← SLOPE (WHERE APPLICABLE)

<u>CALLOUT</u>	<u>DESCRIPTION</u>	<u>DETAIL REF.</u>
CO	CLEANOUT TO GRADE	2/C6.1
DW	DRYWELL	
IRR	IRRIGATION	
WM	WATER METER	
WQCB	WATER QUALITY CATCH BASIN	1/C6.2

<u>NOTE</u>	<u>DESCRIPTION</u>	<u>Detailed REF.</u>
1	INSTALL PRIVATE SUB- METER	
2	INSTALL FITTINGS AS REQUIRED FOR PRIVATE SUB METER SERVICES	
3	POTHOLE AND FIELD VERIFICATION LOCATION AND IE OF EXISTING P- SEWER LATERAL PRIOR TO CONSTRUCTION. CONNECT TO EXISTING 8" SANITARY LINE WITH INSERTA-TEE	
4	INSTALL CLEANOUT	
5	INSTALL SEWER DRAIN STUB OUT WITH P-TRAP	3/C6.1
SD	CONNECT TO STORM DRAIN/ROOF DRAIN. SEE PLUMBING PLANS FOR CONTINUATION. SIZE AND IE AS NOTED.	
W	CONNECT TO DOMESTIC WATER. SEE PLUMBING PLANS FOR CONTINUATION.	
IRR	SEE LANDSCAPE PLANS FOR CONTINUATION.	



Ex 3e

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PROJECT

VARSITY GRILL REMODEL

52001 Columbia River Hwy
 Scappoose, OR 97056

DRAWING TITLE
**EROSION AND
 SEDIMENT
 CONTROL PLAN**

REVISIONS

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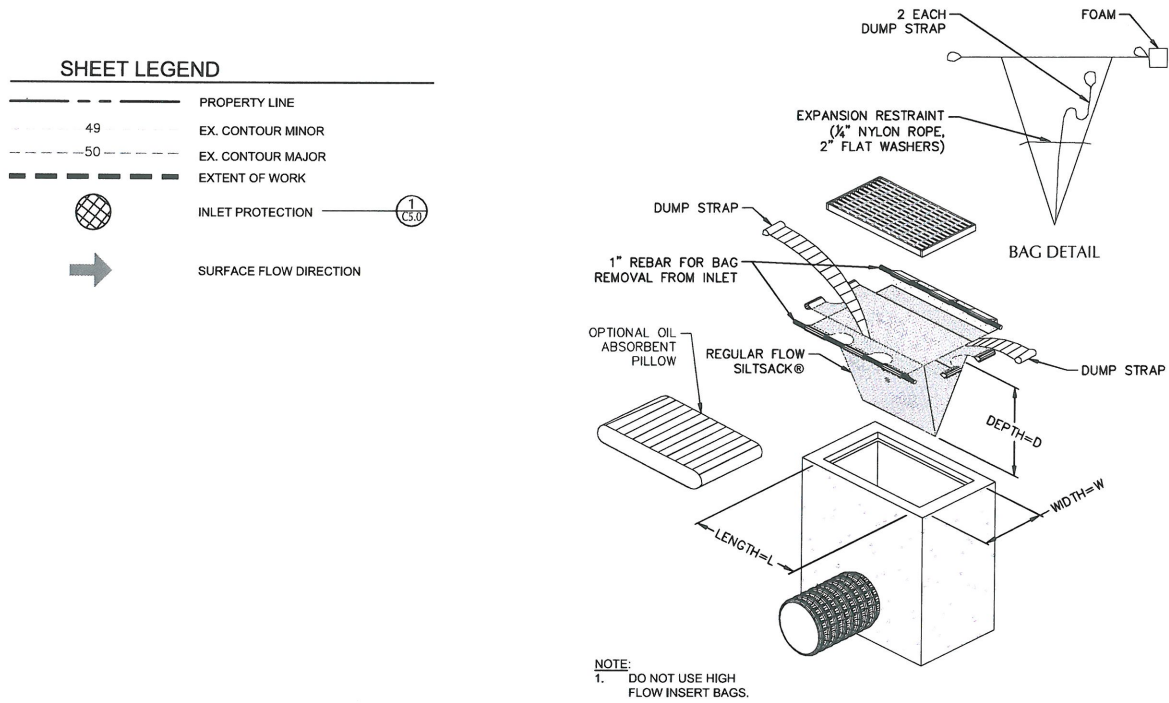
DATE January 17, 2020

JOB NO. 19.04

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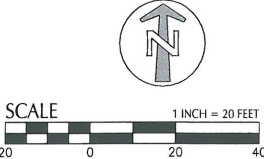
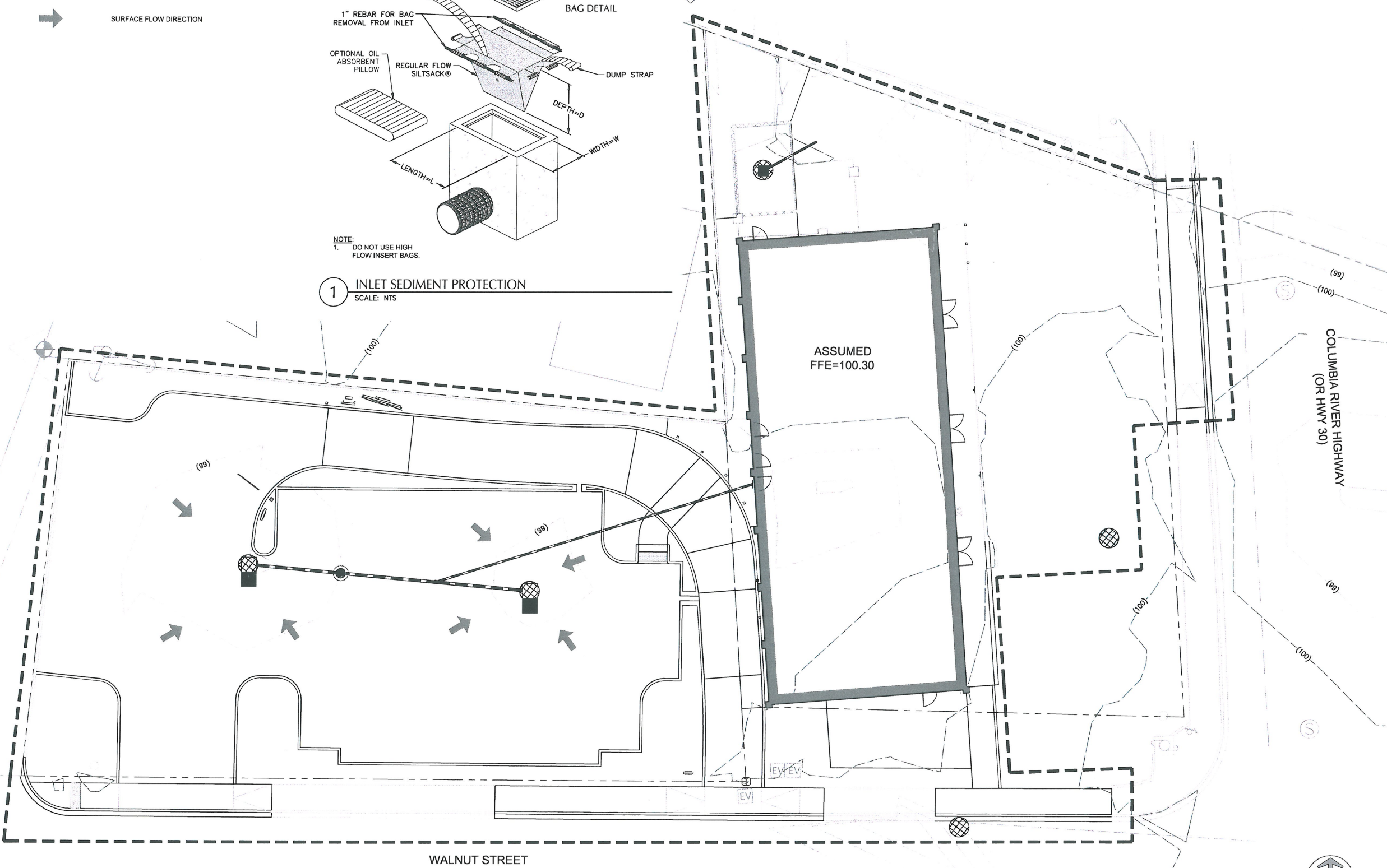
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Page 59 of 109
 SITE DEV REVIEW
















NOTE:
 1. DO NOT USE HIGH FLOW INSERT BAGS.

1 INLET SEDIMENT PROTECTION
 SCALE: NTS



PLANT SCHEDULE

TREES									
TREES	CODE	BOTANICAL / COMMON NAME	CONT	CAL	SIZE				
	AC	ACER BUERGERIANUM TRIDENT MAPLE MINIMUM 10' HEIGHT AT TIME OF PLANTING ALL TREES OVERHANGING ANY STREET OR R.O.W. TO BE MAINTAINED AT A MIN. OF 8' ABOVE TOP OF STREET SURFACE OR SIDEWALK SURFACE	B & B	2" CAL	12-15' H		YURE	YUCCA RECURVIFOLIA SOFT LEAF YUCCA	5 GAL
	LI	LIRIODENDRON TULIPIFERA 'ARNOLD' ARNOLD TULIP POPLAR LIMBED-UP 8'	B & B	2" CAL	12-15' H		MSI	MISCANTHUS SINENSIS 'YAKUJIMA' YAKUJIMA DWARF MAIDEN GRASS	1 GAL
SHRUBS									
SHRUBS	CODE	BOTANICAL / COMMON NAME	SIZE	HT.					
	BETH	BERBERIS THUNBERGII SUNJOY CITRUS SUNJOY CITRUS BARBERRY	1 GAL				EQU	EQUISETUM HYEMALE HORSETAIL REED GRASS	1 GAL
	EUJG	EUONYMUS JAPONICUS 'GREENSPIRE' GREENSPIRE UPRIGHT EUONYMUS TO BE MIN. 4' HEIGHT AT TIME OF PLANTING	15 GAL	4'			CACO	CAREX COMANS 'FROSTED CURLS' FROSTED CURLS SEDGE	1 GAL
	LIJA	LIGULSTRUM JAPONICUM 'TEXANUM' WAX LEAF PRIVET TO BE MIN. 4' HEIGHT AT TIME OF PLANTING	25 GAL	4'			THCI	THYMUS X CITRIODORUS 'AUREUS' LEMON THYME	4" POT
	LOPI	LONICERA PILEATA PRIVET HONEYSUCKLE	1 GAL						
	NADO	NANDINA DOMESTICA 'LEMON LIME' LEMON LIME NANDINA TO BE MIN. 2' IN HEIGHT AT TIME OF PLANTING, MAINTAINED HEIGHT NOT TO EXCEED 3'	2 GAL						
	ROOF	ROSMARINUS OFFICINALIS 'ROMAN BEAUTY' TM CHEF'S CHOICE ROSEMARY	3 GAL						



WILLIAM KAVEN

Ex. 3f

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PORTLAND, OR 97227
www.wkarch.com

PROJECT

VARSITY GRILL REMODEL

52001 Columbia River Hwy
Scappoose, OR 97056

DRAWING TITLE
LANDSCAPE
PLAN

REVISIONS

SHAPIRO / DIDWAY
1204 SE Water Ave
Portland, Oregon 97214
t. 503.232.2520
www.shaprodidway.com

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CONSTRUCTION

DATE: February 14, 2020
JOB NO.: 19-04

DRAWING NO.:

L1.00

Page 60 of 109
SITE DEV REVIEW

01 LANDSCAPE PLAN

Planning Commission Packet ~ Feb. 27th, 2020

0 8 16 24 32 feet
1/8" = 1'-0"

VARSITY GRILL REMODEL

52001 Columbia River Hwy
Scappoose, OR 97056

DRAWING TITLE
SITE PLAN

REVISIONS

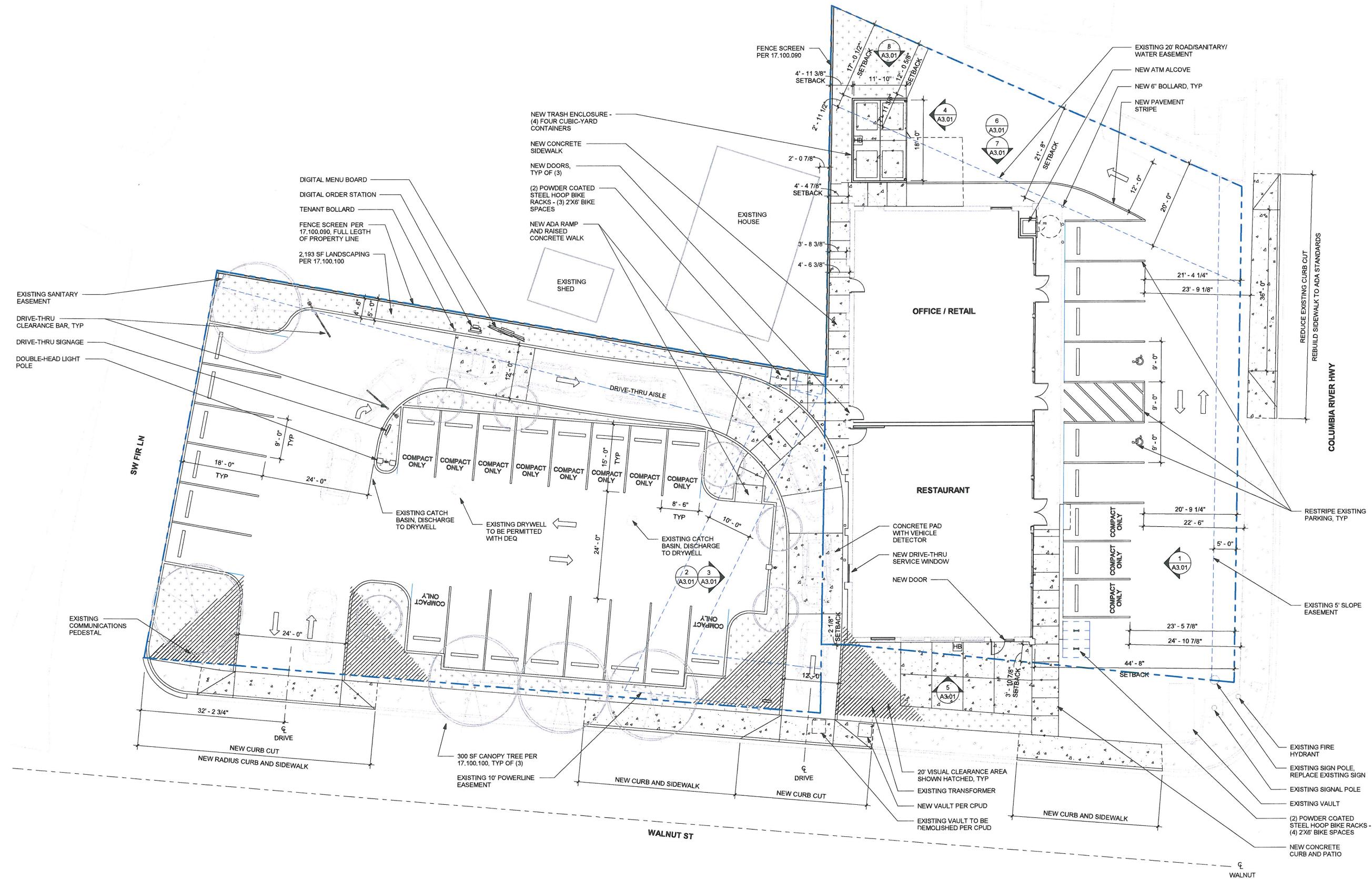
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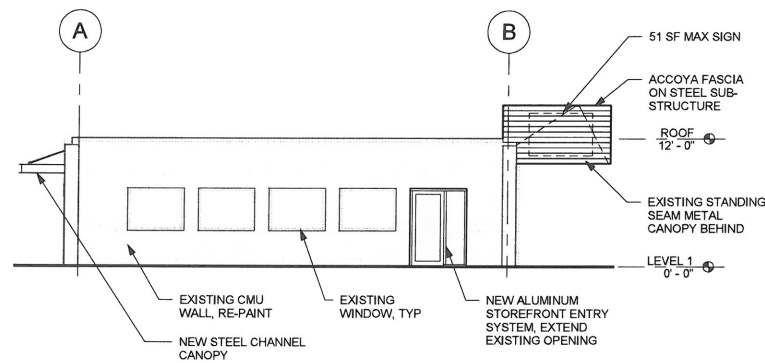
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JOB NO. 19.04

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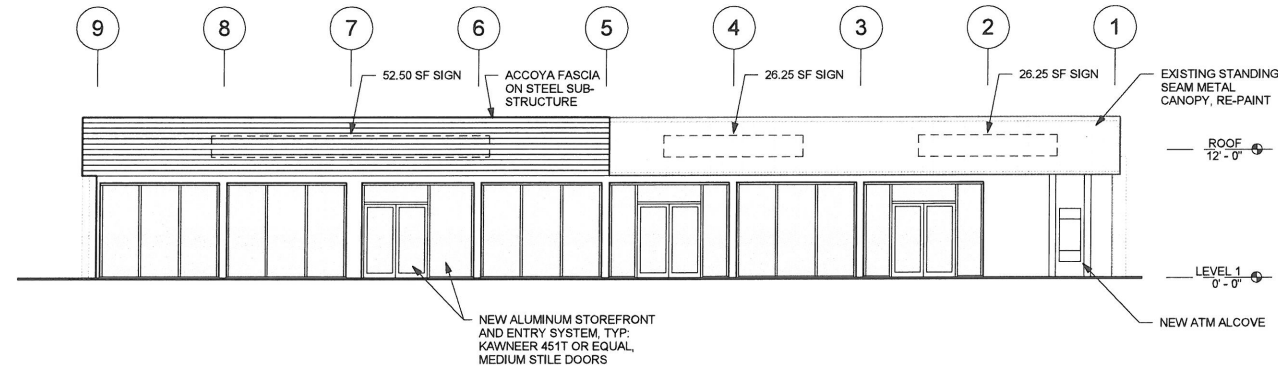
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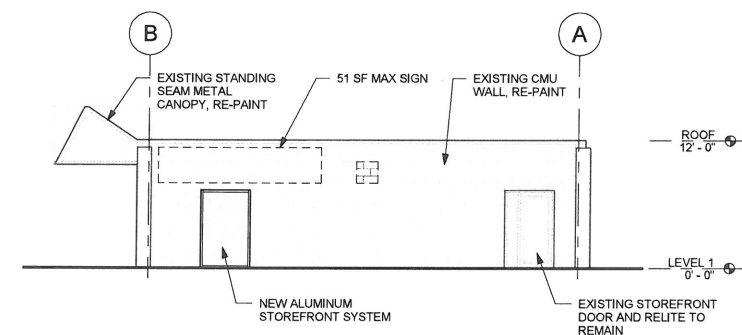
01 SITE PLAN
1" = 10'-0"



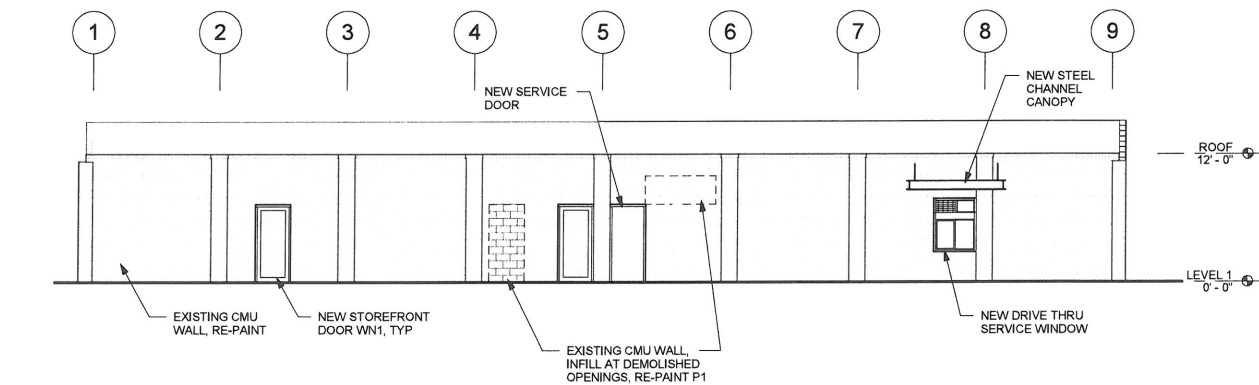
05 SOUTH ELEVATION
1/8" = 1'-0"



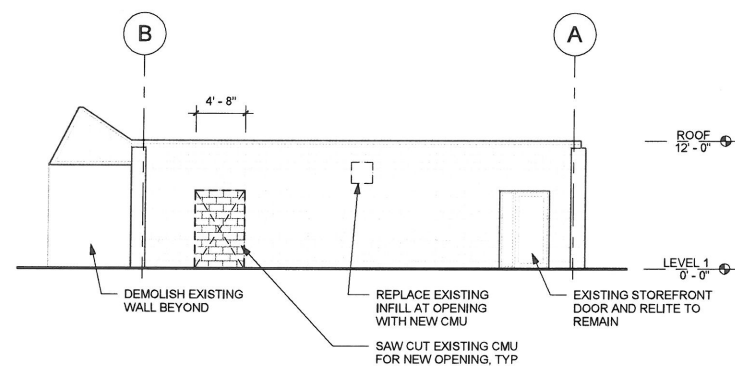
01 EAST ELEVATION
1/8" = 1'-0"



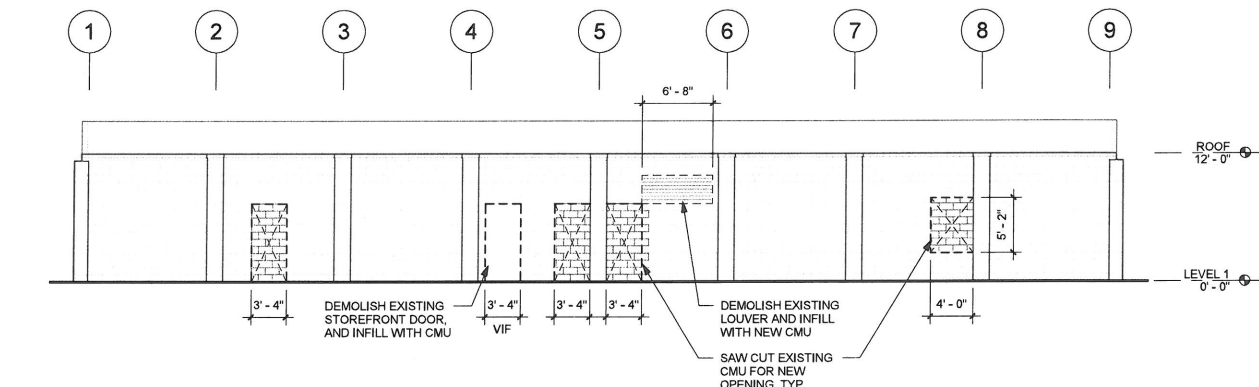
06 NORTH ELEVATION
1/8" = 1'-0"



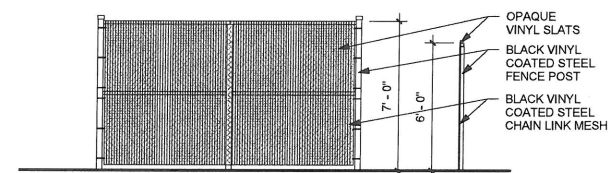
02 WEST ELEVATION
1/8" = 1'-0"



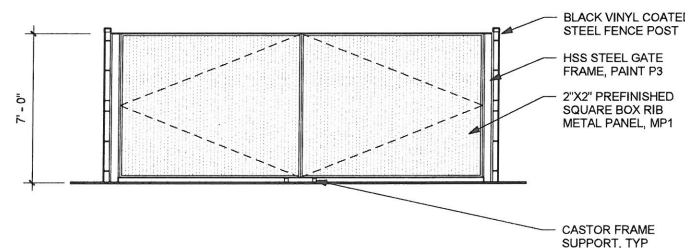
07 NORTH ELEVATION - DEMO
1/8" = 1'-0"



03 WEST ELEVATION - DEMO
1/8" = 1'-0"



08 TRASH ENCLOSURE - NORTH ELEVATION
1/4" = 1'-0"



04 TRASH ENCLOSURE - EAST ELEVATION
1/4" = 1'-0"

Planning Commission Packet ~ Feb. 27th, 2020

GENERAL NOTES

1. THESE DRAWINGS ARE DIAGRAMMATIC IN NATURE. DO NOT SCALE. FOLLOW DIMENSIONS SHOWN ON DRAWINGS AND FIELD VERIFY EXISTING CONDITIONS.
2. SEE ALL PLAN, ELEVATION, SECTION AND DETAIL SHEETS FOR ADDITIONAL INFORMATION.
3. ALL GATES AND RAILINGS TO BE DESIGN BUILD. INFORMATION INCLUDED FOR DESIGN INTENT ONLY. ENGINEERING TO BE PROVIDED ON ALL REQUIRED ASSEMBLIES.

LEGEND

- (11) WINDOW TAG

WILLIAM KAVEN

Ex 3h

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PROJECT

VARSITY GRILL REMODEL

52001 Columbia River Hwy
Scappoose, OR 97056

DRAWING TITLE
EXTERIOR
ELEVATIONS

REVISIONS

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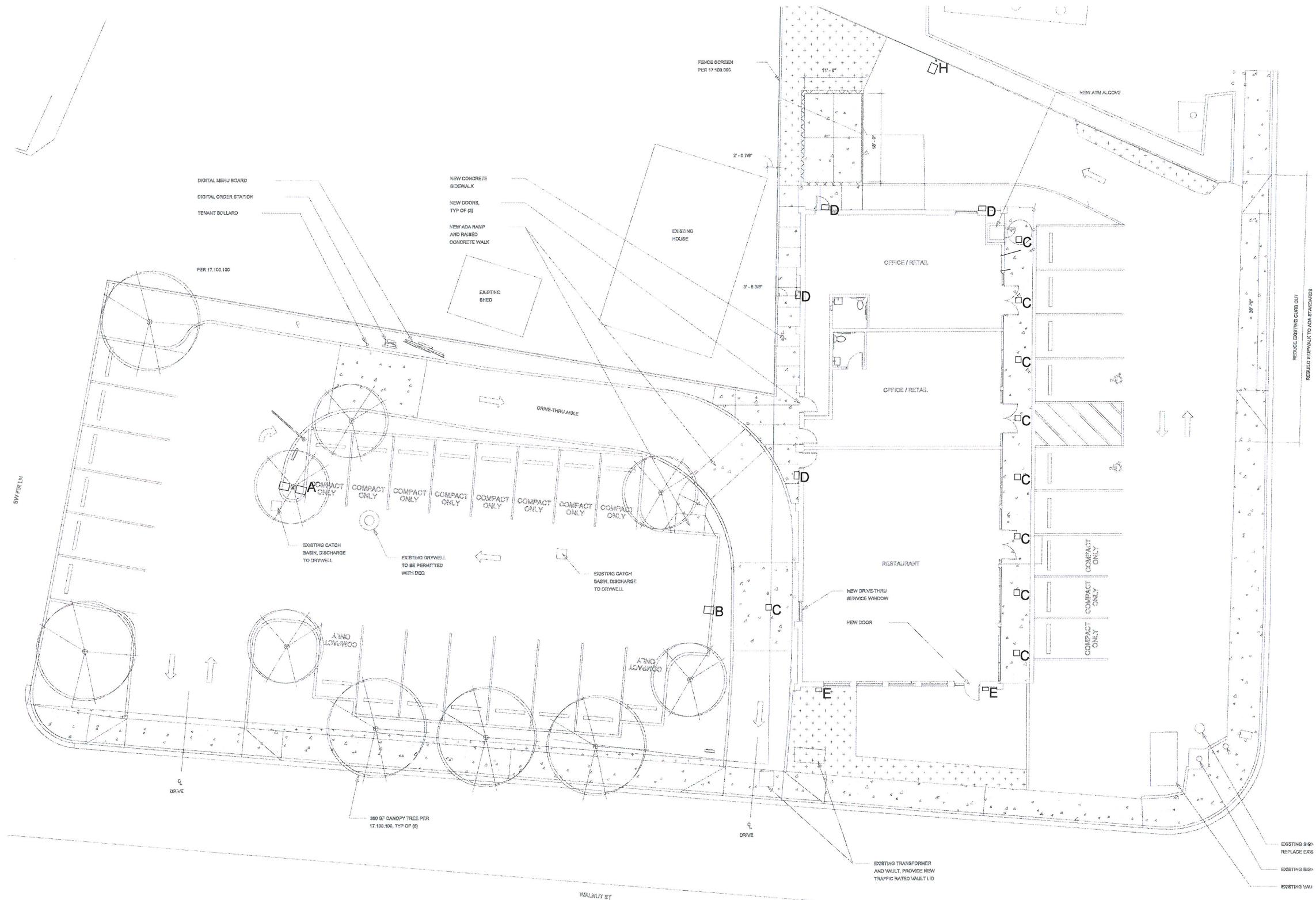
Page 62 of 109

SDR

N:\Document Services (CAD)\01-814-Documents\Portland Office\Varsity Grill\05 - CAD\REP\Photometric\1-21-20.dwg, 1/21/2020 1:57:43 PM, DWG To PDF.pc3

Schedule							
Label	Quantity	Manufacturer	Catalog Number	Description	Lamp	Number Lamps	Wattage
A	1	Lithonia Lighting	RSX1 LED P3 40K R55	RSX Area Fixture Size 1 P3 Lumen Package 4000K CCT Type R55 Distribution	LED	1	14778
D	4	Lithonia Lighting	TWR1 LED P2 40K	Outdoor Wallpack approx. 3500lm	LED	1	3449
E	2	Lithonia Lighting		BUILDING MOUNTED WALLPACK WITH VERTICAL LAMP AND GLASS LENS	LED	1	1710
B	1	Lithonia Lighting	RSX1 LED P3 40K R55	RSX Area Fixture Size 1 P3 Lumen Package 4000K CCT Type R55 Distribution	LED	1	14778
C	9	Cyber Tech Lighting	C1475Q-DISK/BZ	Flush Mount LED Ceiling Fixture	LED	1	720
H	1	Lithonia Lighting	RSX1 LED P3 40K R4 HS	RSX Area Fixture Size 1 P3 Lumen Package 4000K CCT Type R4 Distribution with HS Shield	LED	1	10322

3 SITE FIXTURE SCHEDULE
SCALE: N.T.S.



1 SITE PHOTOMETRIC
SCALE: 1/8"=1'-0"

Statistics			
Description	Avg	Max	Min
Calc Zone #1	2.7 fc	9.0 fc	0.0 fc

2 STATISTICS
SCALE: N.T.S.

Ex. 3i

The Stoner Electric Group

Stoner ELECTRIC, INC.
Stoner PROTECTIVE SYSTEMS
Stoner CONTROLS
Stoner LIGHTING SERVICES
Stoner TECHNOLOGY SERVICES
Stoner ELECTRIC, INC. VANCOUVER

1904 SE Ochoco
Milwaukie, Oregon 97222
(503)462-6500
FAX (503)659-4968
www.stonergroup.com

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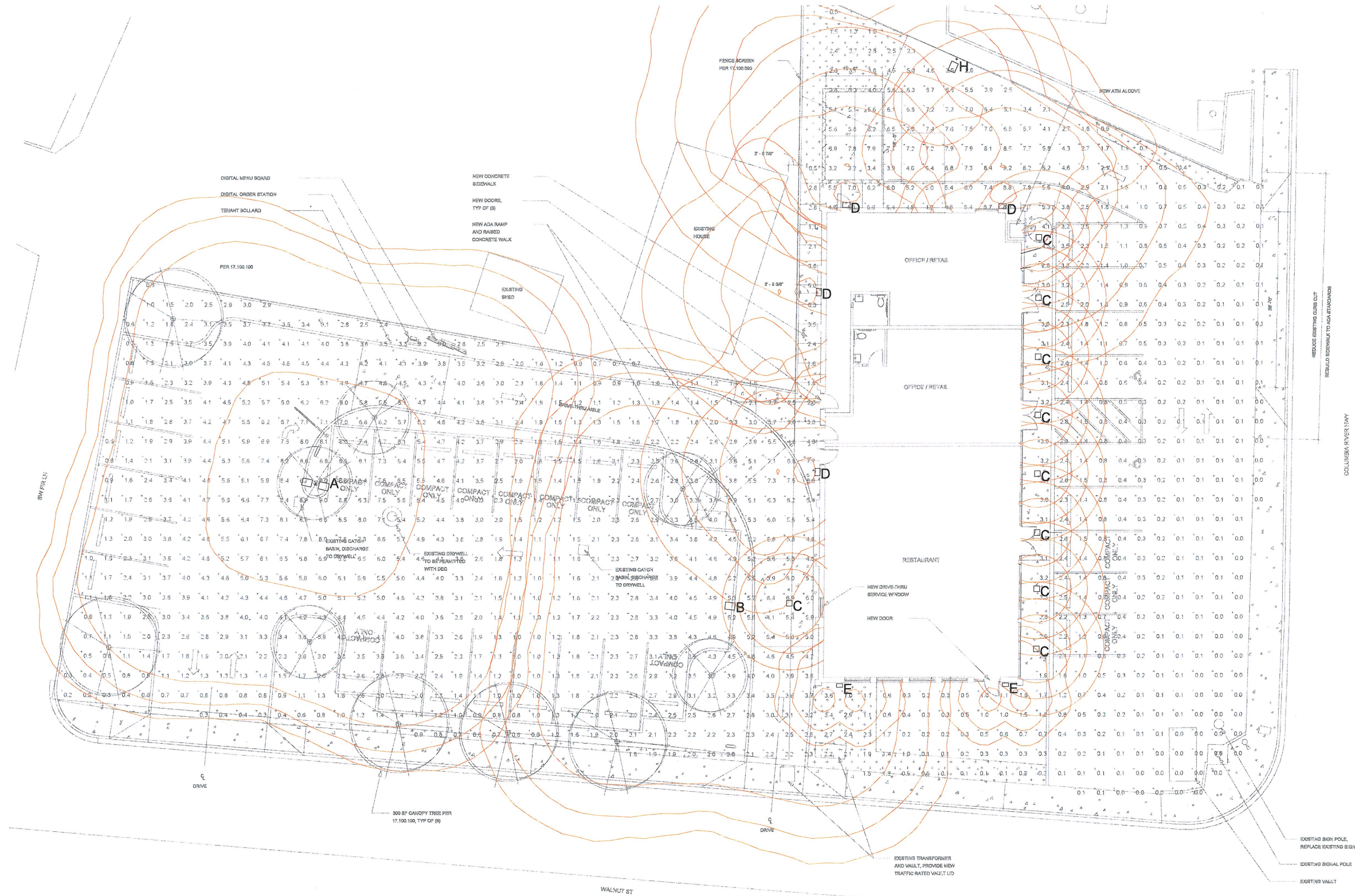
REVISIONS

Scale	AS NOTED
Date	1/21/2020
Project No.	XXXXX-1
Drawn By	DDW
Checked By	J.RAY

VARSITY GRILL
52001 COLUMBIA RIVER HIGHWAY
SCAPPOOSE, OREGON 97056

SHEET TITLE
SITE LIGHTING

SHEET NO.
E
0.03



Schedule						
Label	Quantity	Manufacturer	Catalog Number	Description	Lamp	Number Lamps
A	1	Lithonia Lighting	RSX1 LED P3 40K RSS	RSX Area Fixture Size 1 P3 Lumen Package 4000K CCT Type RSS Distribution	LED	1
D	4	Lithonia Lighting	TWR1 LED P2 40K	Outdoor Wallpack approx. 3500lm	LED	1
E	2	Lithonia Lighting		BUILDING MOUNTED WALLPACK WITH VERTICAL LAMP AND GLASS LENS	LED	1
B	1	Lithonia Lighting	RSX1 LED P3 40K RSS	RSX Area Fixture Size 1 P3 Lumen Package 4000K CCT Type RSS Distribution	LED	1
C	9	Cyber Tech Lighting	C1475Q-DISK/BZ	Flush Mount LED Ceiling Fixture	LED	1
H	1	Lithonia Lighting	RSX1 LED P3 40K R4 HS	RSX Area Fixture Size 1 P3 Lumen Package 4000K CCT Type R4 Distribution with HS Shield	LED	1
					Lumens Per Lamp	Wattage
					14778	218.88
					3449	28.2
					1710	18
					14778	109.44
					720	14
					10322	109.442

3 SITE FIXTURE SCHEDULE
SCALE: N.T.S.

1 SITE PHOTOMETRIC
SCALE: 1/8"=1'-0"

Statistics			
Description	Avg	Max	Min
Calc Zone #1	2.7 fc	9.0 fc	0.0 fc

2 STATISTICS
SCALE: N.T.S.

Ex. 3j

The
Stoner
Electric
Group

Stoner ELECTRIC, INC.
Stoner PROTECTIVE SYSTEMS
Stoner CONTROLS
Stoner LIGHTING SERVICES
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Stoner ELECTRIC, INC.
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Milwaukie, Oregon 97222
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www.stonergroup.com

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REVISIONS

Scale
AS NOTED

Date
1/21/2020

PROJECT NO.
XXXXXX.1

Drawn By
DDW

Checked By
J.RAY

VARSITY GRILL

52001 COLUMBIA RIVER HIGHWAY
SCAPOOSE, OREGON 97056

SHEET TITLE
SITE
PHOTOMETRIC

SHEET NO.

E
0.04

ALTA/NSPS LAND TITLE SURVEY

LOCATED IN THE NW 1/4 SECTION 13
TOWNSHIP 3 NORTH, RANGE 2 WEST, WILLAMETTE MERIDIAN.,
51001 COLUMBIA RIVER HIGHWAY
CITY OF SCAPPOOSE, COLUMBIA COUNTY, OREGON
OCTOBER 2019

ALTA RESEARCH:

THIS ALTA SURVEY WAS BASED ON LEGAL DESCRIPTIONS OF SALE IN A CHAIN OF TITLE SEARCH BY TICOR TITLE. THE PROPERTY RESEARCHED WAS LOT 300, TAX MAP 3N2W13-BA. THE LEGAL DESCRIPTION FOR THIS LOT INCLUDE A MUCH LARGER PARCEL THAN WHAT ACTUALLY IS TO BE SOLD AS LOT 300. THE LEGAL HAS EXCEPTIONS OF ROAD AND PROPERTY TO THE SOUTH OF THE ROAD DEDICATION THAT WERE SUBTRACTED FROM THE PROPERTY.

THE TRANSFERS OF LAND AND RECORDED EASEMENTS RESEARCHED INCLUDED:

- BOOK 210 PAGE 336, 337, 338 (02/25/1977)
- BOOK 214 PAGE 778,779,780 (11/08/1977)
- TICOR TITLE ESCROW 07-07077 (30/24/1995)
- TICOR TITLE ESCROW 0748254 (04/06/2005) 2005) COLUMBIA COUNTY DEED RECORDING (2005-004662)
- COLUMBIA COUNTY DEED RECORDING (2010-006118)
- BOOK 261 PAGE 200 (STREET DEDICATIONS)
- BOOK 238 PAGE 233 AND 234 (SANITARY SEWER EASEMENT AT LINE, LOCATION UNDEFINED BUT LINE SHOWN ON EASEMENT SHEET 2 OF 4)
- BOOK 208 PAGE 645 (WATER LINE EASEMENT)
- BOOK 253 PAGE 304 (POWER LINE EASEMENT)
- LEGAL DESCRIPTIONS IN GENERAL STARTED ALONG THE NORTH LINE OF THE JESSE MILES DLC AND THEN DEFINED THE PARCELS BY USING THE BEARING AND DISTANCES RELATIVE TO THE NORTH LINE OF THE JESSE MILES DLC ESTABLISHED BY FINDING THE NORTH WEST CORNER OF THE JESSE MILES DLC AND THE SW CORNER OF THE WILLIAM WATT DLC AND USING THE COLUMBIA COUNTY SURVEY 0135, 1806, AND 4390.

SURVEY MONUMENTS FOUND DURING ALTA SURVEY ARE MARKED AS NOTED ON SHEET 1 OF 4
LOT 100 WAS NOT RESEARCHED FURTHER THAN TO IDENTIFY THE LEGAL DESCRIPTION BOUNDARIES AND TO IDENTIFY THE ROADWAY DEDICATIONS

ENCROACHMENTS ON TO ADJACENT PROPERTIES

CONCRETE CROSSES ON TO THE ADJACENT PROPERTY TO THE NORTH OF LOT 300. THE ADJACENT PARCEL OWNED BY OTHERS THE OWNERSHIP IS NOT KNOWN
CONCRETE CURB AND DRIVEWAY PAVEMENT FROM LOT 300 CROSS INTO WALNUT RIGHT OF WAY
CONCRETE CURBS, PAVEMENT AND DRIVEWAY OF LOT 100 CROSSES ON TO WALNUT RIGHT OF WAY

FLOOD PLAIN:

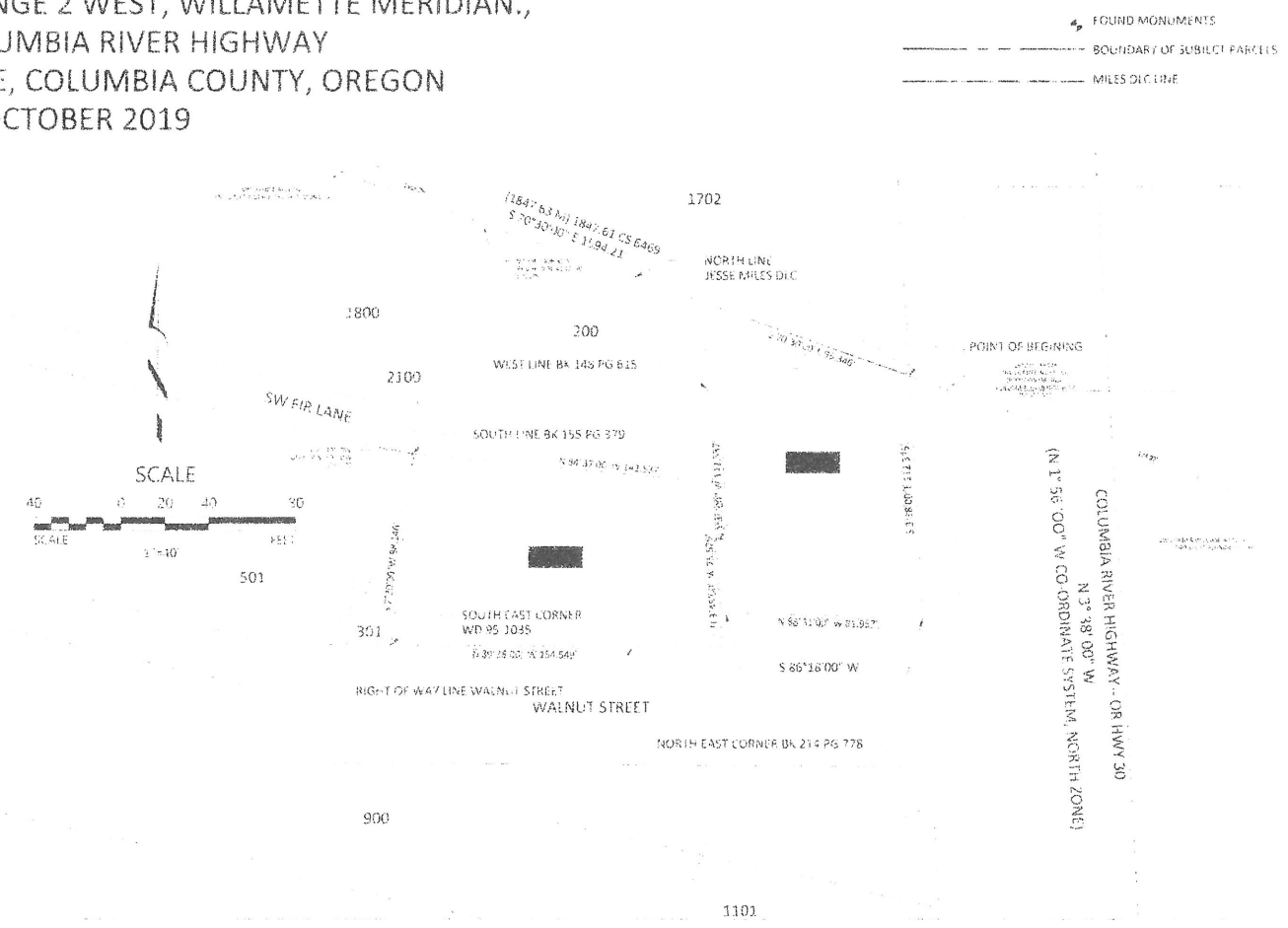
FEMA MAP NUMBER 410130178E CALLS THE AREA AS AN AREA OF MINIMAL FLOOD HAZARD
LEGAL DESCRIPTION ENCOMPASSING LOT 100 AND 300 MAP N2W13-BA

TABLE "A" ITEMS

- 5. VERTICAL ELEVATION CONTOURS ARE BASED ON A NAVD 88 BENCH MARK WITH AN ELEVATION OF 64.78 IN THE SOUTH PART OF SCAPPOOSE, ABOUT 100 FEET WEST OF COLUMBIA RIVER HIGHWAY. AT THE EAST ENTRANCE TO THE SCAPPOOSE MIDDLE SCHOOL IN THE TOP OF THE NORTH END OF THE LOWEST STEP, THE MONUMENT IS A OREGON STATE HIGHWAY DEPARTMENT STANDARD DISK STAMPED 61 x 101 1933.
- 6. SUBSTANTIAL FEATURES INCLUDING CURBS, BUILDING, NUMBER OF PARKING SPACES DRYWELLS, CATCHBASINS, WALLS FENCES, CONCRETE AREAS AS WELL AS EXPOSED UTILITIES FEATURES AND VAULTS ARE SHOWN.
- 11. UNDERGROUND UTILITIES AS WELL AS THOSE CABLE UTILITIES WITHIN 5 FEET OF THE PROPERTY LINE WERE MARKED BY A PRIVATE UTILITY LOCATE COMPANY HIRED BY THE PHK PROPERTIES, THE FUTURE OWNER AND CLIENT

SURVEYOR'S CERTIFICATE

TO: PHK DEVELOPMENT INC. AND TICOR TITLE COMPANY OF OREGON
THIS IS TO CERTIFY THAT THIS MAP AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2016 MINIMUM STANDARD ESTABLISHED AND ADOPTED BY ALTA AND NSPS AND INCLUDES ITEMS 5, 8, AND 11 OF THE TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON OCTOBER 8, 2019.



ENCROACHMENTS

WEST END OF WALNUT STREET DRIVEWAY AND PARKING 16.74' MAX INTO RIGHT OF WAY
EAST END OF WALNUT STREET CURB AND DRIVEWAY 1.34' MAX INTO RIGHT OF WAY
RAMP NEAR BACK DOOR 0.86' MAX INTO LOT 200

LEGAL DESCRIPTION: ENCOMPASSING LOT 100 AND 300 MAP N2W13-BA

A TRACT OF LAND LOCATED IN THE NORTHWEST QUARTER OF SECTION 13, T3N, R2W OF THE WILLAMETTE MERIDIAN, FURTHER DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF THE JESSE MILES DONATION LAND CLAIM; THENCE SOUTH 70° 30' EAST ALONG THE NORTH LINE OF THE SAID DLC, A DISTANCE OF 1594.21 FEET TO THE TRUE POINT OF BEGINNING OF THE FOLLOWING DESCRIBED TRACT OF LAND; SAID POINT FALLS ON THE WEST RIGHT OF WAY LINE OF THE COLUMBIA RIVER HIGHWAY, BEING DESCRIBED IN DEED BOOK 261, PAGES 200-202, COLUMBIA COUNTY DEED RECORDS; THENCE SOUTH 03° 38' 00" EAST ALONG SAID RIGHT OF WAY LINE, A DISTANCE OF 110.69 FEET TO THE NORTH EAST CORNER OF THE PARCEL CONVEYED TO NEWTON D. AND MARTHA A. STOKES IN DEED BOOK 214, PAGE 778; THENCE NORTH 88°31'00" WEST ALONG SAID PROPERTY LINE, A DISTANCE OF 86.98 FEET TO A POINT; THENCE SOUTH 86°18'00" WEST TO THE CORNER OF SAID PROPERTY, A DISTANCE OF 5.76 FEET; THENCE SOUTH 3°45'00" EAST TO THE RIGHT OF WAY LINE OF WALNUT STREET, A DISTANCE OF 15.30 FEET; THENCE NORTH 89°38'00" WEST ALONG SAID RIGHT OF WAY, A DISTANCE OF 151.77 FEET TO THE SOUTHEAST CORNER OF THE PARCEL CONVEYED TO CESSNA R. SMITH IN WARRANTY DEED 95-1035; THENCE NORTH 05°48'00" EAST, A DISTANCE OF 87.84 FEET ALONG SAID PARCEL TO THE SOUTH LINE OF THE PARCEL CONVEYED TO CLYDE A. PUGH AND MARIE E. PUGH IN DEED BOOK 155 PAGE 379; THENCE SOUTH 84°37'00" EAST, A DISTANCE OF 138.56 FEET TO THE WEST LINE OF THE PROPERTY CONVEYED TO WALTER AND FRANCES DONALD IN DEED BOOK 148, PAGE 615; THENCE NORTH 03°45'00" WEST ALONG WEST LINE OF SAID PROPERTY, A DISTANCE OF 82.30 FEET TO A POINT ON THE NORTH LINE OF THE JESSE MILES D.L.C. BEING THE NORTHWEST CORNER OF THE SAID DONALD TRACT; THENCE SOUTH 70°30'00" EAST ALONG SAID NORTH LINE OF THE D.L.C., A DISTANCE OF 100.78 FEET MORE OR LESS TO THE POINT OF BEGINNING.

CONTAINS APPROXIMATELY 23,492.06 SQUARE FEET WITH A PERIMETER OF 779.89'

THERE IS A CLOSURE ERROR DISTANCE OF 0.08' AT S81° 21' 44"E WITH A PRECISION OF 1/9748.625



DAN GILBERT, PLS OREGON REGISTRATION NO. 69121
DATED THIS 14TH DAY OF OCTOBER 2019

JLL	10/10/19
Designed	Date
JLL	10/10/19
Drawn	Date
GTK	10/10/19
Checked	Date

EXPIRES: 6/30/21
REGISTERED
PROFESSIONAL
LAND SURVEYOR
Dan Gilbert
OREGON
JULY 11, 2000
DAN GILBERT
60121

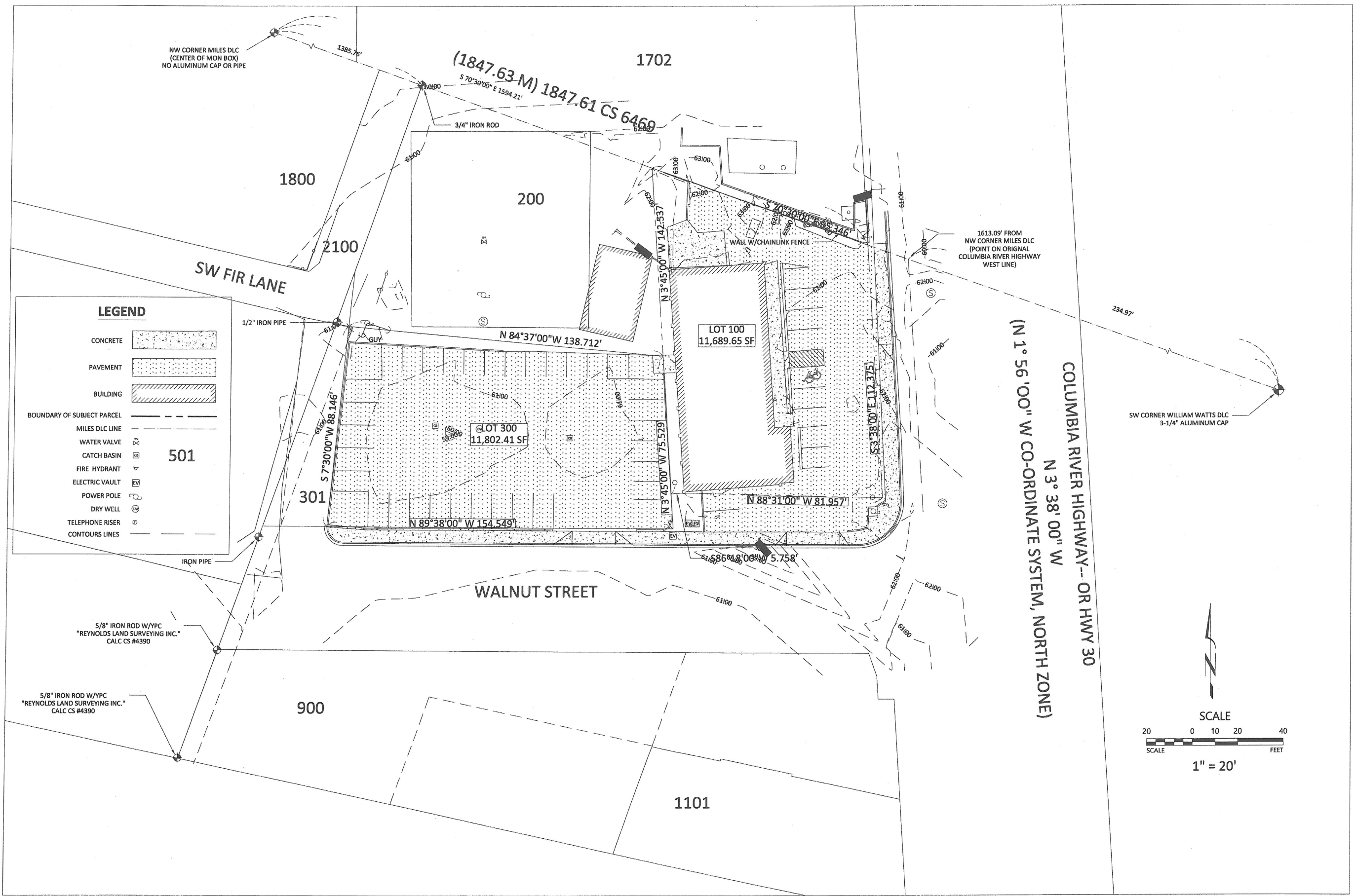
DAN L. GILBERT
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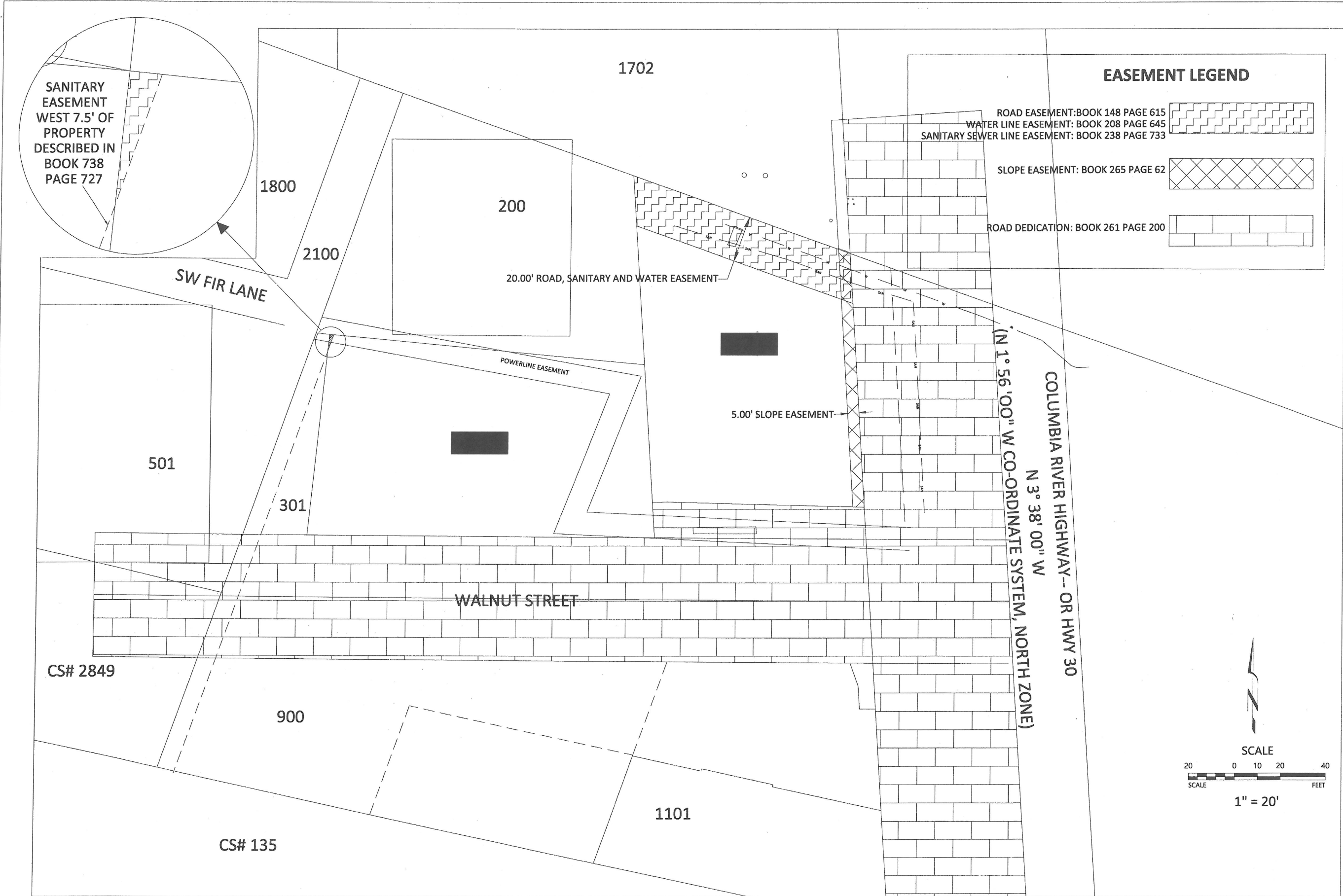
SITE PLAN

Varsity Grill
PHK Development, Inc.
3330 NW Yeon Ave., Suite 210
Portland, OR 97210

KURAHASHI AND ASSOCIATES
COMPANY
4470 SW HALL BLVD STE C
BEAVERTON, OR 97005

2 OF 4
Sheet No.
Project No. 2797





JLL	10/10/19
Designed	Date
JLL	10/10/19
Drawn	Date
GTK	10/10/19
Checked	Date

EXPIRES: 6/30/21

REGISTERED PROFESSIONAL LAND SURVEYOR

Dan L. Gilbert

OREGON JULY 11, 2000 DAN GILBERT 80121

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EASEMENTS AND DEDICATIONS

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3330 NW Yeon Ave., Suite 210
Portland, OR 97210

KURAHASHI AND ASSOCIATES COMPANY
4470 SW HALL BLVD STE C
BEAVERTON, OR 97005

3 OF 4
Sheet No.



JLL	10/10/19
Designed	Date
JLL	10/10/19
Drawn	Date
GTK	10/10/19
Checked	Date



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KURAHASHI AND ASSOCIATES
COMPANY
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BEAVERTON, OR 97005

4 OF 4
Sheet No.

MEMORANDUM

DATE: February 18, 2020

TO: Mike Perso, William / Kaven

FROM: Monica Leal, PE, PTOE
Richard Martin, EIT

SUBJECT: 52001 S Columbia River Highway Transportation Impact Analysis

P19-094-000

This memorandum summarizes the traffic impact analysis associated with the proposed development located at 52001 S Columbia River Highway in the City of Scappoose, Oregon. The purpose of this analysis is to identify potential impacts to the transportation network for the year of the opening of the site, based on the standards established by the City of Scappoose and the Oregon Department of Transportation (ODOT). Based on the City of Scappoose Transportation Impact Study Guidelines¹, this traffic impact analysis considers the following elements:

- Introduction
- Existing Conditions
 - Existing Deficiencies
 - Existing Traffic Volumes
 - Existing Intersection Operations
 - Crash Data Analysis
- Planned Improvements
- Impacts Analysis
 - Trip Generation and Trip Distribution
 - Background Growth
 - In-Process Trips
 - 2021 No-Build Volumes
 - 2021 Buildout Volumes
 - Future Intersection Performance
- Sight Distance Evaluation
- US 30 at Site Approach Analysis
- Site Circulation/Queueing and Parking Analysis
- Queueing Analysis
 - On-Site Queueing Analysis
 - Intersection Queueing Analysis
- Access Spacing Standards
- Public Works Design Standards Compliance
- Bicycle and Pedestrian Facilities
- Mitigations and Summary
- Appendix

¹ City of Scappoose Transportation Impact Guidelines, June 2016. The report outline was also coordinated with ODOT, November 2019.

Based on coordination with the City of Scappoose and ODOT, the following intersection and site accesses were evaluated:

- SW Walnut Street / SE High School Way at S Columbia River Highway
- Existing site approach to US 30²
- Proposed site access points on Walnut Street

ODOT recommends studying all the state highway intersections that may be anticipated to see an increase in either 50 peak hour trips and/or 300 ADT. Based on the trip generation and distribution evaluation, it was determined that one intersection would be impacted by the number of trips necessary to trigger capacity analysis. The intersection is that of SW Walnut Street / SE High School Way at S Columbia River Highway. It is also anticipated that the number of new trips added to the adjacent intersections on US 30 will be reduced due to the commercial land use characteristics and access points in the area.

INTRODUCTION

The new development will be located on the northwest quadrant of the Columbia River Highway (US 30) and Walnut Street intersection (See Figure 1 for the vicinity map). The Varsity Grill and Bar is currently operating at this location. Access to the existing property includes two driveways on Walnut street and one driveway on US 30.

The new development will include office/retail and coffee/donut shop with drive-through window land uses and two parking lots (See Figure 2 for the proposed site plan). The parking lot on the western portion of the lot will be reconfigured, and two new driveways will be implemented to accommodate a drive-through, the service window for which will be located on the back side of the proposed restaurant portion of the existing building, as shown in Figure 1.

The eastern parking lot on US 30 will be maintained and will serve the proposed land uses. The existing driveway on US 30 will be redesigned to be a 36 feet full access driveway. The driveway on Walnut Street approximately 40 feet west of US 30 will be closed as part of this project. The closure of this driveway will eliminate cut-through traffic, using the eastern parking lot to access the eastbound through lane to skip the southbound left queues at the US 30/Walnut Street/SE High School Way intersection.

EXISTING CONDITIONS

Existing transportation conditions were evaluated along S Columbia River Highway (US 30), SW Walnut Street, and SE High School Way. All modes of travel including pedestrians, bicycles, and motor vehicles were evaluated. The existing transportation conditions are summarized in Table 1. As shown in Figure 1, SE High School Way is divided by a railroad line that runs adjacent to US 30.

² Coordination with the City of Scappoose and ODOT, November 2019.

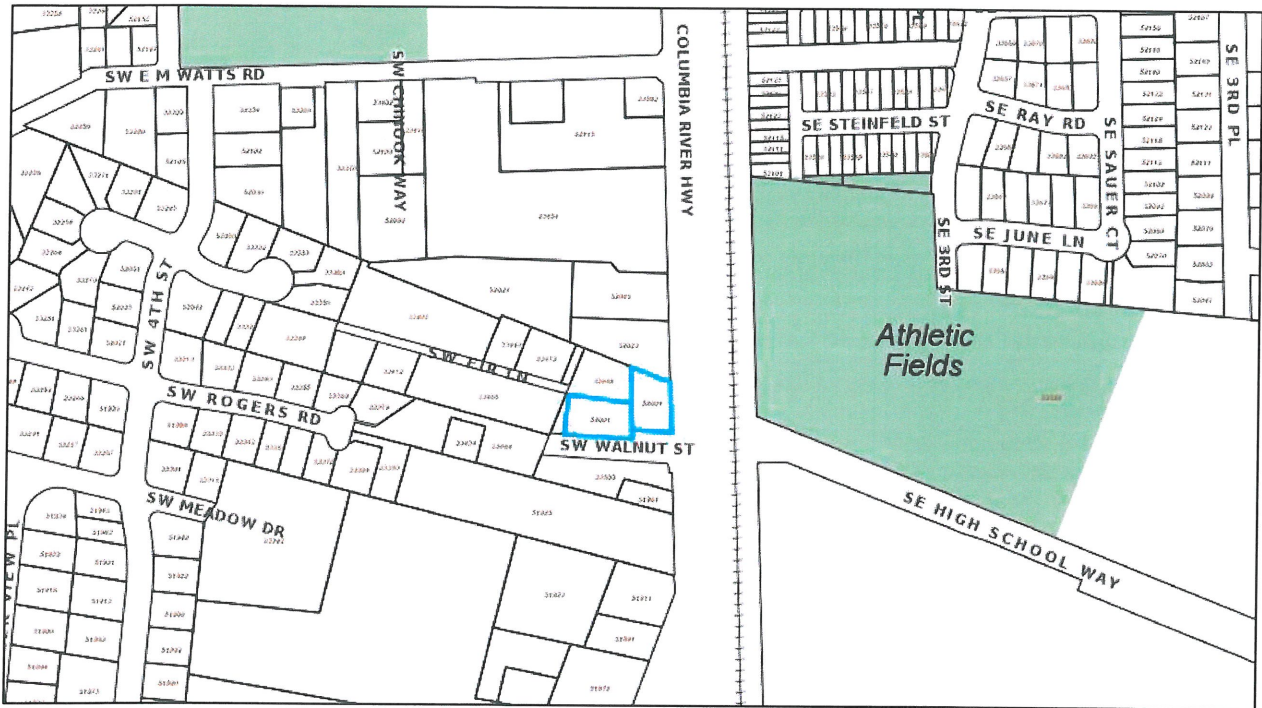


Figure 1: Vicinity Map

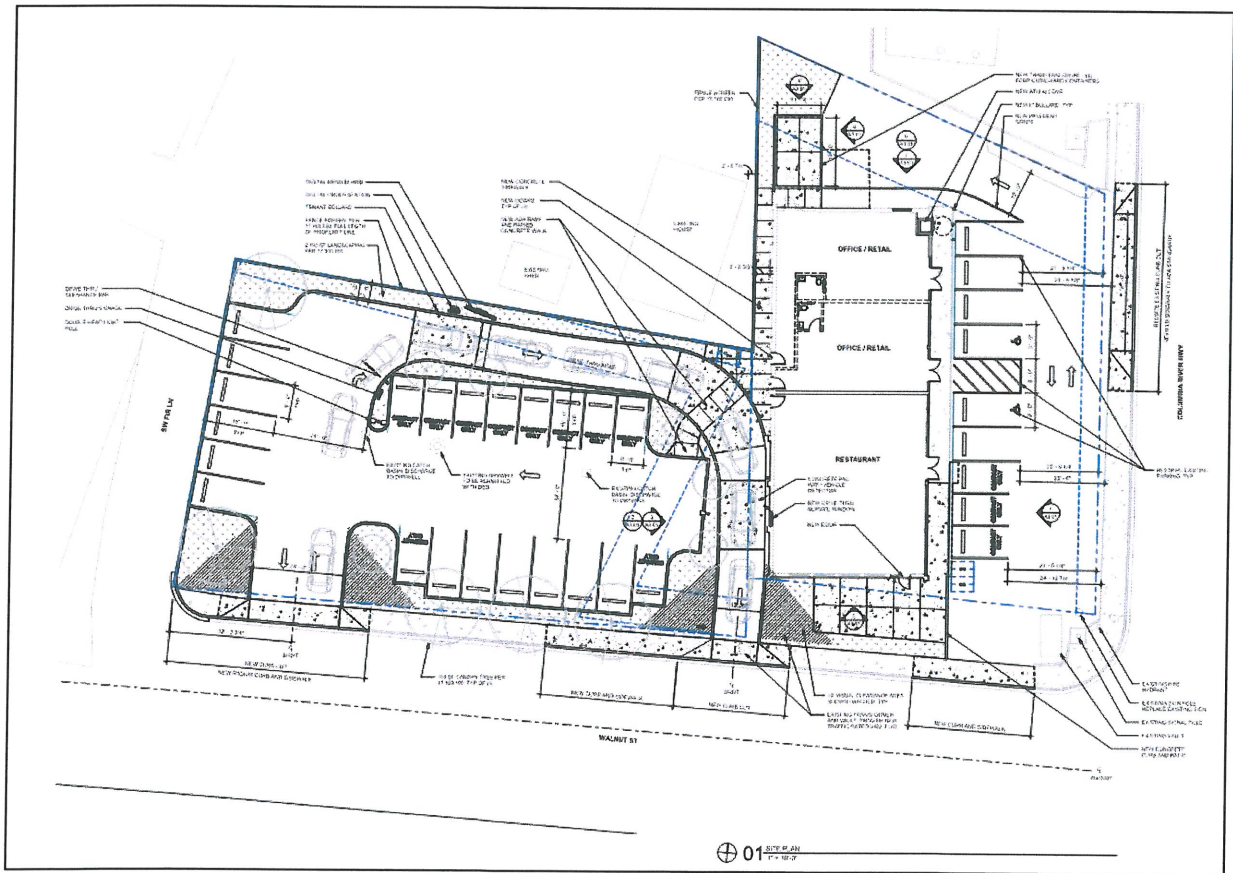


Figure 2: Site Plan

Table 1: Roadway Characteristics

Roadway	Functional Classification ¹	Posted Speed Limit	Sidewalks	Transit	Bike Lanes	Lane Geometry	On-Street Parking
US 30²	Arterial (City) /Urban Other Principal Arterial (ODOT)	35 mph	Partial – Sidewalks do not exist on the east side of US30 south of SE High School Way	NW Collector Line 3	Both Sides	Two 12' lanes in each direction and a two-way left-turn lane	None
SW Walnut Street	Minor Collector	25 mph	Both Sides	None	None	One 11'-12' lane in each direction	Yes
SE High School Way	Neighborhood Route	20 mph	Both Sides	None	None	One 12' lane in each direction	None

Notes: ¹Based on the City of Scappoose Transportation System Plan (TSP), Figure 12 and ODOT TransGIS .
² US 30 is classified as a Statewide Highway, is part of the National Highway System (NHS), and is a designated freight route and a truck route.

Existing Deficiencies

The following deficiencies were identified:

- The project site currently hosts two accesses serving the eastern parking lot of the property, one fronting US 30 and one fronting Walnut Street. During the AM peak hour, vehicle traffic uses the Varsity Bar and Grill parking lot and the existing eastern driveway on Walnut Street as a cut-through, moving into the eastbound through lane and skipping the southbound left queues at the US 30/Walnut Street/SE High School Way intersection. The existing eastern driveway on Walnut Street will be closed as part of this project to mitigate this issue.
- No existing sidewalks exist on the east side of US 30 just south of SE High School Way. A sidewalk infill project from Havlik Drive to SE High School Way is identified in the City of Scappoose Transportation System Plan (TSP), Financially Constrain and Aspirational Project List with ODOT as the primary funding source³.

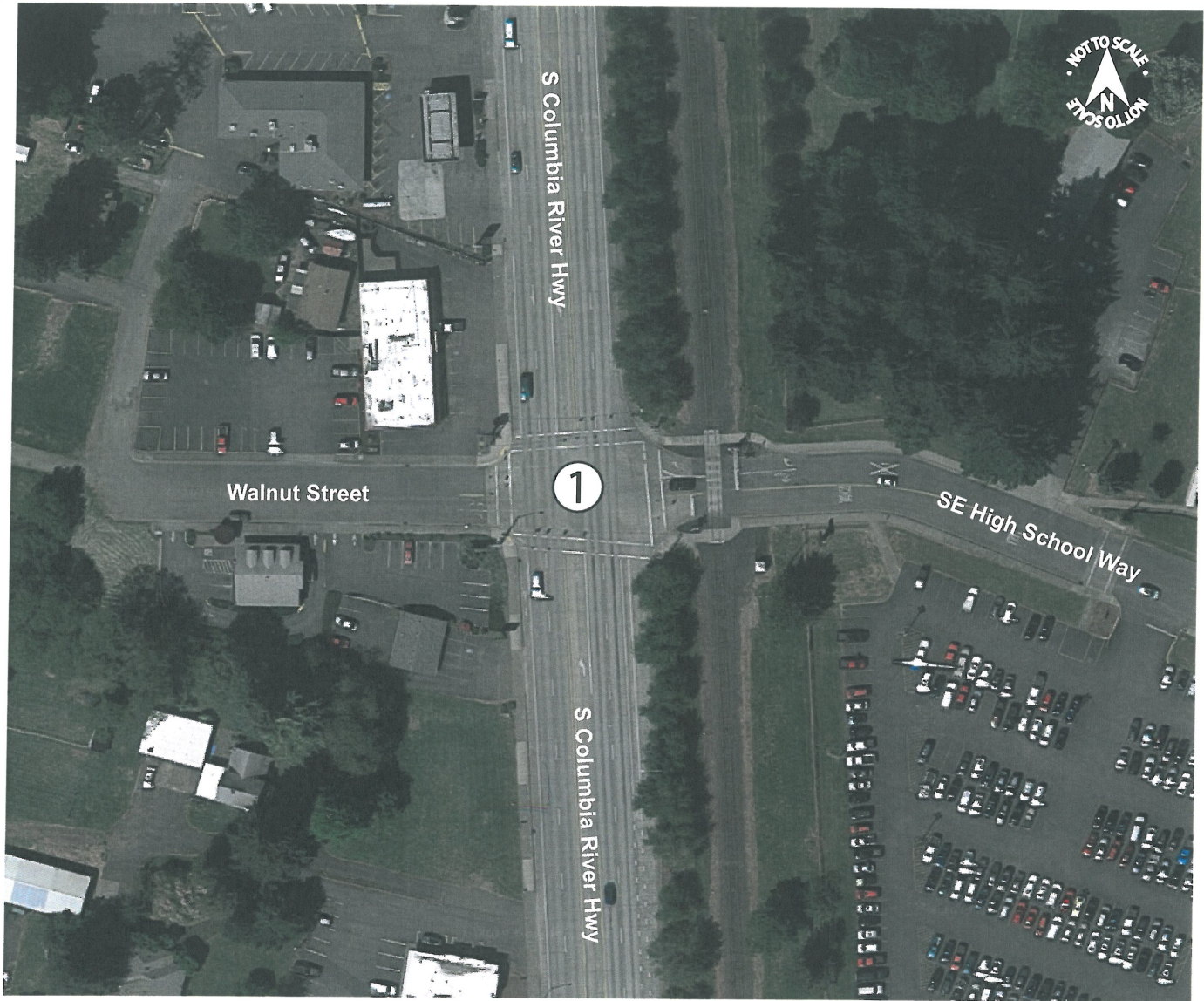
Existing Traffic Volumes

Traffic count data was collected at the SW Walnut Street / SE High School Way at S Columbia River Highway intersection on October 22, 2019 during the AM peak period (7:00 to 9:00 AM) and the PM peak period (4:00 to 6:00 PM). The peak hour traffic count data and existing lane configurations are shown in Figure 3. The detailed traffic count data has been included in Appendix A.

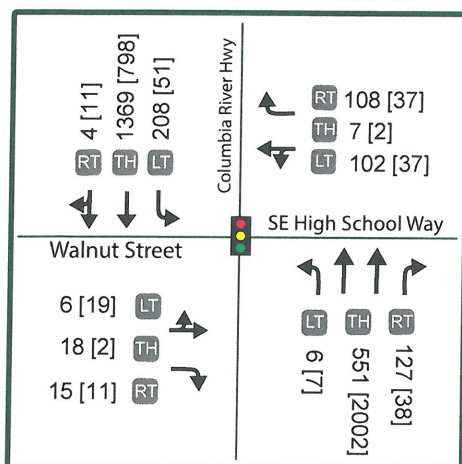
Based on the ODOT Analysis Procedure Manual, the collected traffic count data was adjusted to replicate the conditions when traffic volumes are typically the highest. A seasonal factor was applied to the traffic count data to develop 30th Highest Hour volumes. Figure 3 shows the 30th Highest Hour traffic volumes utilized in this analysis. This seasonal factor was determined to be 1.10 based on an average of the “Commuter” and “Summer” trends provided in the ODOT Seasonal Trend Table provided in Appendix B. This approach is consistent with the City of Scappoose TSP⁴. Generally, PM peak hour volumes are higher than the AM peak hour. However, since the trip generation evaluation shows a higher number of generated trips during the AM peak hour, both AM and PM traffic count data were adjusted by the seasonal factor and were considered in this analysis.

³ City of Scappoose Transportation System Plan, Volume 1, Table 2, Project #W40.

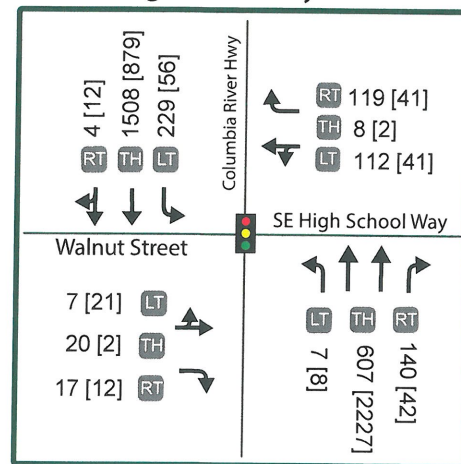
⁴ City of Scappoose Transportation System Plan Update, Volume 2, Page 8, January 8, 2014.



Raw Traffic Count Volumes



30th Highest Hourly Volumes



Traffic Signal
 = Study Intersection
 AM [PM] = Peak Hour Traffic Volumes
 = Lane Configuration
 LT TH RT = Left / Through / Right Turn

Figure 3: Existing Traffic Volumes

Existing Intersection Operations

An intersection performance analysis was conducted to document the existing operations for the study intersection and to develop a baseline for analyzing future intersection operational needs associated with the proposed development. The peak periods analyzed for this evaluation include the AM peak period (7:00 to 9:00 AM) and the PM peak period (4:00 to 6:00 PM).

The level of service and volume-to-capacity (V/C) analyses presented in this report has been completed using the Synchro (Version 10) analysis software. Synchro is based on the Highway Capacity Manual 6th Edition (HCM 6) methodology for signalized intersections. The V/C ratios are calculated according to ODOT's Analysis Procedures Manual Version 2, Section 13.4.4. The analysis was performed for the signalized study intersection of SW Walnut Street / SE High School Way at US 30.

The City of Scappoose utilizes level of service D or better with a maximum recommended volume-to-capacity (v/c) ratio of 0.90 for signalized intersections, as stated in the City of Scappoose TSP⁵. ODOT utilizes v/c mobility targets based on the functional classification and speeds of the crossing streets at the intersection. Based on the 1999 Oregon Highway Plan, a mobility target of 0.85 was utilized for the study intersection. Table 2 summarizes the mobility targets and the existing traffic operations for the study intersection. The detailed analysis results have been included in Appendix C. Under the existing intersection performance analysis, the intersection operates below acceptable standards for LOS under City of Scappoose guidelines in the AM peak hour and V/C ratio standards for both governing bodies during the PM peak hour.

Table 2: Existing Intersection Performance Summary

Intersection	Peak Hour	Mobility Targets	2019 Existing Conditions		
			Control Delay (Sec) ¹	Level of Service	V/C
SW Walnut Street / SE High School Way at US 30	AM	LOS D (City) v/c=0.90 (City)	60.7	E	0.84
	PM	v/c=0.85 (ODOT) ²	49.3	D	1.19
Notes: ¹ Delay is reported as the average total delay of the intersection ² Based on the 1999 Oregon Highway Plan, Policy 1F, Table 6. Red = does not meet mobility target					

Crash Data Analysis

The last five years of available crash data (January 2013 through December 2017) was obtained from the ODOT Crash Data System and was reviewed to identify traffic safety concerns at the study intersection. The crash rates presented in Table 3 are based on the number of crashes per million entering vehicles (MEV). Typically, an intersection is not considered unsafe unless its crash rate exceeds the threshold of 1.0 crashes per MEV. The crash rate was also compared to the statewide 90th percentile crash rate of 0.86. If the crash rate is higher than the statewide 90th percentile crash rate, the intersection is flagged for further analysis and needs to be reviewed in more depth⁶. Currently the SW Walnut Street / SE High School Way at US 30 is not part of the ODOT SPIS 10% list.

⁵ 2016 Scappoose Transportation System Plan: Volume 1, page 52.

⁶ ODOT Analysis Procedure Manual, Section 4.2, Crash Data.

Table 3 Crash Rate – Five Years

Intersection	Crash History (Years)	Number of Crashes	Crashes per year	Annual Traffic Entering (veh/yr)	Crash rate per M.E.V. ¹	Statewide 90 th Percentile Crash Rate ²
SW Walnut Street / SE High School Way at US 30	5	9	1.8	9,161,500	0.200	0.86 (4SG)
Note: ¹ M.E.V. – million entering vehicles. ² Assessment of Statewide Intersection Safety Performance, FHWA-OR-RD-18, Portland State University and Oregon State University, June 2011, Table 4.1, p. 47. 4SG = four-leg signalized intersection						

During the analysis period, 9 crashes were reported at the study intersection. Four of the crashes were rear-end collisions (one with snow and another with wet surface conditions), three were turning-movement collisions, one was an angle-type collision, one was a bicycle collision, and one was a pedestrian collision. The crash reported as a bicycle collision occurred when a car turning west-to-south did not have or yield the right of way to a bicycle and could not avoid collision because it was driving too fast for the wet and raining conditions. The cyclist sustained a non-incapacitating injury. The crash reported as a pedestrian collision occurred when a stopped vehicle attempted to make a right-turn at a red light. The vehicle failed to yield the right-of-way to a person in a motorized wheelchair utilizing the marked pedestrian crosswalk. The two collided, causing the person in the wheelchair to sustain a non-incapacitating injury. Three other minor injuries were reported. No crash patterns were identified. The intersection does not exceed the crash rate threshold of 1.0 crash per MEV and is below the statewide average crash rate. Therefore, mitigations were not considered. Detailed crash data is available in Appendix D.

PLANNED IMPROVEMENTS

The Scappoose TSP outlines several areas of improvements⁷. A new Collector roadway is planned to connect Walnut Street to SW Old Portland Road to the south. This roadway, along with Walnut Street and SE High School Way, will be part of a proposed bicycle route. A sidewalk infill project is outlined in the TSP for the new Collector and for parts of SE High School Way (east of the project site) and US 30 (south of the SE High School Way).

IMPACTS ANALYSIS

A Future Conditions Analysis was conducted to determine the expected traffic operating conditions for the study intersection for the buildout year 2021⁸. Background growth and site-generated trips were added to existing 30th Highest Hour volumes to develop the following scenarios:

- 2021 No-Build – existing 30th highest hour volumes plus background growth
- 2021 Buildout – existing 30th highest hour volumes plus background growth and site-generated trips

⁷ City of Scappoose Transportation System Plan, Table 2, D12, W3, W40, B3, B23

⁸ The future year of analysis was coordinated with the City of Scappoose on October 31, 2019.

Trip Generation and Trip Distribution

The current use for the facilities located at 52001 S Columbia River Highway is a drinking place⁹ (ITE land use Code 925). The proposed development is for the following land uses:

- Office/Retail (2,030 sq. ft.). A Small Office Building use was assumed in this analysis.
- Coffee/Donut Shop with Drive-Through Window (2,030 sq. ft.)

Trip rates presented in the Institute of Transportation Engineer's (ITE) *Trip Generation Manual*, 10th Edition, were utilized to estimate the number of vehicle trips based on square footage of each land use. The site's trip generation is based on ITE trip generation rates for weekdays during the peak hour of adjacent street traffic. The analysis includes all trips that enter and exit the site, which includes the pass-by trips. Table 4 summarizes the estimated trip generation for the site.

As summarized in Table 4, the proposed uses are expected to generate more morning peak hour trips and less afternoon peak hour trips than the current use. With pass-by trips removed from the coffee/donut shop land use, the proposed land uses have the potential to generate additional 96 trips during the AM peak hour and one trip during the PM peak hour. The daily trip generation calculation was omitted from these net increase calculations due to small sample size for the ITE 937 land use code and lack of data for the ITE 925 land use code.

Table 4: Trip Generation Summary

Land Use	Size (sf)	Weekday						
		Daily	AM Peak Hour			PM Peak Hour		
			Total	Enter	Exit	Total	Enter	Exit
Existing Land Use								
Drinking Place (ITE 925)	4,170	--	--	--	--	11.36	66%	34%
Site Trips		--	--	--	--	48	32	16
Proposed Land Use								
Small Office Building (ITE 712) ¹	2,030 ³	16.19	1.92	83%	18%	2.45	32%	68%
Site Trips		33	4	3	1	5	2	3
Coffee/Donut Shop with Drive-Through Window (ITE 937)	2,030	--	88.99	51%	49%	43.38	50%	50%
Site Trips		--	181	92	89	88	44	44
Pass-By Trips (49% AM, 50% PM) ²		--	-89	-45	-44	-44	-22	-22
Net New Site Trips								
Proposed Minus Existing Land Use	--	--	185	95	90	45	14	31
Proposed Minus Existing (without Pass-By Trips)	--	--	96	50	46	1	-8	9
Notes: ¹ A Shopping Center (ITE Code 820) was also considered for potential retail options, but a Small Office Building has a higher trip generation rate during the morning peak hours. No Pass-by trip reductions were applied to represent a more conservative approach. ² Pass-by trips: Traffic already on the way from an origin to a primary trip destination that will make an intermediate stop at the site being studied without a route diversion. Pass-by trips will not add vehicles to the adjacent street network. The pass-by trips were based on the ITE Trip Generation Handbook, 10 th Edition code recommended by ODOT Region 2, "Fast Food Restaurant with Drive-Through" (ITE Code 934), which are identified as 49% (AM peak hour) and 50% (PM peak hour). ³ Includes 1,171 sq. ft and 848 sq. Ft								

⁹ A drinking place contains a bar, where alcoholic beverages and food are sold, and possibly some type of entertainment, such as music, television screens, video games, or pool tables. Varsity Grill & Bar is current located at the project site.

Background Growth

A background growth rate was developed by comparing the 2013 and 2035 PM Peak Hour volumes included in the City of Scappoose TSP. Based on this comparison, a growth rate of 3.11% per year was determined for use in the analysis.

In-Process Trips

Per discussions with staff from the City of Scappoose, no trips from in-process developments were included in this analysis.¹⁰

2021 No-Build Traffic Volumes

The background growth rates were applied to the existing 30th Highest Hour volumes to develop the 2021 No-Build traffic volumes. This represents the projected traffic volumes along the transportation network without the proposed development. The 2021 No-Build traffic volumes are shown in Figure 4.

2021 Buildout Traffic Volumes

To develop the 2021 Buildout traffic volumes, project site generated trips were distributed and added to the 2021 No-Build volumes. Trips were distributed based on the existing traffic patterns in the study area. The 2021 Buildout traffic volumes are presented in Figure 4.

Future Intersection Performance

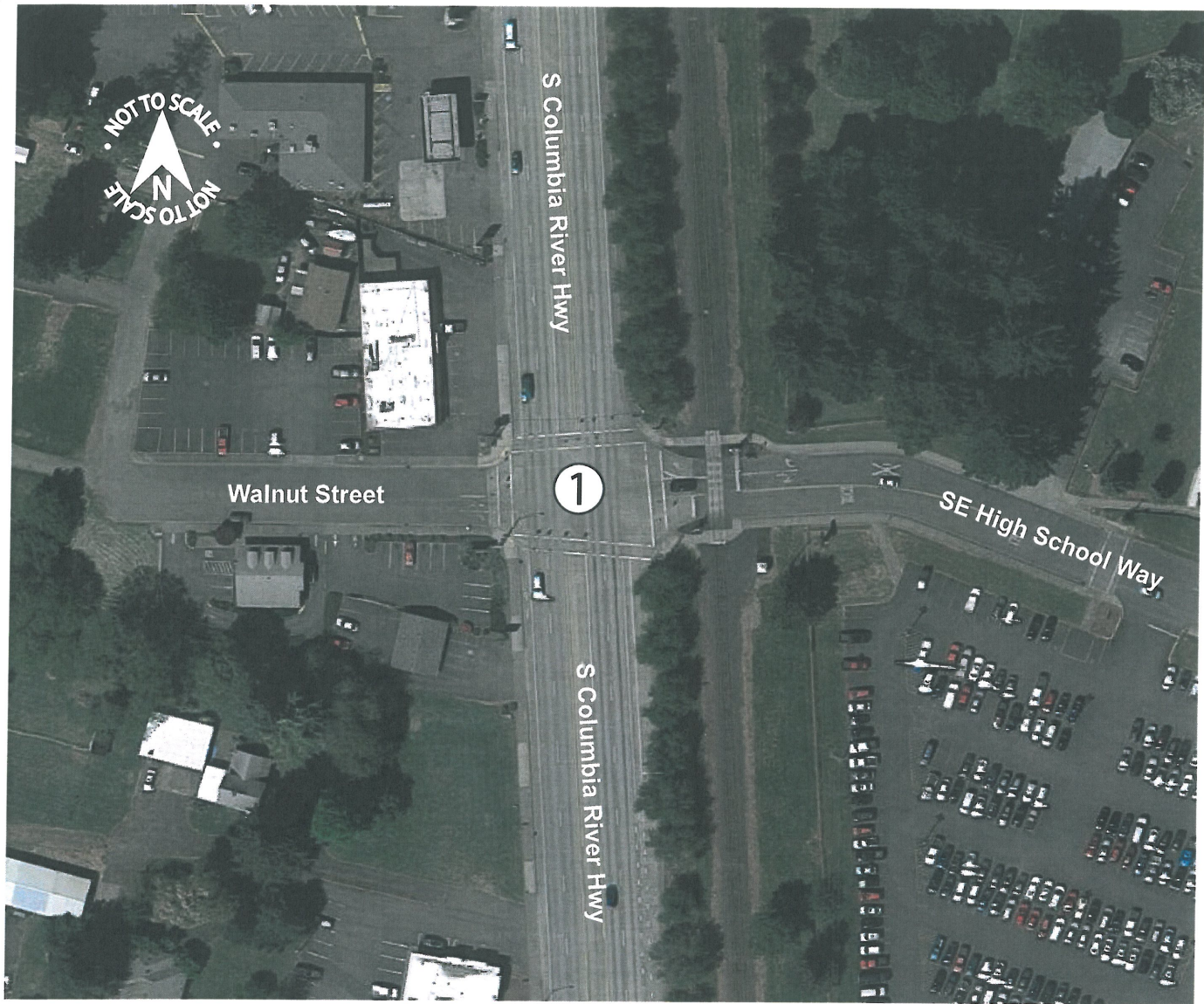
Table 5 summarizes the AM and PM peak hour analysis results for the 2021 Background and 2021 Buildout future scenarios. Intersection geometry was assumed to be the same as under the existing conditions at the intersection of Walnut Street / SE High School Way at US 30. The detailed analysis results have been included in Appendix E.

Table 5: Intersection Performance Summary - 2021 Traffic Volumes

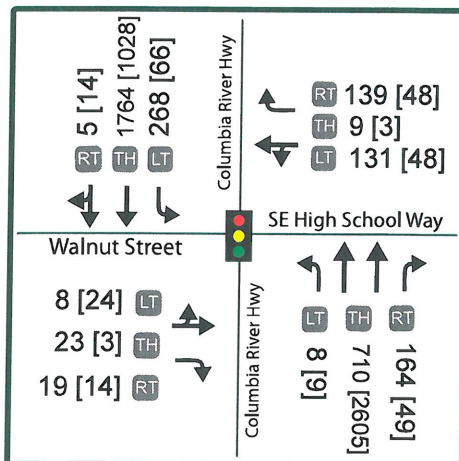
Description	Mobility Targets	2021 Background		2021 Buildout	
		AM	PM	AM	PM
Walnut Street / SE High School Way at US 30					
Level of Service	LOS D (City)	E	F	E	F
Control Delay (Sec) ¹	v/c=0.90 (City)	62.4	89.2	70.3	88.1
Volume to Capacity	v/c=0.85 (ODOT) ²	0.86	1.34	0.88	1.33
Notes: ¹ Delay is reported as the average total delay of the intersection					
² Based on the 1999 Oregon Highway Plan, Policy 1F, Table 6.					
Red = does not meet mobility target					

As shown in Table 5, the intersection does not meet performance standards during both 2021 background peak hour scenarios. Since standards are not met regardless of the addition of site trips, this is considered a preexisting condition and mitigation is not recommended. Based on the trip generation analysis, the proposed land uses have the potential to add 1 trip during the PM peak hour which represents the worst peak hour condition.

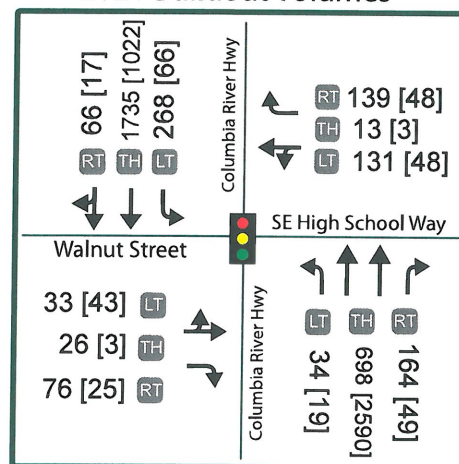
¹⁰ Email from City Engineer Chris Nagelspace dated October 31, 2019.



2021 No-Build Volumes



2021 Buildout Volumes



Traffic Signal = Study Intersection
 AM [PM] = Peak Hour Traffic Volumes
 = Lane Configuration
 LT TH RT = Left / Through / Right Turn

Figure 4: Future Traffic Volumes

SIGHT DISTANCE EVALUATION

Intersection and stopping sight distances were evaluated for the proposed site accesses along Walnut Street. The sight distance evaluation follows the guidance provided in the American Association of State Highway and Transportation Officials (AASHTO) Geometric Design of Highways and Streets, 2011.

For a 25-mph roadway, AASHTO requires 155 feet of stopping sight distance and 280 feet of intersection sight distance. AASHTO also requires an assumed object height of 4.5 feet, a driver's eye height of 3.5 feet, and a driver's setback of 15 feet from the traveled way. Intersection sight distance was compared to the AASHTO design intersection sight distance for the following cases:

- Case B1, Left Turn from the Minor Road¹¹
- Case B2, Right Turn from the Minor Road¹²

Table 6 shows a summary of the sight distance evaluation. Sight distance was found to be clear in both directions for over 280 feet at the proposed western site access. Sight distance at the eastern site access was found to be clear for over 280 feet to the west and limited to 100 feet to the east by on-site vegetation. Clearing of this vegetation would allow for clear sight distance to the east for over 400 feet, through the intersection at US 30. As summarized in Table 6, intersection sight distance standards are met for all scenarios, provided the removal of the vegetation shown in Figure 5. No other mitigations are recommended.

Table 6: Sight Distance Evaluation

Sight Distance Evaluated	Estimated Available Sightline (ft)	Sight Distance Standards (ft)	Meets Standard?
Proposed western site access			
Case B1: Left-turn	To the east ≈ 520	280	Yes
	To the west ≈ 340	280	Yes
Case B2: Right-turn	≈ 520	240	Yes
SSD – WB on Walnut St	520	155	Yes
SSD – EB on Walnut St	340	155	Yes
Proposed eastern site access			
Case B1: Left-turn	To the East ≈ 100	280	No*
	To the West ≈ 280	280	Yes
Case B2: Right-turn	100	240	No*
SSD – WB on Walnut St	100	155	No*
SSD – EB on Walnut St	280	155	Yes
Notes:			
*Line of sight blocked by existing vegetation (See Figure 5).			

¹¹ AASHTO, Case B1 – Intersections with stop control on the minor road (AASHTO, Case B1, Table 9-6).

¹² AASHTO, Case B2 – Intersections with stop control on the minor road (AASHTO, Case B2, Table 9-8).



Figure 5: Sight Distance Obstructing Vegetation

US 30 AT SITE APPROACH ANALYSIS

Since the proposed development is going to provide parking on the western half of the property, it is anticipated that most traffic coming from the south on US 30 will use the Walnut Street/SE High School Way at US 30 intersection to access the proposed coffee shop which is the major trip generator. The number of parking spaces on the eastern side of the property is not proposed to change, so an increase in parking demand is not anticipated at the US 30 access. Access use and circulation will increase during morning hours because of the change in land use from a bar that opens at 4:00 PM to a coffee shop that opens early in the morning, but it will reduce during the PM peak hours. Potential cut-through traffic via the US 30 access to the existing eastern driveway on Walnut Street will be mitigated by closing the existing driveway that fronts Walnut Street. The parking area on the west side of the property will still be the main trip generator for parked vehicles. Therefore, mitigations at the US 30 access are not recommended.

Since the drive-through portion of the facility will be located on the western half of the property and not accessible from the site approach on US 30, no queueing issues from that land use are anticipated to take place at the existing site approach to US 30.

SITE CIRCULATION/QUEUEING AND PARKING ANALYSIS

Site entrance/exit access for the eastern portion of the development will be limited to the US 30 driveway when the developer closes the Walnut Street access. This parking lot will include three 8.5 ft by 15 ft compact parking spaces, four 9 ft by 18 ft standard parking spaces, and two 9 ft by 18ft accessible parking spaces with an adjacent 9 ft access compact aisle. The closure of the eastern driveway on Walnut street will change the existing parking circulation creating the need for a two-way circulation aisle. Approximately 24 ft and 10 inches between the compact only spaces and the back of sidewalk will be available for the two-way circulation aisle. This meets the City of Scappoose

Development Code which requires 24 feet two-way circulation aisles for 90-degree parking spaces¹³. However, only a 23 ft and 9 1/8 inches two-way circulation aisle can be accommodated between the standard/accessible parking spaces and the back of sidewalk (See Figure 2). This is a compromise between eliminating cut-through traffic and providing a 24 feet two-way circulation aisle.

The western parking lot will include approximately 230 feet of drive-through vehicle storage, ten 8.5 ft by 15 ft compact parking spaces, twelve 9 ft by 18 ft standard parking spaces, and 24 feet two-way circulation aisles. A full access driveway (entrance/exit access) will be installed approximately 220 feet west of US 30 (measured from the intersection curb return). The existing driveway located approximately 120 feet west of US 30 will be relocated slightly to the east and will serve as an exit for the coffee/donut shop drive-through aisle.

The project site currently offers a combined 41 parking stalls between the two parking areas. Under the proposed site configuration, 31 parking stalls will be available.

QUEUEING ANALYSIS

A peak hour queueing analysis was conducted for the on-site drive-through and the study intersection to determine vehicles storage needs.

On-Site Queueing Analysis

A peak hour queueing analysis was conducted for the on-site drive-through to determine vehicle storage needs. A 2015 analysis performed by Global Transportation Engineering for a coffee shop with a drive through window and no indoor seating studied three similar drive-through coffee shops to determine on-site queues for this type of developments¹⁴. Based on this study, a 95th percentile queue of 10 vehicles was observed during the AM peak hour (peak with highest on-site queues). Based on Figure 2, there is enough drive-through storage to accommodate the estimated 10 vehicles during the AM peak hour. There is also an additional 210 feet of storage on Walnut Street before queues reach the intersection at US 30.

Intersection Queueing Analysis

A queueing analysis was completed for the 2021 Background and Buildout scenarios based on the Synchro 10 operations analysis and SimTraffic. Table 7 summarizes the queueing analysis and detailed queueing reports can be found in Appendix F.

According to the queueing analysis, the movements generating the greatest queueing penalties are the westbound right movement during the AM peak hour and the northbound through movement during the PM peak hour, highlighted in red. However, these penalties occur during both 2021 background and buildout scenarios and it is considered a preexisting condition. Neither movement is projected to be significantly impacted by the proposed development. No queueing mitigations are recommended.

¹³ City of Scappoose Municipal Code, Chapter 17.106, Page 10.

¹⁴ Queueing Analysis for the "5810 N Lombard Street – Starbucks Traffic Impact Analysis", December 2015. Global Transportation Engineering.

Table 7: Synchro 10 SimTraffic Queueing Analysis Summary

AM Peak Hour											
Movement	EB	EB	WB	WB	NB	NB	NB	NB	SB	SB	SB
Directions Served	LT	R	LT	R	L	T	T	R	L	T	TR
Average Queue – 2021 Background	50	25	150	75	25	250	200	75	250	325	300
Average Queue – 2021 Buildout	75	75	150	75	75	250	200	75	275	450	450
95 th Percentile Queue-2021 Background	75	50	275	150	75	400	350	175	400	550	525
95 th Percentile Queue-2021 Buildout	125	125	275	150	175	375	350	200	500	750	725
Link Distance	232	-	371	-	-	836	836	-	-	838	838
Storage Bay Distance	-	200	-	100	200	-	-	200	500	-	-
PM Peak Hour											
Movement	EB	EB	WB	WB	NB	NB	NB	NB	SB	SB	SB
Directions Served	LT	R	LT	R	L	T	T	R	L	T	TR
Average Queue – 2021 Background	50	25	50	50	25	2750	2775	50	75	125	100
Average Queue – 2021 Buildout	50	25	50	50	50	2800	2825	50	75	150	125
95 th Percentile Queue-2021 Background	75	50	125	100	50	3775	3750	175	150	250	225
95 th Percentile Queue-2021 Buildout	100	50	125	100	125	3750	3725	175	150	275	250
Link Distance	232	-	370	-	-	3016	3016	-	-	838	838
Storage Bay Distance	-	200	-	100	200	-	-	200	500	-	-
Notes:											
XX=Queue exceeds available storage/link distance											

ACCESS SPACING STANDARDS

The City of Scappoose TSP states that the minimum driveway spacing for commercial properties on Collector streets is 100 feet¹⁵. As Walnut Street is classified as a Collector, 100 feet of access spacing is required. The proposed new driveways for the drive through on the western portion of the site will be spaced approximately 110 feet apart, and the eastern new driveway will be approximately 65 feet from the existing driveway on the eastern portion of the site and 135 feet from the intersection at US 30. The access spacing standards for the new driveways are met, except for driveway-to-driveway between the new drive-through egress driveway and the existing eastern driveway. The existing eastern driveway is proposed for closure to eliminate cut-through traffic, as noted previously. Doing so would allow the access spacing standards to be met.

The proposed westernmost driveway will be located approx. 20 ft from Fir Lane. The access spacing standard for proposed westernmost driveway is not met. Since the minimum driveway spacing standard is not met, potential safety hazard at the Fir Lane/SW Walnut Street intersection were evaluated. The substandard separation between the proposed westernmost driveway entrance and Fir Lane could potentially create a conflict point between southbound left-turn traffic on Fir Lane and westbound traffic on SW Walnut Street. A stop sign for southbound traffic on Fir Lane at SW Walnut Street is recommended, so southbound traffic on Fir Lane can slow, stop, and avoid other vehicles, should it become necessary. This will also help to mitigate potential sight distance visibility issues due to parked vehicles and proposed vegetation at the intersection.

¹⁵ 2016 Scappoose Transportation System Plan: Volume I, page 47.

PUBLIC WORKS DESIGN STANDARDS COMPLIANCE

A review of the City of Scappoose Public Works Standards yielded the following compliance point. Access spacing standards dictate that Minor Collector streets must have minimum centerline separation distances of 100 feet for driveway spacing from commercial properties. The proposed eastern access of the drive through will be approximately 135 feet from the intersection at US 30. The proposed western access of the drive through will be approximately 260 feet from the intersection at US 30 and 20 feet from the intersection at Fir Lane to the west. As such, the design standard is not met and mitigations were investigated.

Since the minimum driveway spacing standard is not met, potential safety hazard at the Fir Lane / SW Walnut Street intersection were evaluated. The substandard separation between the proposed westernmost driveway entrance and Fir Lane could potentially create a conflict point between southbound left-turn traffic on Fir Lane and westbound traffic on SW Walnut Street. A stop sign for southbound traffic on Fir Lane at SW Walnut Street is recommended so that southbound traffic on Fir Lane can slow, stop, and avoid other vehicles, should it become necessary. This will also help to mitigate potential sight distance visibility issues due to parked vehicles and proposed vegetation at the intersection.

TRANSIT, BICYCLE, AND PEDESTRIAN FACILITIES

The Columbia County Rider is a public transit bus route that has stops in the site vicinity on US 30. It is part of the Northwest Connector system that serves the cities of Columbia City, Saint Helens, and Scappoose. The Columbia County Rider provides service between the communities of Rainier, Saint Helens, Scappoose, and Vernonia; and it provides trips to Banks, Beaverton, Portland, and Kelso/Longview in Washington.

For bicycle facilities, there are bike lanes on both sides of the road on US 30. There are sidewalks on both sides of Walnut Street, SE High School Way, and US 30, except on the eastside of US 30 just south of SE High School Way. However, a sidewalk infill is outlined in the TSP for this segment of US 30. The proposed development includes improvements to the sidewalks along the site frontage.

MITIGATIONS AND SUMMARY

This memorandum summarizes the traffic impact analysis associated with the proposed development located at 52001 S Columbia River Highway in the City of Scappoose, Oregon. This analysis evaluated potential impacts to the transportation network for the year of the opening of the site, based on the standards established by the City of Scappoose and the Oregon Department of Transportation (ODOT).

A five-year crash analysis indicates no safety concerns at SW Walnut Street / SE High School Way at US 30 intersection. The intersection does not exceed the crash rate threshold of 1.0 crash per MEV and is below the statewide average crash rate.

An intersection performance analysis was conducted to document the existing and future operations for the SW Walnut Street / SE High School Way at US 30 intersection. This analysis shows that the intersection operates below standards during both peak hours under existing conditions. Since standards are not met under the existing and background growth conditions, this is considered a preexisting condition and mitigations are not recommended. Based on the trip generation analysis, the proposed land uses have the potential to add one trip during the PM peak hour which represents the worst peak hour intersection operations condition.

A peak hour queueing analysis was conducted for the proposed on-site drive-through to determine vehicle storage needs. A 2015 analysis performed by Global Transportation Engineering for a coffee shop with a drive through window and no indoor seating studied three similar drive-through coffee shops to determine on-site queues for this type of developments. Based on this study, a 95th percentile queue of 10 vehicles was observed during the AM peak hour (peak with highest on-site queues). There is enough drive-through storage to accommodate the estimated 10 vehicles during the AM peak hour. There is also an additional 210 feet of storage on Walnut Street before queues reach the intersection at US 30.

A queueing analysis was also conducted at the SW Walnut Street / SE High School Way at US 30 intersection. According to the queueing analysis for future conditions, the movements generating the greatest queueing penalties are the westbound right movement during the AM peak hour and the northbound through movement during the PM peak hour. Neither movement is projected to be significantly impacted by the proposed development. No queueing mitigations are recommended.

Sight distance was evaluated for the proposed roadway access points on Walnut Street. An existing bush located on the south side of the Varsity Grill building should be removed in order to achieve acceptable sight distance from the egress of the proposed drive-through.

The project site currently hosts two accesses serving the eastern parking lot of the property, one fronting US 30 and one fronting Walnut Street. During the AM peak hour, vehicle traffic uses the Varsity Bar and Grill driveway on US 30 and the eastern driveway on Walnut Street as a cut-through, moving into the eastbound through lane and skipping the southbound left queues at the US 30/Walnut Street/SE High School Way intersection. The existing eastern driveway on Walnut Street will be closed as part of this project to mitigate this issue. The closure of the eastern driveway on Walnut street will change the existing circulation of the eastern parking lot creating the need for a two-way circulation aisle. Only a 23 ft and 9 1/8 inches two-way circulation aisle can be accommodated between the standard/accessible parking spaces and the back of sidewalk. This does not meet the City of Scappoose Development Code which requires 24 feet two-way circulation aisles for 90-degree parking spaces. This is a compromise between eliminating cut-through traffic and providing a 24 feet two-way circulation aisle.

Access and driveway spacing standards were evaluated, and the proposed western driveway was found to not comply with Scappoose Public Works Design Standard 5.0070. A stop sign for southbound vehicles on Fir Lane is recommended to mitigate potential safety issues.

Preliminary Stormwater Drainage Report

Varsity Grill Redevelopment

52001 Columbia River Highway
Scappoose, OR 97056

Prepared for: William Kaven Architecture

Prepared by: Sofi Lam, EIT

Project Engineer: Ryan Milkowski, PE

November 2019 | KPFF Project #1900318



KPFF'S COMMITMENT TO SUSTAINABILITY

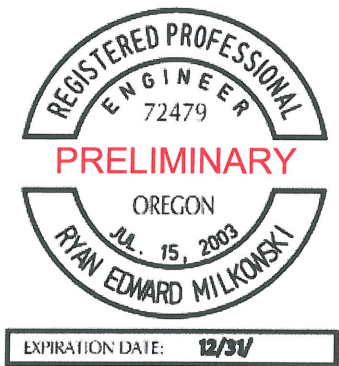
As a member of the US Green Building Council, KPFF is committed to the practice of sustainable design and the use of sustainable materials in our work.

When hardcopy reports are provided by KPFF, they are prepared using recycled and recyclable materials, reflecting KPFF's commitment to using sustainable practices and methods in all of our products.

Designer’s Certification and Statement

“I hereby certify that this Stormwater Management Report for the Varsity Grill Redevelopment has been prepared by me or under my supervision and meets minimum standards of the City of Scappoose and normal standards of engineering practice. I hereby acknowledge and agree that the jurisdiction does not and will not assume liability for the sufficiency, suitability, or performance of drainage facilities designed by me.”

Ryan Milkowski, PE



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Project Overview and Description

The project site is located at 52001 Columbia River Highway, on the northwest corner of Columbia River Highway and Walnut Street. The project will consist of tenant improvements to the existing building, parking lot revisions, and adding a drive through lane to service the existing building. The site is currently located in an expanded commercial (EC) zone with a gas station north of the property and residential housing west of the property, see below for vicinity map.



Figure 1 – Vicinity Map

Existing Conditions

The project site is currently on two separate lots. The drainage on the western lot drains into the two existing catch basins located in the parking lot and are routed to the existing drywell. The drywell was tested to determine its capacity and will need DEQ registration for permit approval. The building on the eastern lot has downspouts that discharge into the mulch area west of the building. The parking in the eastern lot drains to a catch basin located east of the parking area. There is an existing 20' road, sanitary, and water easement along the north property of the eastern lot and a powerline easement in the western lot.

Proposed Conditions

The project will consist of tenant improvements to the existing building and routing the roof drainage from the building to the storm system. The driveways on the eastern lot will be adjusted so there is a single point of entry on Columbia River Highway. The western lot will be reconfigured to include a drive through for the restaurant. Driveways and sidewalk will be constructed to facilitate the new drive through lane. The site will be graded to maintain existing drainage patterns and conveyance to the existing drywell.

There will be no increase of impervious area in the project scope. The total impervious areas for existing and post development are shown in Table 1.

TABLE # 1: On-Site Existing and Post-Development Areas

Description	Impervious Surface Area		Pervious Surface Area		Total Site Area	
	(sf)	(ac)	(sf)	(ac)	(sf)	(ac)
Pre-Development	27,375	0.63	596	.01	27,971	0.64
Post-Development	25,349	0.58	2,622	.06	27,971	0.64
Net Difference	2,026	0.05	2,026	.05	0	0

Methodology

The proposed stormwater management systems are designed in accordance with the City of Scappoose Public Works Design Standards (PWDS) and City of Portland Stormwater Management Manual (SWMM) for treatment and conveyance. The Santa Barbara Urban Hydrograph (SBUH) method was used verify the capacity of the existing drywell and the rational method was used for pipe conveyance.

Infiltration Rate Test

The existing drywell located in the western lot was tested per the PWDS Section 2.0046 using the EPA falling head percolation test. Based on the testing results, the capacity of the drywell is approximately 925 gpm prior to any safety factors. The water elevation in the drywell was held at a stable elevation to determine the maximum flow it can handle. See appendix 2 for the results of the rate test.

Stormwater Hierarchy

Based on the existing storm system of the project site, Category 2 Hierarchy is required to be met. Per SWMM Section 1.3.3, the drywell is required to handle a 10-year storm peak flow rate with a safety factor of 2 applied.

Proposed Stormwater Design

The proposed stormwater design will maintain existing drainage patterns with most of the runoff sheet flowing into the existing two catch basins which drain into the existing drywell. See Appendix 1 for basin map. The intent is to meet the required landscape coverage on the project site while maximizing the parking count. Grading will be minimized to maintain existing drainage patterns while meeting ADA standards. The existing downspouts from the building will be routed from a roof drain to the lateral discharging to the existing drywell. The existing lateral will be upsized to address the increased flow from the building.

Analysis

Due to the increase landscaped areas, the post development flow is less than the existing flows. See Table 2 for the runoff calculations based on the 10- and 100-year storm event.

Based on the infiltration rate test, the drywell is able to dispose of 925 gpm. Accounting for a safety factor of 2, the capacity of the drywell is 1.03 cfs, which is well beyond the required 0.482 cfs of the project. See Appendix 4 for the hydrographs for pre-development and post-development.

TABLE # 2: Pre vs. Post Construction Flow Rates

	Peak Flow Rates (cfs)	
	10 yr	100 yr
Pre-Development	0.493	0.710
Post-Development	0.482	0.701

The catchment areas are summarized in Table 3 below. The curve number is based on BES Sewer and Drainage Facilities Manual for paved streets and herbaceous grass cover and is assumed to be 98 and 85 for the impervious and pervious areas, respectively.

TABLE # 3: Catchment and Facility Summary

Basin	Source	Impervious Area (sf)	Pervious Area (sf)	Weighted C	Flows to
1	Asphalt Pavement	4,790	0	0.90	WQCB-1
2	Asphalt Pavement	4,801	0	0.90	WQCB-2
3	Roof	4,054	0	0.90	DW-1

Conveyance Design

The proposed pipes are adequately sized to convey the 10-year storm event using the Rational Method, see Appendix 5 for conveyance calculations.

Treatment

Drainage from the parking lot is captured by the catch basins before entering into the drywell. The existing catch basins will be replaced with a new stormfilter cartridge catch basin which will provide water quality treatment to the storm system. The number of cartridge and depth of the catch basin will be determined based on the water quality flow using the SBUH method. See hydrographs in Appendix 6. See Table 4 below for the water quality results. Per the SWMM, rooftop or pedestrian-only plaza runoff are not required to provide pollution reduction prior to subsurface infiltration therefore, the building downspouts will only have a filter to prevent large debris from entering into the storm system.

TABLE # 4: Water Quality Cartridges

	Source	Collected Area (sf)	WQ Flows (cfs)	Number of Filters Needed	Cartridge Height (in)
WQCB-1	Asphalt Pavement	4,790	0.018	1	27
WQCB-2	Asphalt Pavement	4,801	0.018	1	27

Maintenance and Operation

The proposed improvements will all be privately owned and maintained infrastructure. Maintenance and operation plan can be found in Appendix 7.

PRE-DEVELOPMENT EXHIBIT

Legend

- Ex. Impervious Area 27,375 sf
- Ex. Pervious Area 596 sf

- ### SHEET LEGEND
- PROPERTY LINE
 - DEMOLITION/WORK LIMITS (SHOWN OFFSET FOR CLARITY)
 - SAWCUT LINE
 - REMOVE OR ABANDON UTILITY LINE IN PLACE
 - EXISTING GRADE CONTOUR
 - REMOVE CONCRETE OR AC PAVEMENT

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PROJECT

VARSITY GRILL REMODEL

52001 Columbia River Hwy
Scappoose, OR 97056

DRAWING TITLE DEMOLITION PLAN

REVISIONS

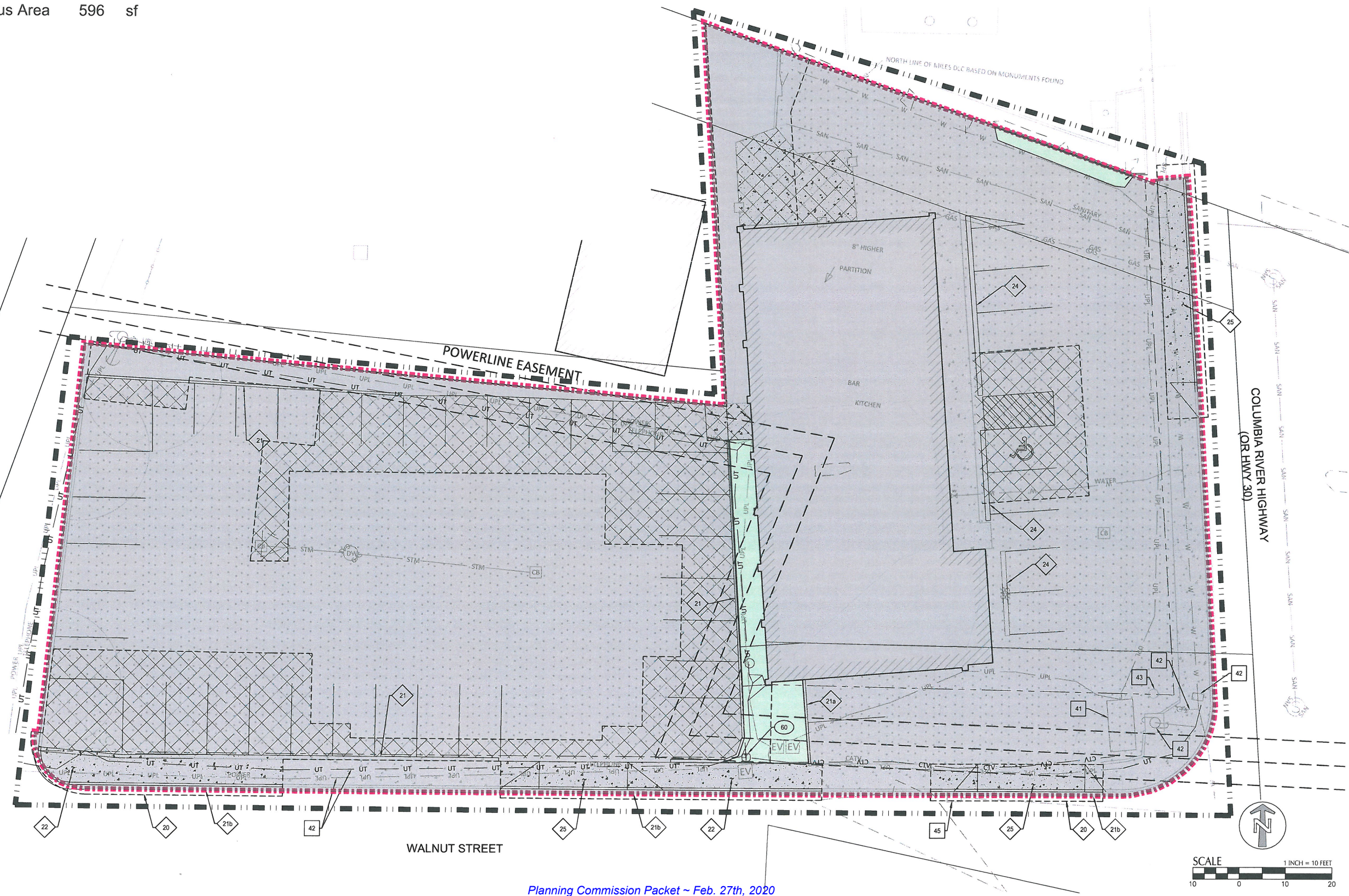
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Page 96 of 109
SITE DEV REVIEW

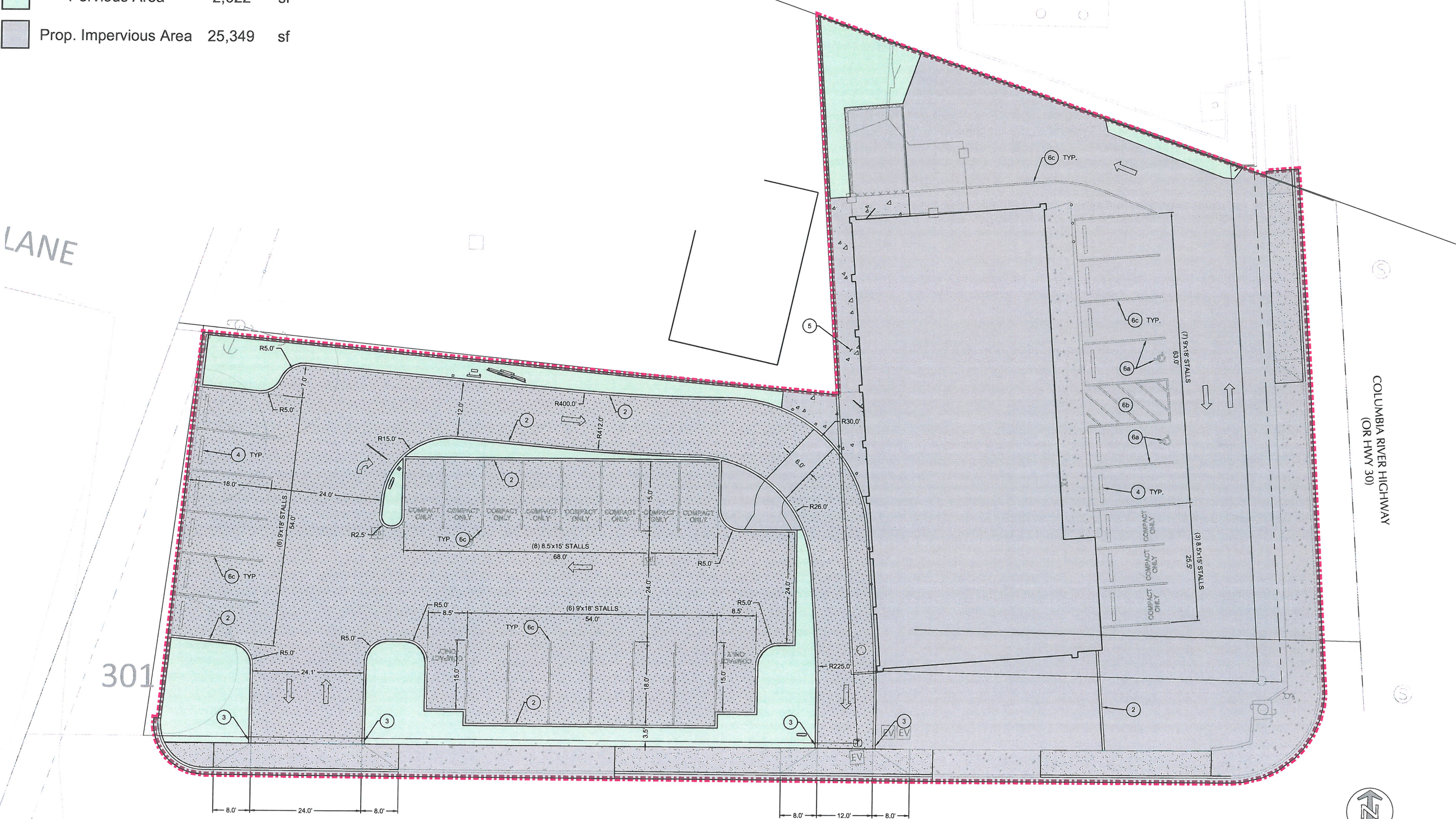


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Plotted: 11/5/19 at 4:12pm By: rcdau

POST-DEVELOPMENT EXHIBIT

Legend

Description	Quantity	Unit
Pervious Area	2,622	sf
Prop. Impervious Area	25,349	sf



WILLIAM / KAVEN

WILLIAM KAVEN ARCHITECTURE
4080 N. WILLIAMS AVE. STUDIO 100
PORTLAND, OR 97227
www.williamkaven.com

PROJECT

VARSITY GRILL REMODEL

52001 Columbia River Hwy
Scappoose, OR 97056

DRAWING TITLE

**SITE AND PAVING
PLAN**

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DRAWING NO.




C2.0

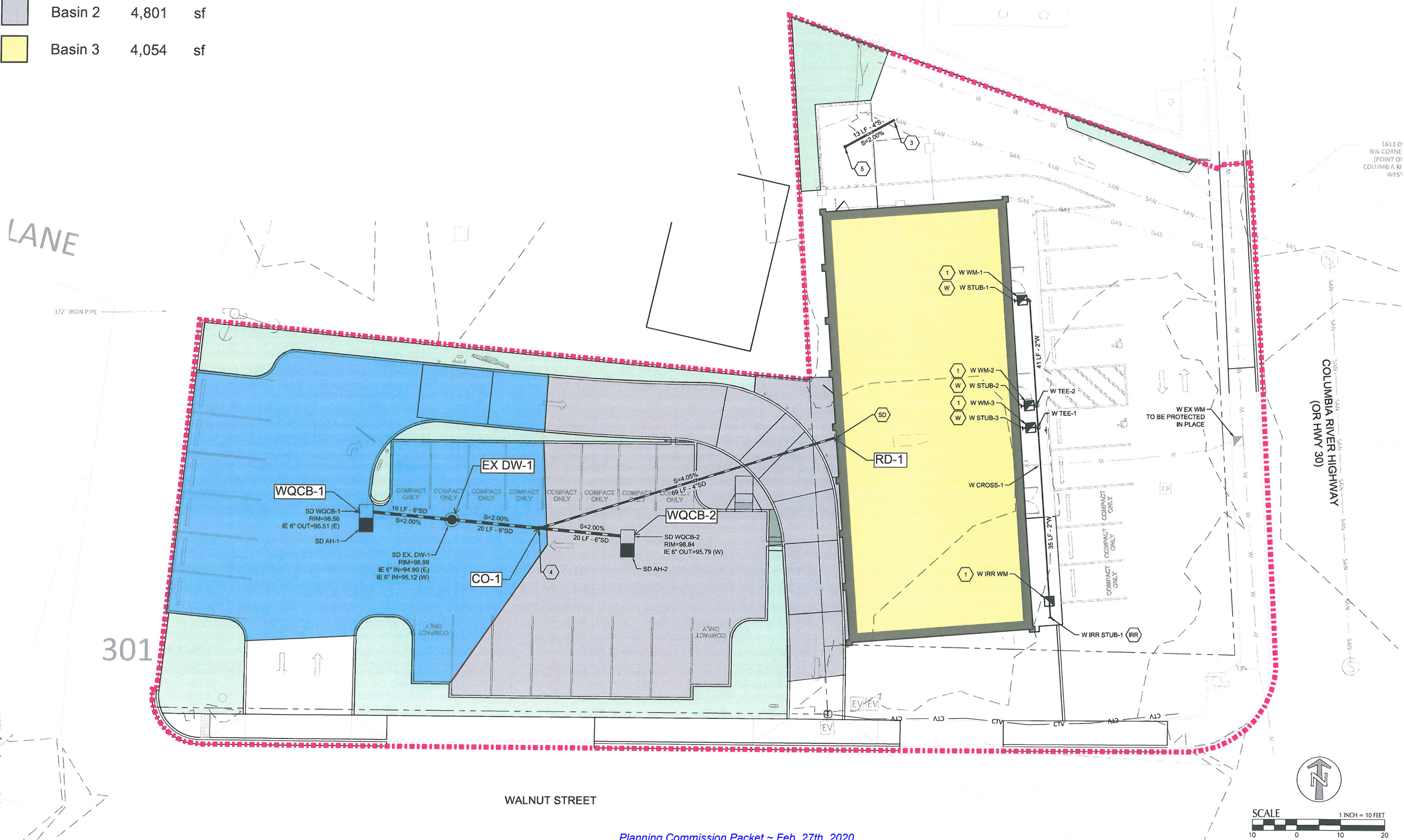
Page 97 of 109
SITE DEV REVIEW

File: \\civildp1\civ\Projects\c\p\2019\1900318-Varsity-Grill-Remodel\CAD\PLT\19318-20-SITE&PAVING.dwg TAB: C2.0
Plotted: 11/1/19 at 6:53am By: rcoada

BASIN MAP

Legend

Description	Quantity	Unit
 Basin 1	4,790	sf
 Basin 2	4,801	sf
 Basin 3	4,054	sf



PROJECT

VARSITY GRILL REMODEL

52001 Columbia River Hwy
Scappoose, OR 97056

DRAWING TITLE
UTILITY PLAN

REVISIONS

NOT FOR
CONSTRUCTION

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DATE November 8, 2019
JOB NO. 19.04

DRAWING NO.

C4.0

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SITE DEV REVIEW



Oregon

Kate Brown, Governor

Department of Transportation

Region 2 Tech Center

455 Airport Road SE, Building A

Salem, Oregon 97301-5397

Telephone (503) 986-2990

Fax (503) 986-2839

DATE: February 19, 2020

TO: Karen Strauss, PE
Development Review Coordinator

FROM: *Keith P Blair*
Keith P. Blair, PE
Interim Traffic Analysis Engineer

SUBJECT: 52001 S Columbia River Highway (Scappoose) – Outright Use
Revised TIA Review Comments

ODOT Region 2 Traffic has completed our review of the submitted revised traffic impact analysis (dated February 18, 2020) to address traffic impacts due to redevelopment on the northwest quadrant of US-30 and SW Walnut Street in the city of Scappoose, with respect to consistency and compliance with current versions of ODOT's Analysis Procedures Manual (APM). Both versions of the APM were most recently updated in December 2019. Current versions are consistently published online at: <http://www.oregon.gov/ODOT/TD/TP/Pages/APM.aspx>. As a result, we submit the following comments for the consideration of Region Development Review and the City:

Proposed mitigation comments:

1. ODOT maintains jurisdiction of the Lower Columbia River Highway No. 92 (US-30) and ODOT approval shall be required for all proposed mitigation measures to this facility.
2. No mitigation measures have been proposed. The US-30/Walnut Street intersection will exceed the current Oregon Highway Plan mobility target (0.85) in 2021 under the AM background scenario (0.86) and the proposal will decrease operations to 0.88. However, the Oregon Transportation Commission (OTC) currently has an action item on its March 2020 agenda to review and make a decision on adopting an alternative mobility target of 1.0 for signalized intersections in Scappoose. If the OTC adopts the proposed mobility target, mitigation may be determined to not be appropriate.

Thank you for the opportunity to review this revised traffic impact analysis. As the analysis software files were not provided, Region 2 Traffic has only reviewed the submitted report. This traffic impact study has been, for the most part, prepared in accordance with ODOT analysis procedures and methodologies. No further analysis work should be required. If there are any questions regarding these comments, please contact me at (503) 986-2857 or Keith.P.Blair@odot.state.or.us.

Laurie Oliver

From: STRAUSS Karen A <Karen.A.STRAUSS@odot.state.or.us>
Sent: Wednesday, February 19, 2020 11:25 AM
To: Laurie Oliver
Cc: BLAIR Keith P; KEARNS Richard A; WILLIAMS Virginia L
Subject: FW: Varsity Grill Remodel Revised TIA (2/18/20) - ODOT Region Traffic Comments
Attachments: 52001 S Columbia River Hwy (Scappoose) - ODOT Region Traffic Comments 20200219.pdf

Hi Laurie,

Looks like Keith's comments are minimal. From what I understand, the developer is proposing to close the access on Walnut that's closest to US-30 which is a good mitigation for the increased traffic rates.

We have no further comments other than the ones attached from Keith.

Thanks,
Karen

Karen A. Strauss, PE

Development Review Coordinator, District I

From: BLAIR Keith P <Keith.P.BLAIR@odot.state.or.us>
Sent: Wednesday, February 19, 2020 11:09 AM
To: STRAUSS Karen A <Karen.A.STRAUSS@odot.state.or.us>
Cc: SHONKWILER Kenneth D <Kenneth.D.SHONKWILER@odot.state.or.us>; UPTON Dorothy J <Dorothy.J.UPTON@odot.state.or.us>
Subject: Varsity Grill Remodel Revised TIA (2/18/20) - ODOT Region Traffic Comments

Karen:

Attached are Region Traffic's comments on the submitted revised TIA for the "Varsity Grill Remodel" in Scappoose. As direct access to a state highway has been proposed, this study has been required under ODOT's authority. Upon your review, please forward these comments to the City along with any additional comments you feel are necessary or prudent.

Please let me know if you have any questions or I can help with anything else and please copy me when you provide comments to the local jurisdiction. Thanks!

Keith P. Blair, P.E.

Region Traffic Manager | ODOT Region 2
455 Airport Rd SE, Bldg. A | Salem, Oregon 97301
(503) 986-2656 | Keith.P.Blair@odot.state.or.us

ODOT's mission is to provide a safe and reliable multimodal transportation system that connects people and helps Oregon's communities and economy thrive.

From: Laurie Oliver <loliver@cityofscappoose.org>
Sent: Tuesday, February 18, 2020 2:04 PM

Laurie Oliver

From: STRAUSS Karen A <Karen.A.STRAUSS@odot.state.or.us>
Sent: Tuesday, February 18, 2020 10:09 AM
To: Laurie Oliver
Subject: RE: Varsity Grill Remodel TIA (12/24/19) - ODOT Region Traffic Comments
Attachments: 52001 S Columbia River Hwy (Scappoose) - ODOT Region Traffic Comments 20200213.pdf

Hi Laurie,

I'll just make official comments that you can forward to the applicant! That way they're in my words and you don't have to interpret. FYI, the engineer sent me a COU permit application this morning, so they are aware of the requirements already.

Here are ODOT's comments regarding the Varsity Grill Remodel project:

ODOT has reviewed the TIA submitted for the Varsity Grill Remodel project. They are attached for your review. After reviewing the proposal, ODOT has concluded that this project will require the applicant to submit a Change of Use permit application. Adding a drive-through coffee will increase the trips at this property, and it is also close to a signal and in proximity to a top 5% SPIS site (Safety Priority Index System.)

The COU application must be complete and ODOT's permit issued prior to operating the business in any capacity.

Karen A. Strauss, PE

Development Review Coordinator, District I



FIRE MARSHAL

Columbia River Fire & Rescue / Scappoose Fire District



Date: 02/5/2020

Laurie Oliver

RE:

LAND USE ACTION REFERRAL (SDR5-19, MiV3-19)

PHK Development

Property Description: 3213-BA-00300 and 3213-BA-00100

Dear Laurie:

I received the Land Use Action Referral regarding the above referenced project. Based on what was submitted, the fire district has a few comments and findings, but we have no objections.

1. All items that are identified in the Fire code Guide (Adopted by ordinance) need to be adhered to and completed by final occupancy and inspection of the building.
 - i. Specifically, the address numbers shall be 10 inches tall by one and three quarters of an inch wide (10" x 1 ¾").
 - ii. Please work with the fire district and the city to identify if the two spaces will be units or suites for addressing.
 - iii. If any sort of hood system is installed, a knox box will be required for this building.

Should you have any questions about anything else, please do not hesitate to give me a call.

Sincerely,

A handwritten signature in blue ink, appearing to read "Jeff Pricher".

Jeff Pricher

Division Chief

Fire Marshal (CRF&R / SRFD)



SCAPPOOSE
Oregon

1/31/2020

Laurie Oliver
City Planner
Scappoose, OR

Dear Laurie,

I have reviewed the Land Use Action Referral (SDR5-19, MiV3-19), Varsity Grill Remodel Proposal. Upon my review, I have noted a few items of concern regarding Public Works.

I understand that this proposal is a preliminary design for land use approval and will likely change. The following bulleted items are my general concerns with the current proposal.

- Near the North end of the structure appears to a proposed trash enclosure. Per SPWD Standard 3.0024, structures cannot be constructed over sewer mains or sewer easements.
- The proposed new entrance near the West end of the property appears to not meet the requirements of SPWD Standard 5.0070. Since Walnut is a Collector, the setback from the private drive to the West should be 100 feet.
- My final concern at this time is that there is a communication pedestal and utility vault located in the new proposed driveway exiting the drive through. My experience is that these can be difficult to be made traffic rated and ADA compliant.

Please note that these are only my observations of items that may need further attention.

Sincerely,

Dave Sukau

Public Works Director

CITY OF SCAPPOOSE

33568 E. COLUMBIA AVE.
SCAPPOOSE, OREGON 97056
(503) 543-7184

LAND USE ACTION REFERRAL (SDR5-19, MIV3-19)

January 23, 2020

RETURN TO: Laurie Oliver, City Planner, City of Scappoose, 33568 East Columbia Ave, Scappoose, OR, 97056 by February 5, 2020. Please email your response to: loliver@cityofscappoose.org.

REGARDING: PHK Development, Inc. has submitted an application for Site Development Review (SDR5-19) to allow for a remodel of the existing Varsity Grill into three new tenant spaces, to include: a drive thru coffee shop, and two retail/office spaces. No additional square footage is proposed. Exterior alterations proposed include storefront replacement, additional doors/windows, drive-thru window, ATM alcove, canopy fascia, and parking lot upgrades. The requested minor variance (MIV3-19) is required to make alterations to the existing building, which is a non-conforming use due to its existing setbacks. The site is located north west of the SW Walnut Street and Columbia River Hwy intersection, on property described as Columbia County Assessor Map # 3213-BA-00300 and 3213-BA-00100.

1. ☐ We have reviewed the enclosed application and have no objection to its approval as submitted.
2. ☒ Please see either our comments (below) or attached letter.
3. ☐ We are considering the proposal further and will have comments to you by _____.
4. ☐ Our board must meet to consider this; we will return their comments to you by _____.
5. ☐ Please contact our office so we may discuss this.
6. ☐ We recommend denial of the application. Please see either our comments (below) or attached letter:

COMMENTS: Columbia River PUD would like to request a 5' PUE along the south and the west property lines of tax lot 0300 (account #4588) and along the easterly property line of tax lot 100 (account #4586). The purpose of these PUE's would be for the replacement of conductor with the least impact on the subject properties.

Signed:



Title:



Date:



JOHN A. RANKIN, LLC.
Attorney at Law
5 Centerpointe Drive, Suite 400
Lake Oswego, OR 97035
(503) 625-9710 / Fax (503) 625-9709
email: john@johnrankin.com

February 20, 2020

VIA EMAIL ONLY

Ms. Laurie Oliver, CFM
City Planner & Planning Department Supervisor
City of Scappoose
33568 E Columbia Ave.
Scappoose, OR 97056
loliver@cityofscappoose.org

Re: Written Testimony
Docket #SDR 5-19, MiV3-19
Applicant: PHK Development Inc.
Site Development and Variance
Varsity Grill Property

Dear Laurie:

Please accept this letter as my clients', Cliff Bauer, Bauer Rentals LLC and Bauer Square LLC, written testimony regarding the above noted application and docket number for your consideration and recommendation for findings of fact and conditions of approval for your drafting your staff report.

Based on our review of the City's Notice and attached Site Plan and the Traffic Impact Analysis you forwarded to me along with my preliminary additional research and conversations with you and City Engineer Chris Negelsbach, we recommend that the City approve the application and variances, subject to our request that the City's approval include the following or similarly stated additional findings of fact and recommended Conditions of Approval:

1. The north-south portion of "Fir Lane", which has been shown on the Applicant's Site Plan as located adjacent and to the west of the subject property, is not an officially designated public or private City street, but is actually a privately-owned tract of land which has been improved and paved over the years to connect the actual dedicated east-west Fir Lane public right-of-way located to the north of the subject property.

2. The north-south privately owned portion of "Fir Lane" has been used for many years by the existing residences and businesses to the west and north of the subject property to access Walnut Street and Highway 30, but in most cases, without a recorded legal right of access and no maintenance agreement.

3. The right of the general public to use this north-south portion is at best uncertain and at worst non-existent.


4. Recommended Conditions of Approval:

- a. The Applicant shall install a new stop sign within and along the north side of the public right-of-way of Walnut Street at a location approved by the City Engineer to stop south bound local traffic on the north-south privately-owned portion of "Fir Lane" before proceeding onto Walnut Street.
- b. The Applicant shall install a new "Private Access - No Outlet" traffic sign along the north side of the public right-of-way of Walnut Street at the north end of the new curb return at the southwesterly corner of the subject property at a location approved by the City Engineer to redirect customers of the businesses on the subject property and the general public away from using the privately-owned portion of "Fir Lane".

Please email me a copy of your staff report and the City's Final Notice of Decision for this application.

I will keep you and Chris informed of the results of our additional research to establish the legal status of the privately owned portions of "Fir Lane" with the hope of getting all local residential and business users to sign a Access Easement and Maintenance Agreement, pending future development of the area served by "Fir Lane".

If you have any questions or need additional information, please call or email me. Thank you for your help!

Very truly yours,

John A. Rankin

JAR/bhs
Encl: None
Pc: Clients

Laurie Oliver

From: Patrick Kessi <pkessi@phkinc.com>
Sent: Thursday, February 20, 2020 2:49 PM
To: Laurie Oliver
Cc: Chris Negelspach; Mike Perso
Subject: RE: Written Testimony - Varsity Grill Docket N. SDR 5-19 and MiV 3-19

Hello Laurie,

"The applicant does not object to providing the requested signage, as approved by the City Engineer"

Warm regards, Pat

PATRICK H. KESSI
PHK Development, Inc.
3330 NW Yeon Ave., Suite 210
Portland, OR 97210
503.248.9370 main office
503.248.9374 direct
503.577.0955 mobile

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CITY OF SCAPPOOSE



February 2020

Sunday	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday
						1
2	3 No City Council meeting	4	5	6	7	8 ATM Scappoose Middle School 8:30 to Noon
9	10	11	12 South County Leadership Collaborative meeting 5:30pm	13	14	15
16	17  President's Day City offices closed	18 Work Session 6pm Council meeting 7pm	19	20 EDC ~ noon Park & Rec Committee 6pm	21	22
23	24	25	26	27 Planning Commission 7pm	28	29 City Council Retreat

CITY OF SCAPPOOSE



March 2020

Sunday	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday
1	2 Urban Renewal Agency Mtg 6pm City Council 7pm	3	4	5	6	7
8	9	10	11	12	13	14
15	16 Council Work Session 6pm City Council 7pm	17	18	19 EDC ~ noon Park & Rec Committee 6pm	20	21
22	23	24	25	26	27	28
29	30	31				